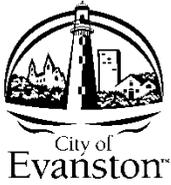


3434 Central Street
Kensington School

Planned Development & Special Use
25PLND-0021

LUC Recommending Body



Memorandum

To: Chair and Members of the Land Use Commission

From: Sam Hubbard, Senior Planner

CC: Sarah Flax, Director of Community Development
Elizabeth Williams, Planning Manager & Interim Housing & Grants Manager
Jeremiah Bebo, Development Services Supervisor

Subject: Special Use for a Planned Development at 3434 Central Street for the Kensington School daycare, 25PLND-0021

Date: January 14, 2026

Request

The applicant, Charles Marlas of the Kensington School, has submitted an application requesting approval of a Special Use for a Planned Development (PD) and a Special Use for a "Daycare Center, Child", to allow construction of a 19,952 square foot daycare with 38 parking spaces in the R2 (Single-Family Residential) zoning district. As part of this application, the following Site Development Allowances have been requested:

- 1) To allow a 5' wide landscape strip along the entire western property line and a 0' wide landscape strip along a portion of the southern property line, where Code requires a minimum 10' wide landscape strip along both lot lines.
- 2) To allow a 62.22% impervious surface lot coverage where impervious surface lot coverage is restricted to a maximum of 55%.
- 3) To allow parking within a front yard setback and a side yard setback where parking is prohibited in front and side yard setbacks.
- 4) To allow a 6' tall dumpster enclosure fence and a 6' tall and 4'-5' tall playground fence within the front yard where fence heights in a front yard are restricted to 4' tall.
- 5) To allow open off-street parking in a residential district that is greater than 30' setback from a rear property line where open off-street parking in a residential district shall be setback no greater than 30' from a rear property line.
- 6) To allow a 16.5' wide drive aisle where a minimum drive aisle width of 24' is required (this applies to the drive aisle adjacent to the First Williamsburg Co-Op parking stalls).

The applicant may seek, and the Land Use Commission may consider, additional Site Development Allowances as may be necessary or desirable for the proposed development.

Notice

The application has been filed in conformance with applicable procedural and public notice

requirements, including a public hearing notification published in the Evanston Review on December 25, 2025, public notice signage posted on the site, and public notification postcards mailed to all property owners/taxpayers of record within 1,000 feet of the planned development boundary. Details of the development have also been posted on the City’s website for several months.

General Information

Applicant/Owner: Charles Marlas of the Kensington School
743 McClintock Dr
Burr Ridge, IL 60527

PINs: 10-10-200-073-0000

Existing Conditions and Project History

The subject property is 1.13 acres (57,942 square feet) in size and located on the southwest corner of Central Street and Gross Point Road. The lot is irregularly shaped and features a dual-angled property line along the south and an angled property line along the east. The site is currently occupied by the Unity Church on the North Shore, which includes a parking lot on the western side of the site that also provides access to the First Williamsburg Co-Op at the south. This access is governed by a 16-foot-wide easement that extends south off Central Street through the existing church parking lot to the common property line with the Co-Op, then extends to the southwest along the common property line to provide access to the Co-Op’s parking spaces. The Co-Op’s parking area, located at the rear of their property, does not have direct access to a public street and is only accessed through the church’s parking lot.

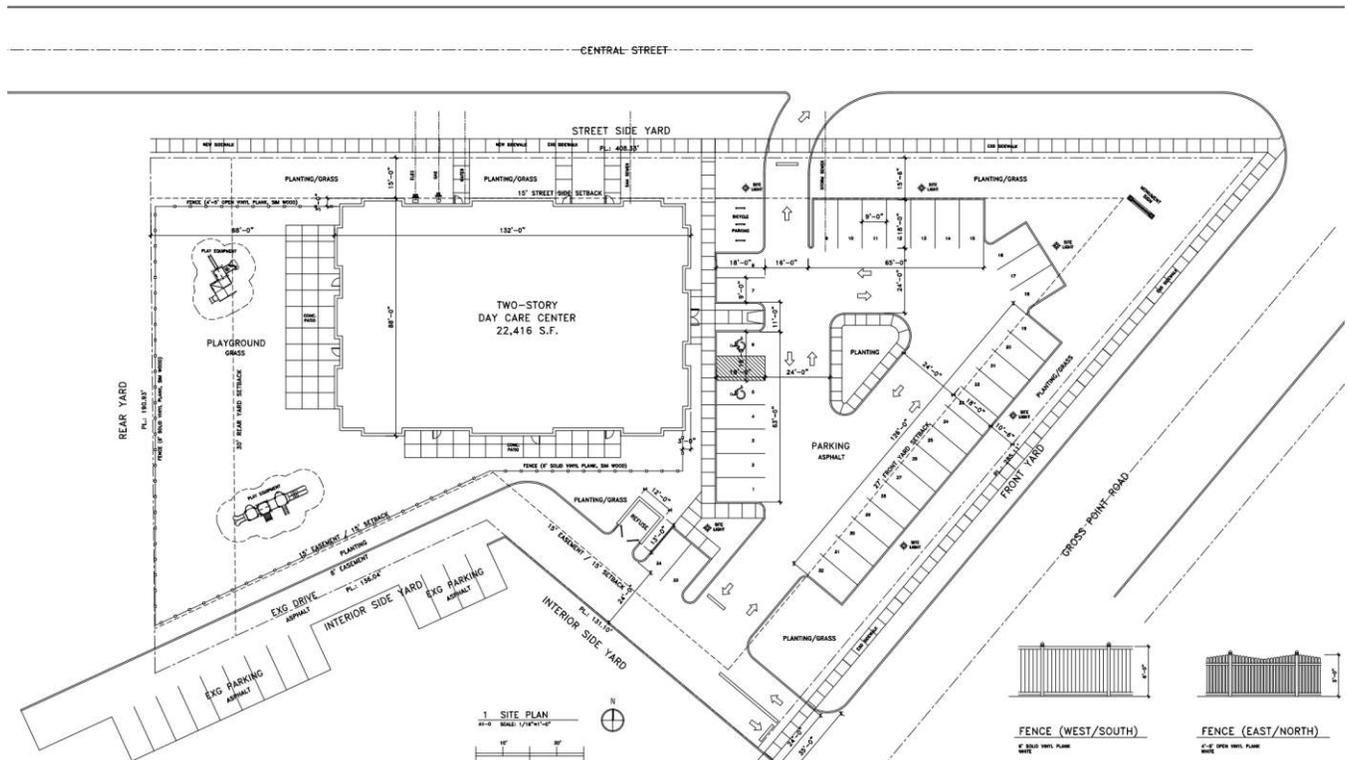
The north side of Central Street abutting the property has an approximately 4’ sidewalk that directly abuts the street, and the south side of Central Street has a traditional 5’ wide sidewalk with a parkway separating it from the street. The south sidewalk extends from Gross Point Road to the existing parking lot entrance for the church, where it subsequently terminates. An aerial of the site is shown below, with the existing easement shaded in blue:



Immediately to the north and west of the subject property are single-family homes. Abutting to the south is the First Williamsburg Co-Op, which is a moderate density multi-family development consisting of three buildings that are 2-stories tall. Across Gross Point Road are the North Pointe Condominiums, which is a 5-acre multi-family development consisting of varied building heights (2 to 4 stories) and a small commercial component at the Crawford Avenue/Central Street intersection. Approximately 60' to the west of the subject property is a large Municipal water tower owned by the City of Evanston.

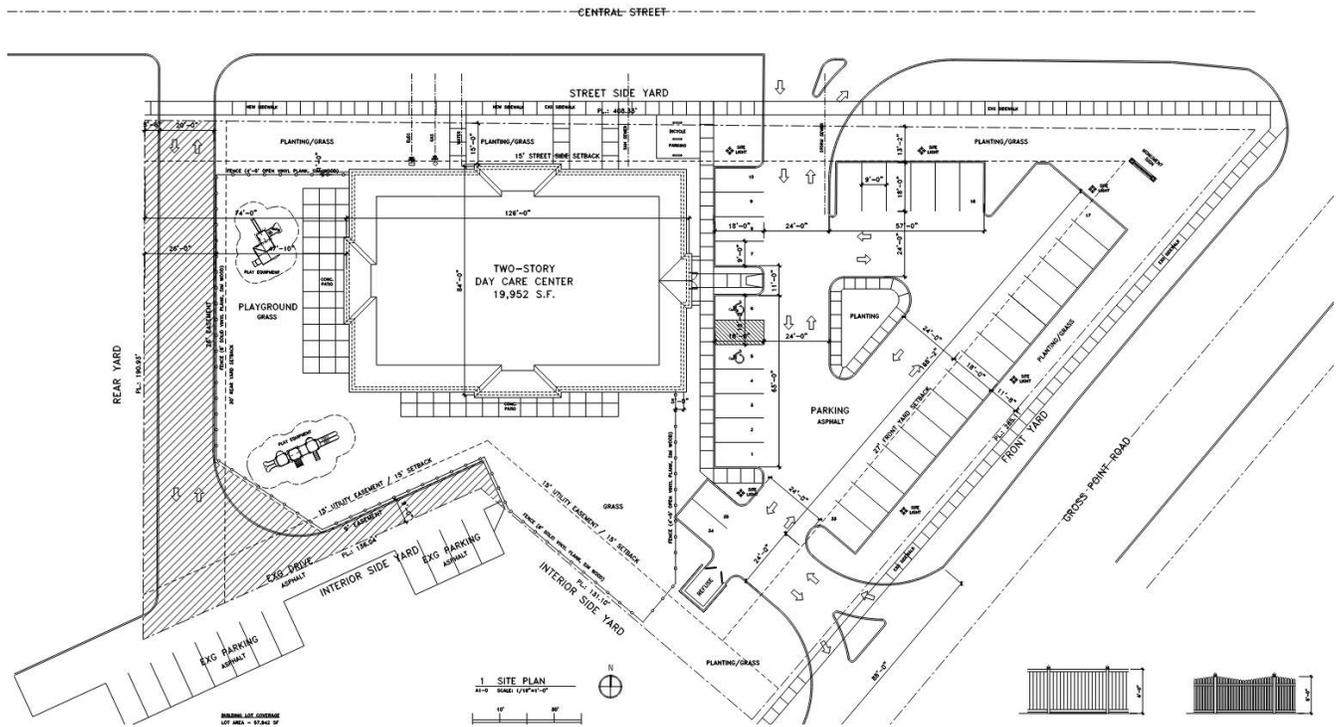
Project History

The Kensington School originally proposed a daycare on the subject property in 2022, which appeared before the Land Use Commission for a public hearing on June 22, 2022. The hearing resulted in a continuance to potential IDOT issues with the proposed full access Gross Point curb cut, as well as issues with the relocated access to the First Williamsburg Co-Op. The 2022 site plan is shown below for reference:



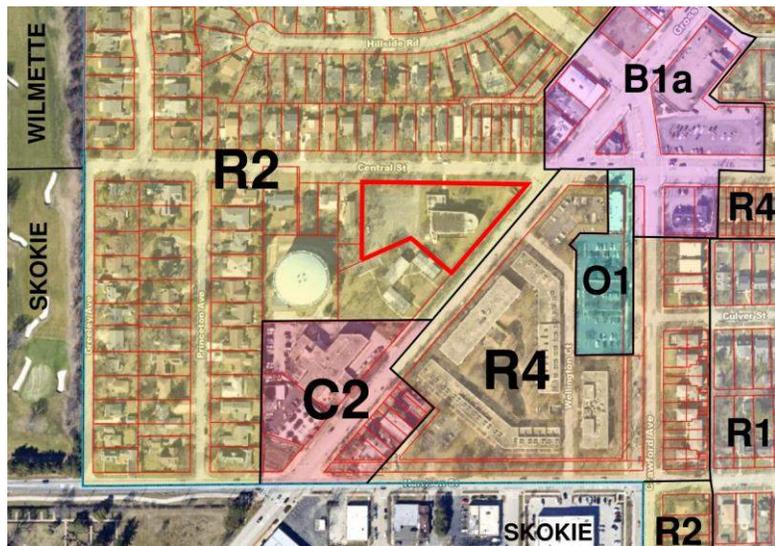
The applicant re-appeared before the Land Use Commission on May 10, 2023 with a revised proposal that is substantially similar to their present request. The revised plan modified the full access curb cut along Gross Point Road to a right-in/right-out configuration and the applicant provided a letter from IDOT demonstrating support for this configuration. Access along Central Street was revised to provide full access ingress and right-out only egress. Access drive to the Co-Op was relocated to the western side of the site and was now completely independent from the Kensington School parking lot. The Land Use Commission voted 3-4 in favor of a recommendation of approval (resulting in a failed motion for approval) and primarily expressed

concerns with the relocated access to the Co-Op (which requires a revised easement that had not yet been provided) and traffic. The application was never forwarded to the City Council since issues related to the relocated Co-Op access were not resolved. The 2023 site plan is included below for reference:



Zoning & Land Use

The subject property is located within the R2 (Single-Family Residential) district. The graphic below and chart on the next page illustrates the existing zoning within the vicinity of the subject property (subject property outlined in red):

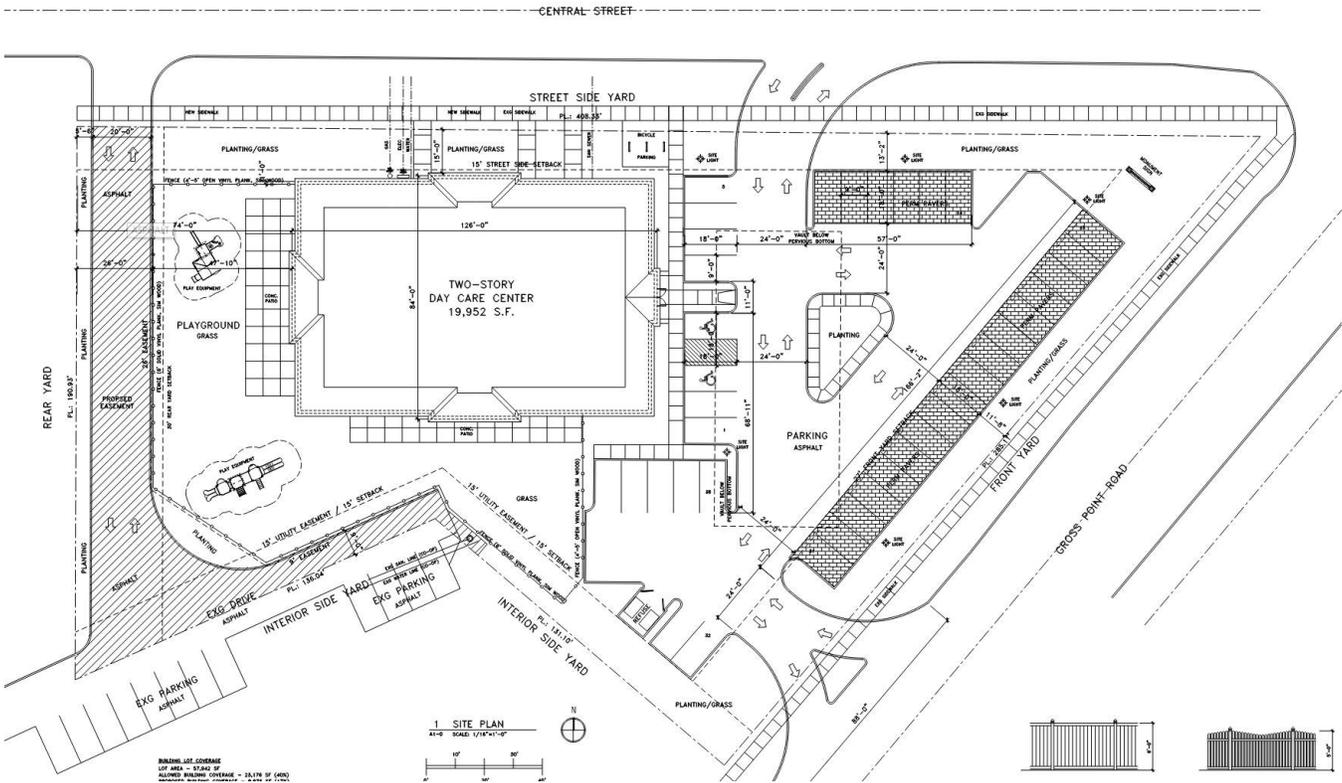


	Zoning District	Land Use
North	R2 Single-Family Residential	Single-Family Homes
South	R2 Single-Family Residential	Multi-Family (First Williamsburg Co-Op)
East	R4 General Residential	Multi-Family (North Pointe Condos)
West	R2 Single-Family Residential	Single-Family Home

The existing land uses to the north and west are single-family and to the east and south are multi-family. The irregular configuration of the site, as well as its frontage along a major arterial street (Gross Point Road) and proximity to the large water tower to the west, makes redevelopment of the site less ideal for single-family homes. Multi-family uses may be viable, however, they are not allowed by the current R2 Single-Family zoning of the site. Daycare uses are allowed in the R2 District via approval as a Special Use.

Analysis

The applicant proposes demolition of the existing church to allow construction of a two-story, 19,952 square-foot daycare with 38 onsite parking stalls. Kensington School is a private nursery, preschool, and kindergarten providing full-day programming. The proposed daycare center will accommodate up to 166 students and have approximately 26 staff. The facility will operate between 6:30 a.m. - 6:30 p.m. on each weekday and is closed over the weekend. The proposed site plan is shown below:



The Kensington School building would be located within the center of the property and would have playground areas on the west side and the parking lot on the east side of the site. The location of the building is directly above the 16' access easement, water main, and sewer line that serves the Co-Op. Access to the Kensington School parking lot would come from a left-in/right-out curb cut along Central Street and a right-in/right-out curb cut along Gross Point Road. A total of 38 parking spaces would be provided, including 2 accessible spaces. The parking lot would be used for parents/guardians to drop-off and pick-up their children and for staff parking. Access to the First Williamsburg Co-Op would be shifted to the western side of the site and governed by a newly created easement that would be a minimum of 20' in width. As in the 2023 proposal, this access would be separate from the Kensington School parking lot, meaning traffic for the Co-Op would not mix with traffic from the school and the driveway would be used only by the Co-Op.

In January of 2025, the Circuit Court of Cook County issued a court order addressing the relocated Co-Op access, finding that the relocation would not substantially alter the Co-Op's use of the easement, provided that the applicant record a new easement that would cover the relocated access and that the Co-Op's access to Central Street would not be interrupted during construction of the daycare. A copy of the Court Order is included in the packet provided to the Land Use Commission. Staff recommends a condition of approval requiring the applicant to record the relocated access easement prior to issuance of a building permit, which aligns with the requirements of the Court Order.

A refuse enclosure is located to the south of the building and screened by a 6' tall masonry fence. The materials will include red face brick, gray limestone, and a white steel gate to match the architecture and materials of the building.

Site Development Allowances

Site Development Allowances are aspects of a development that do not conform to code requirements but may be granted by the City Council as an incentive for providing the benefits of a planned development. The proposed development conforms to all setback, height, bulk, and parking requirements, with exception to six areas of non-compliance that require the following Site Development Allowances:

- Reduction to the width of a required landscape strip
- Setback of the parking lot
- Location of the parking lot
- Increase in the maximum allowable Impervious Surface Lot Coverage.
- Increase in the maximum allowable height of fences.
- Reduction to the minimum allowable width of a parking lot drive aisle.

Landscape Strip

Per Code, Planned Developments ("PDs") in all residential districts are required to provide a 10' wide landscape strip along property lines that do not abut a public street. Because PD's are typically larger-scale developments, this requirement is meant to provide relief to neighboring property owners as may be necessary from a large PD. However, along the west property line,

located within 30' of a rear property line (the western property line is considered the rear property line and the eastern lot line is considered the front property line). The proposed parking lot is located east of the building and encroaches into the front yard. Additionally, a small portion of the northern-most parking stalls extend past the façade of the building and encroach into the street side yard. None of the parking is located within 30' of the rear lot line.

The 30' requirement is more directly applicable to residential developments in residential districts as the location of parking in the rear of a lot provides better screening and is typically accessed from an alley. For a commercial development in a residential zoning district, such as the Kensington School, the 30' requirement is not as practical. The rear 30' of the subject property is not big enough to accommodate a parking lot large enough to provide the minimum number of parking spaces as required by Code for the proposed commercial use. Additionally, the subject property does not have access to an alley.

Given the irregular and angled shape of the lot, to avoid placing the parking in the front and street side yards, the building and playground areas would need to be shifted closer to Gross Point Road. This would require the parking areas to be further from the building and blocked by the playgrounds, or the playgrounds would need to be placed directly abutting Gross Point Road and Central Street. Neither of these scenarios are desirable and would likely create the need for additional Site Development Allowances. Additionally, placement of the parking lot in front of the building and along the east side of the site keeps the curb cut closer to Gross Point and further from the residential areas to the west. The Community Development Department is supportive of both requested Site Development Allowances.

Maximum Allowable Impervious Surface Lot Coverage

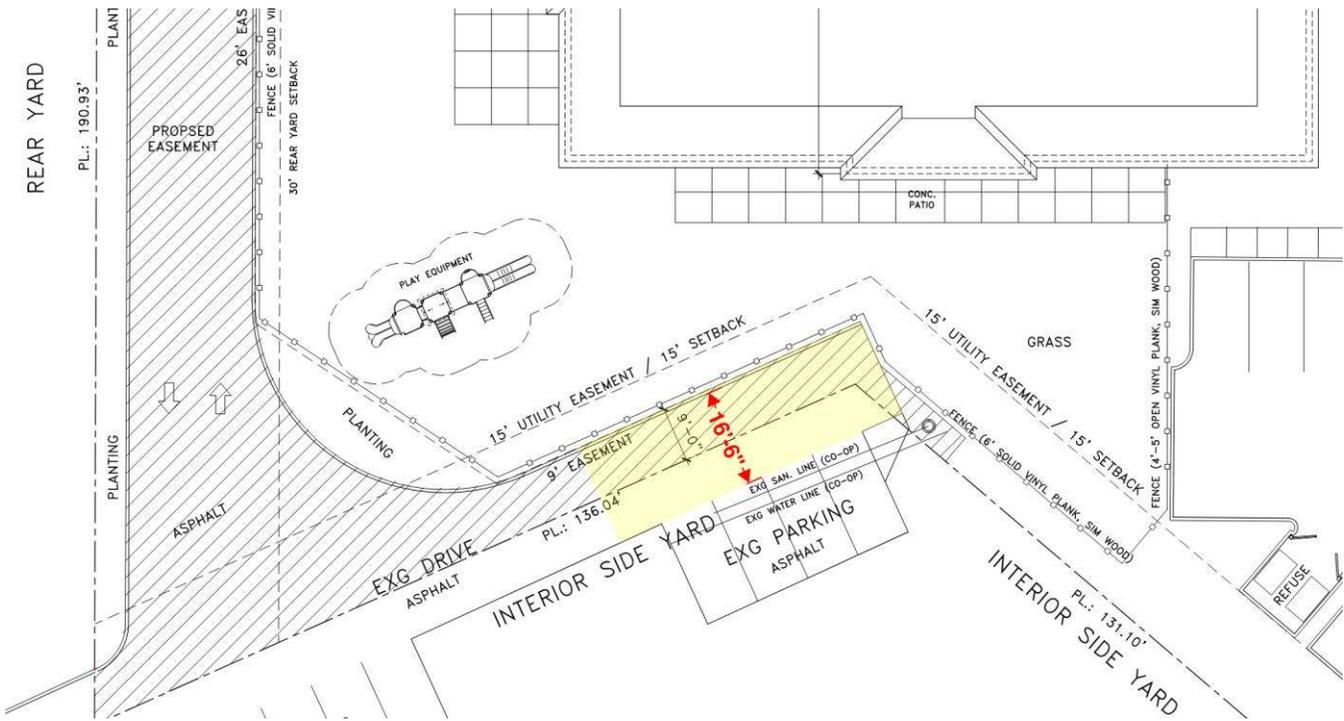
Impervious surface lot coverage in the R2 District is limited to a maximum of 55% and the applicant has proposed 62.22% impervious surface coverage. The existing church and associated improvements amount to approximately 69% impervious surface coverage. The proposed development results in a more compliant use of the property. Furthermore, the need to provide access to the Co-Op site, as required by the Court Order, necessitates more impervious surfaces on the subject property. Without the Co-Op access drive, the site would comply with impervious surface coverage requirements. In an effort to further reduce the impervious surfaces on the site, the applicant has incorporated permeable pavers within certain areas of the parking lot. The Community Development Department is supportive of the impervious surface Site Development Allowance.

Fence Height

Fences within a front yard are limited to 4' in height. The 6' tall dumpster enclosure is considered a fence and is proposed within the front yard. Additionally, a small portion of the playground fence, which will be 4'-5' in height and 6' in height in certain portions, encroaches slightly into the front yard. Both of these site improvements require a Site Development Allowance. Staff notes that the 6' height of the enclosure is necessary to fully screen the dumpsters. Additionally, the enclosure is designed of quality materials to match the building, which mitigates the impact of the encroachment. The extent of encroachment of the playground fences into the front yard is minimal and the height is necessary for security of the children attending the daycare. Staff is supportive of the requested fence height Site Development Allowance.

Drive Aisle Width

Where a two-way drive aisle in a parking lot provides access to a parking stall, the drive aisle shall provide a minimum width of 24' where access to parking stalls is provided. The Kensington School proposal would result in a 16.5' wide drive aisle adjacent to 4 parking stalls located on the Co-Op property. The specific area of non-compliance is shown below, shaded in yellow:



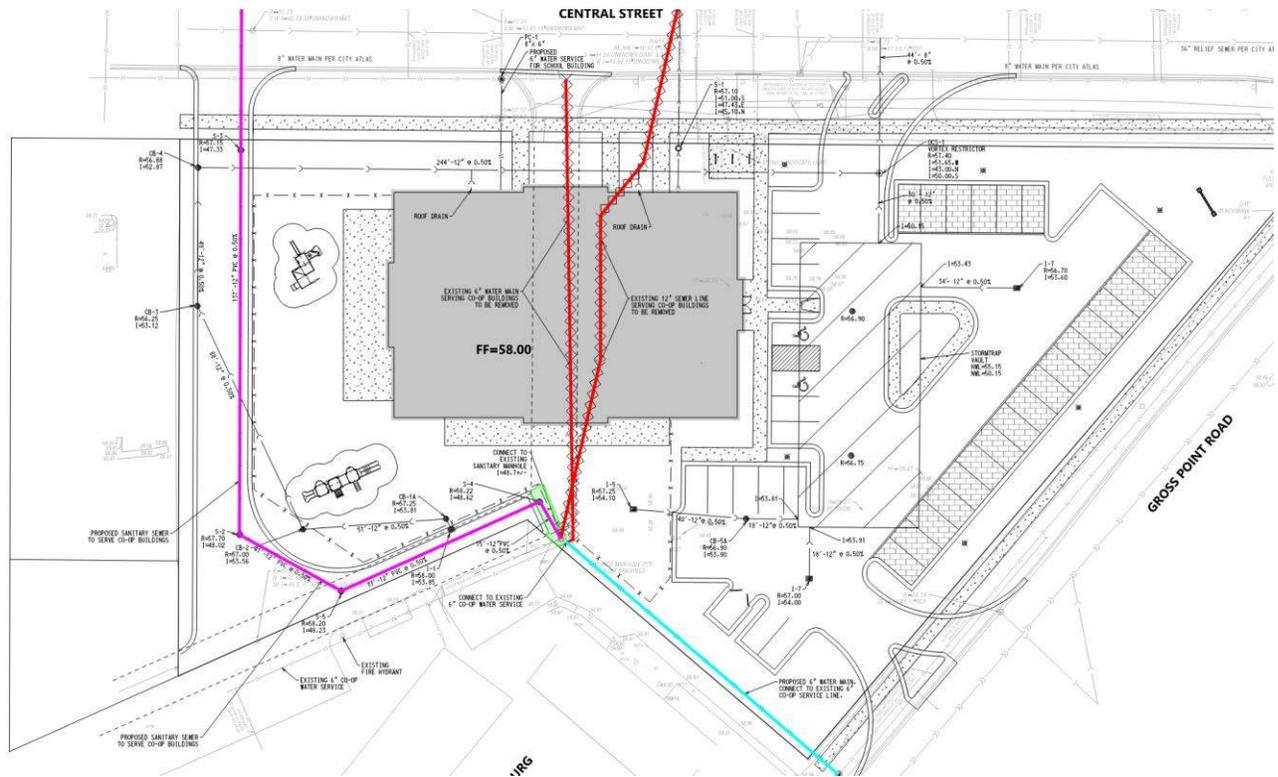
Staff notes that the existing drive aisle adjacent to these parking stalls varies in width from approximately 16' to approximately 23' and so the requested Site Development Allowance is similar to the non-compliant situation that exists on the site. Furthermore, the Cook County Circuit Court judgement found that the relocated access would not substantially alter the Co-Op's use of the easement. Staff is supportive of the requested Site Development Allowance.

Utilities

As noted previously, the existing domestic sanitary sewer line and water main that serves the Co-Op are located directly below where the proposed Kensington School building would be constructed, which means they must both be relocated. The petitioner has proposed the relocated sanitary sewer line to run within the Co-Op's access easement along the western side of the site, however, a small portion of the line would fall outside of where access is provided but still must be covered by an easement.

The water main would be rerouted to be adjacent to the southern lot line where it would lead out to Gross Point Road and connect to the existing water main within the right-of-way. The location of each improvement is shown in the map below, where Purple represents the new sanitary line, Blue represents the new water main, Red represents the existing water main and sanitary lines

that will be removed, and the shaded **Green** represents the small area where the sanitary line falls outside of the access area.



The Public Works Agency has concern that the new water main will be difficult to maintain/service if a leak occurs given the lack of pavement in the vicinity and the proposed playground fence. They have requested that the applicant work with the Co-Op to determine if an alternative configuration can be mutually agreed upon by both the Co-Op and the applicant where the main is abandoned but separate/private water service lines are constructed to each Co-Op building to connect them to the existing water main in Gross Point Road (which did not exist when the Co-Op was originally constructed). A condition of approval has been recommended to require this collaboration prior to permit issuance.

Additionally, a condition of approval is recommended to require that prior to permit issuance, the applicant execute and record a private utility easement on the subject property for the sanitary sewer line that serves the neighboring Co-Op. Finally, if the water main is relocated as depicted on the plans above, prior to building permit issuance the applicant shall execute and record a public utility easement for said water main, for final review and approval by the City.

Parking & Mobility

The development complies with parking requirements and proposes 38 onsite parking stalls where a minimum of 37 spaces are required. The number of parking stalls aligns with similar sized daycares in comparable suburban locations. For reference, the Goddard School daycare, located less than a mile southwest on Gross Point Road, is licensed for 188 children and provides 39 parking spaces. This translates to a 0.2 parking-space-per-child ratio. The proposed

Kensington School would provide 0.23 parking spaces per child, which slightly exceeds the ratio of parking at the Goddard School. Staff believes the subject property provides sufficient onsite parking to accommodate demand, however, a condition of approval is recommended to prohibit staff from parking within the surrounding residential streets.

The subject property has good access to the bike transportation network, with bike lanes located along Central approximately 750' east of the subject property, on Gross Point approximately 575' southwest of the subject property and approximately 650' northeast of the subject property. Six bike parking spaces will be provided at the northeast corner of the building near the main entrance, which are large enough to potentially accommodate cargo and trailers pulled behind bikes.

Landscaping

The site currently includes 37 trees (16 within the parkway and 21 onsite). All of the 21 existing onsite trees are within the development footprint and will be removed, and several of the parkway trees will be removed to accommodate the newly proposed access points/curb cuts. A total of 11 parkway trees will be preserved. The proposed landscape plan indicates 18 new trees will be planted (2 in the parkway and 16 onsite). In addition to these new trees, over 250 deciduous and evergreen shrubs will be planted across the site, including a hedgerow of Forsythia and Cotoneaster shrubs within the 5' landscape strip along the western property line to create a thick buffer between the subject property and single-family home abutting at the west. The remaining landscaping consists of perennials and ornamental grasses that will be dispersed within the landscaped areas of the site. Compliance with the City's Tree Preservation Ordinance shall be required.

Sustainability

The following sustainable features will be included within the development:

- The building is required to obtain LEED Gold certification, or comparable equivalent, and
- Compliance with the Illinois Stretch Energy Code will be required.
- Permeable pavers are proposed within a portion of the parking lot.
- Compliance with the Bird Friendly Ordinance will be required.
- The proposed parking lot light fixtures are Dark Sky compliant.

The applicant is also considering the following additional sustainable features:

- The provision of composting for organic refuse.
- The salvage and reuse of the existing building materials during deconstruction of the church building.
- The use of recycled and low embodied carbon building materials in construction of the proposed daycare building.

Construction Management Plan

The applicant shall be required to submit a Construction Management Plan (CMP) as part of their building permit submittal, which will establish specific parameters for management of the construction operation, to be reviewed and approved by the City. Of particular importance will be the preservation of access to the Co-Op throughout construction, and the CMP will need to

demonstrate how this will be achieved.

Traffic and Circulation

Kensington School requires parents/guardians to park their cars and enter the building to pick up children and does not provide drive-by service for drop-offs and pick-ups. Traffic will enter the site from either a southwest bound Gross Point Road right turn into the site, or a westbound Central Street left turn into the site. Left turn ingress from northeast bound Gross Point Road is prohibited and right turn ingress from eastbound Central Street is prohibited based on the geometry of the curb cut and the operational procedures of Kensington School, which is meant to reduce cut-through traffic through the residential neighborhoods to the west. Egress from the site can only be achieved via a right-out on Central Street or Gross Point Road.

The applicant hired Kenig, Lindgrin, O'hara & Abonna (KLOA), a professional traffic engineer, to prepare a Traffic Impact Study analyzing possible impacts of the proposed development on traffic in the area. The study was part of the original application for the Kensington School, and staff in the Engineering Bureau reviewed the study and determined the results were still valid.

The traffic study evaluated crash data over a five-year period (2016 to 2020) at nearby intersections. The review indicates there were 10 crashes at the intersection of Gross Point Road and Wellington Court over the five-year period but there were no fatal crashes at any of the intersections within the five-year period. The traffic study concludes that area intersections have sufficient reserve capacity to accommodate the projected traffic volumes generated by the proposed daycare and does not suggest roadway improvements and/or traffic control modifications are necessary.

Staff notes that the three intersections at Central/Gross Point, Central/Crawford and Gross Point/Crawford are in very close proximity with limited vehicle stacking area between them. The signals at these intersections are coordinated for optimal traffic flow, but the intersections still experience traffic backups during peak times. Eastbound Central Street at Gross Point has limited green time due to the low traffic volumes and to maintain proper traffic flow on the other major legs of these intersections.

The signal capacity analysis within the KLOA study indicates that the eastbound Central Street/Gross Point Road intersection is currently operating at a level of service "D" with 45-48 seconds of delay during the peak hours. Some of the other movements at the adjacent intersections also currently operate at level of service "D". The signal analysis indicates the Central Street/Gross Point Road intersection will continue to operate at a level of service "D" with 52 seconds of delay when factoring in the projected traffic from the proposed development. To address potential future traffic and circulation issues, the City is recommending two conditions of approval:

1. Should City staff observe traffic issues at the intersection of Central Street/Gross Point Road, the applicant shall work with the City to address such issues, which may include (but is not limited to) daycare staff stationed within the parking lot during peak drop-off/pick-up times to ensure that turning restrictions are adhered to, the prohibition on right turns out from the parking lot onto Central Street during the peak travel hours, and/or the

- prohibition of parents/guardians from parking on Central Street for drop-off/pick-up.
2. Should efforts to address traffic issues at the Central Street/Gross Point Road intersection prove unsuccessful, which shall be at the discretion of the City, the applicant shall hire an IDOT pre-qualified Signal Capacity and Timing (SCAT) traffic consultant to study the Central/Gross Point, Central/Crawford and Gross Point/Crawford intersections to see if timing adjustment can be made without impacting the traffic flow at the other major legs and implement the changes upon review and approval by the City.

Compliance with the Zoning Ordinance

The R2 Single-Family Residential District is intended to provide for small lot single-family development at a relatively low density and to preserve the present physical character of that area while providing for infill development. Both the current and proposed uses on the subject property are non-residential and both are allowable special uses in the R2 district. The proposed development does not significantly change the physical character of the area, and with exception to the six Site Development Allowances requested as part of this application, the proposed development conforms to all Zoning Ordinance regulations.

Compliance with the Design Guidelines for Planned Developments

The proposed building is consistent with the Design Guidelines for Planned Developments. The building is sited in a logical position on the property complying with required building setbacks and to minimize impacts onto adjacent residences. The parking is located away from adjacent residences as much as is feasible, and the playground area is located away from the adjacent roadways. The building massing is broken up by several vertical planes on all facades, window, fenestration, and other architectural details. The scale of the 2-story building is compatible with the surrounding area of mostly 2-story structures and 4-story multi-family residential buildings to the east.

Compliance with the Comprehensive Plan

The proposal is compatible with the Evanston Comprehensive General Plan. The guiding principle of the Plan is to encourage new development that improves the economy, convenience and attractiveness of Evanston while simultaneously working to maintain a high quality of life within the community where new developments should be integrated within existing neighborhoods to promote walking and the use of mass transit.

The site is within an identified corridor along Gross Point Road where opportunities for commercial and mixed-use redevelopment revitalization are probable, and sensitive transit-oriented development is encouraged.

The proposed development is consistent with the following Comprehensive Plan objectives:

- Preserve neighborhood character while supporting redevelopment efforts that add to neighborhood desirability. *The neighborhood character will not change significantly since the proposed development characteristics are similar to existing site conditions. The site is not currently occupied by a residential land use and the resulting development will be non-residential as well. The proposed daycare center fills a need for child daycare in proximity to where people live.*

- Minimize the adverse effects of traffic and parking congestion or incompatible hours of operation of businesses proposing expansion or relocation to sites adjacent to residential areas. *The site plan is designed with the parking and curb cut locations/configurations to minimize impacts onto adjacent residents. Staff recommends that after operations begin, if traffic added by the daycare center becomes an issue on Central Street, the applicant will be required to explore and implement a variety of strategies to address traffic issues.*
- Seek creative means to increase Evanston's property tax base to maintain the provision of quality services while relieving some of the tax burden placed on homeowners. *The subject property is currently tax-exempt. The proposed development returns the parcel back onto the tax roll.*

Public Benefits

Public benefits are intended to address impacts that a planned development has on the community. The applicant has proposed the following public benefits as part of their development package:

1. A new public sidewalk will be constructed along the south side of Central Street from the traffic intersection to the western property edge and on the west side of Gross Point Road from the traffic intersection southwest to the southern property edge.
2. A \$25,000 contribution to the City for new/repared public sidewalks in the area.
3. Creation of an ongoing scholarship program for reduced tuition for existing Kensington families who encounter economic hardships.
4. Reduced tuition for sustainable drop-off/pick-ups to discourage vehicle usage in the area (\$40 off per month when a vehicle is used less than 25% of the time).

The applicant is open to modifying the public benefits based on community feedback at the land Use Commission hearing. Prior to appearing before the Planning & Development Committee, the applicant should provide additional details on public benefit #3 to clarify whether the scholarship program will be just for their Evanston location, what percentage of reduced tuition is envisioned, how many families at a time would be eligible, what would qualify as an economic hardship, etc. Additionally, the applicant is encouraged to structure the financial incentives as percentages rather than defined amounts; a \$40 tuition reduction in 20 years may be insufficient to incentivize sustainable drop-off/pick-up.

Department Recommendation

This application has been reviewed by all development review staff within the City, including staff from the Building Division, Public Works Agency (traffic, utilities, forestry, waste management, and engineering), Fire Department, Health & Human Services Department, and City Manager's Office (sustainability, transportation & mobility). All review comments are included within the materials transmitted to the Land Use Commission, and with exception to any issues identified in this report, all reviewing comments have been adequately addressed by the applicant. The Community Development Department has evaluated the proposal relative to the Standards for Approval and recommends approval of the application subject to certain conditions as identified below.

The Land Use Commission should review the application to determine if the applicable standards are met. If the Commission determines the Standards for Approval are met, the Commission should recommend approval of the application to the City Council, and may include conditions for approval including but not limited to:

1. Prior to appearing before the Planning & Development Committee, the applicant shall provide additional details on the proposed scholarship program for reduced tuition for existing Kensington families who encounter economic hardships.
2. The applicant shall grant an access easement to the benefit of the property to the south/southwest (First Williamsburg Corporation PIN 10-10-200-058-0000) in substantial compliance with the Cook County Circuit Court Order from Case No. 2023-CH-06757, which shall provide access through the subject property from Central Street to the First Williamsburg Corporation parking areas. Said easement shall be recorded prior to building permit issuance.
3. The applicant shall grant a private utility easement to the benefit of the property to the south/southwest (First Williamsburg Corporation PIN 10-10-200-058-0000), which shall cover the portion of the sanitary service line on the subject property that serves the First Williamsburg Corporation property. Said easement shall be recorded prior to building permit issuance.
4. Prior to building permit issuance, the applicant shall collaborate with the First Williamsburg Corporation to determine if the relocated water main proposed along the southern side of the subject property can be eliminated, and water service for the First Williamsburg Co-Op can run directly from each Co-Op building to the existing water main in Gross Point Road. If this arrangement is mutually agreeable by both parties, and if approved by the City, the cost of such work shall be borne by the Applicant.
5. Should it be necessary to relocate the water main along the southern property line of the subject property, as depicted on the Utility Plan (dated 1-2-26), the applicant shall execute and record a public utility easement for the water main prior to building permit issuance, for final review and approval by the City.
6. IDOT review and approval shall be required for any work within the Gross Point Road right-of-way.
7. Staff shall be prohibited from parking within the surrounding residential streets.
8. Should City staff observe traffic issues at the intersection of Central Street/Gross Point Road, the applicant shall work with the City to address such issues, which may include (but is not limited to) daycare staff stationed within the parking lot during peak drop-off/pick-up times to ensure that turning restrictions are adhered to, the prohibition on right turns out from the parking lot onto Central Street during peak travel hours, and/or the prohibition of parents/guardians from parking on Central Street for drop-off/pick-up.
9. Should efforts to address traffic issues at the Central Street/Gross Point intersection prove unsuccessful, which shall be at the discretion of the City, the applicant shall hire an IDOT pre-qualified Signal Capacity and Timing (SCAT) traffic consultant to study the Central/Gross Point, Central/Crawford and Gross Point/Crawford intersections to see if timing adjustment can be made without impacting the traffic flow at the other major legs and implement the changes upon review and approval by the City.

10. The applicant shall allow access for the maintenance of trees located on the property immediate abutting to the west, which trees are adjacent to/straddle the western property line of the subject property.
11. Prior to building permit issuance for any site demolition or new building construction on the subject property, the applicant shall sign and agree to a Construction Management Plan (CMP) with the City of Evanston. The CMP should include but is not limited to the following: construction staging plan, on-street and on-site construction parking restrictions, hours of operation, details on how access to the Co-Op will be maintained throughout construction, foundation survey of surrounding structures including weekly reporting of seismographs for the duration of construction (as necessary), submittal of environmental testing report prior to construction, proposed schedule for street openings for utility connections, and project updates via monthly newsletter and/or project website;
12. Substantial compliance with the application documents and public hearing testimony on record shall be required.
13. Recordation: Pursuant to Subsection 6-3-6-10 of the Zoning Ordinance, the Applicant shall, at its cost, record a certified copy of this ordinance, including all exhibits attached hereto, with the Cook County Recorder of Deeds, and provide proof of such recordation to the City, before the City may issue any permits pursuant to the Planned Development authorized by the terms of this ordinance.

Standards for Approval

Prior to making a recommendation of approval, approval with conditions, or denial of any Special Use for a Planned Development and Child Daycare Center, the LUC must review the request against the following standards: 1) Standards for Special Uses (Section 6-3-5-10); 2) Standards for Planned Developments (Section 6-3-6-9), and; 3) General Conditions, Site Controls, and Standards for Planned Developments in the Residential zoning districts (Section 6-8-1-10).

For the Land Use Commission to recommend that the City Council grant a Special Use, the Land Use Commission must find that the proposed Special Use:

1. Is one of the special uses specifically listed in the zoning ordinance. *Child Daycare Centers and Planned Developments are listed as allowable special uses in the R2 district;*
2. Is in keeping with purposes and policies of the adopted comprehensive general plan and the zoning ordinance as amended from time to time. *As noted above, the development is compatible with the Evanston Comprehensive General Plan;*
3. Will not cause a negative cumulative effect, when its effect is considered in conjunction with the cumulative effect of various special uses of all types on the immediate neighborhood and the effect of the proposed type of special use upon the City as a whole. *In consideration to the conditions of approval related to the resolution of any future traffic issues, the proposal will not cause a negative cumulative effect on the immediate neighborhood or the City as a whole;*
4. Does not interfere with or diminish the value of property in the neighborhood. *The site has been well designed to minimize impacts on neighboring properties and will not interfere or diminish the value of properties in the neighborhood. In several instances, the development will enhance existing infrastructure, including the reconstruction of public*

sidewalks abutting the property and the contribution of \$25,000 for additional sidewalk improvements in the vicinity;

5. *Can be adequately served by public facilities and services. Adequate public facilities and services exist to serve the proposed development and no large-scale upgrades are needed to accommodate for the development;*
6. *Does not cause undue traffic congestion. The applicant has submitted a traffic study indicating there will be minimal effect on the level of service at existing surrounding roadways. Additionally, the conditions of approval related to the resolution of future traffic issues will ensure that the proposal will not cause undue traffic congestion;*
7. *Preserves significant historical and architectural resources. There are no significant historical or architectural resources on the subject property;*
8. *Preserves significant natural and environmental features. There are no significant natural or environmental features on the subject property; and*
9. *Complies with all other applicable regulations of the district in which it is located and other applicable ordinances, except to the extent such regulations have been modified through the planned development process or the grant of a variation. The applicant is required to comply with all applicable ordinances except to the extent such regulations have been modified through the Planned Development process.*

For the Land Use Commission to recommend that the City Council grant a Special Use for the Planned Development with the requested site development allowances, the Land Use Commission must find:

1. *The requested Site Development Allowance(s) will not have a substantial adverse impact on the use, enjoyment or property values of adjoining properties that is beyond a reasonable expectation given the scope of the applicable Site Development Allowance(s) of the Planned Development location. The development has been carefully designed to minimize impacts on neighboring properties with the parking areas sited along Gross Point Road and Central Street so as to not directly abut any residential property. The building and play areas are centralized within the site to prevent substantial adverse impacts from the proposed use on enjoyment or property values of adjoining properties that is beyond a reasonable expectation. The building utilizes high quality masonry materials and is of a scale that is compatible with surrounding structures.*
2. *The proposed development is compatible with the overall character of existing development in the immediate vicinity of the subject property. The subject property is located along Gross Point Road (an arterial street), which consists of a variety of uses, including multi-family, short term residential rehabilitation, commercial, and office uses.*
3. *The development site circulation is designed in a safe and logical manner to mitigate potential hazards for pedestrians and vehicles at the site and in the immediate surrounding area. The site has been designed with restricted access points to neighboring streets to mitigate potential hazards. Conditions of approval related to the resolution of possible future traffic issues will ensure that the proposal address potential hazards should they arise.*
4. *The proposed development aligns with the current and future climate and sustainability goals of the City. The site will feature permeable pavers and the proposed parking lot light fixtures will be Dark Sky Compliant.*

5. Public benefits that are appropriate to the surrounding neighborhood and the City as a whole will be derived from the approval of the requested site development allowance(s). *The applicant will reconstruct the sidewalks abutting the site, extend the sidewalk along the south side of Central Street to the western edge of the site, and contribute \$25,000 for sidewalk improvements in the vicinity of the subject property.*

For the Land Use Commission to recommend that the City Council grant a Special Use for the Planned Development with the requested Site Development Allowances in the R2 Single-Family Residential District, the Land Use Commission must find:

A. General Conditions

1. Each planned development shall be compatible with surrounding development and not be of such a nature in height, bulk, or scale as to exercise any influence contrary to the purpose and intent of the Zoning Ordinance as set forth in Section 6-1-2, "Purpose and Intent."

If the proposed planned development is for a property listed as an Evanston landmark, or for property located within a historic district listed on the National Register of Historic Places or for property located within a historic district so designated by the Evanston Preservation Commission, the planned development shall be compatible with the "Secretary of the Interior's Standards for Rehabilitation" as set forth in the National Historic Preservation Act of 1966, as amended.

2. Each planned development shall enhance the identity and character of the downtown, by preserving where possible character-giving buildings, enhancing existing streetscape amenities, maintaining retail continuity in areas where it is prominent, strengthening pedestrian orientation and scale, and contributing to the mixed-use vitality of the area.
3. Each planned development shall be compatible with and implement the adopted Comprehensive General Plan, as amended, any adopted land use or urban design plan specific to the area, this Zoning Ordinance, and any other pertinent City planning and development policies, particularly in terms of: (a) Land use (b) Land use intensity (c) Housing (d) Preservation (e) Environmental (f) Traffic impact and parking (g) Impact on schools, public services and facilities (h) Essential character of the downtown district, the surrounding residential neighborhoods, and abutting residential lots (i) Neighborhood planning (j) Conservation of the taxable value of land and buildings throughout the City, and retention of taxable land on tax rolls.
4. All landscaping treatment within the planned development shall be provided in accordance with the requirements set forth in Chapter 17, "Landscaping and Screening," and shown on the required landscape plan that shall be submitted as part of the planned development application.

B. Site Controls and Standards. The following site controls and standards are established to provide a regulatory framework that will promote excellence in site design. Their establishment is not intended to restrict or inhibit the Design and Project Review Committee or the applicant from applying other site design principles and standards that may be applicable to the planned development being proposed and that may be found in or interpolated from the Plan for Downtown Evanston, and the City's Manual of Design Guidelines or in common use by design professionals.

1. For each planned development there shall be submitted a tree preservation statement evaluating each building site as to whether desirable tree stands or other natural features exist and can be preserved. The preservation statement shall be made part of the required landscape plan submitted as part of the planned development application.
2. For all boundaries of the planned development not immediately abutting dedicated and improved public streets, there shall be provided a transition landscaped strip of at least ten (10) feet consisting of vegetative screening, fencing, or decorative walls in accordance with the Manual of Design Guidelines and Chapter 17, "Landscape and Screening." Natural features or tree stands identified as desirable in the tree preservation statement shall be incorporated in the transition landscaped strip where possible. The transition landscaped strip and its treatment shall be depicted on the required landscape plan submitted as part of the planned development application.
3. Walkways developed for a planned development shall form a logical, safe and convenient system for pedestrian access to all dwelling units, all project facilities, as well as any off-site destination likely to attract substantial pedestrian traffic. Walkways, when used by substantial numbers of children as play areas, routes to school or other principal destinations, shall be so located and safeguarded as to minimize contacts with normal automobile traffic. Street crossings shall be located, designed, and marked to promote the utmost safety. If substantial bicycle traffic is anticipated, bicycle paths shall be incorporated into the walkway system. Pedestrian ways shall not be used by other automotive traffic.
4. The location, construction, and operation of parking, loading areas, and service areas shall be designed to avoid adverse effects on residential uses within or adjoining the development.
5. Principal vehicular access points shall be designed to permit smooth traffic flow with controlled turning movements and minimum hazards to vehicular and pedestrian traffic.
6. The planned development shall provide, if possible, for underground installation of utilities (including electricity and telephone) both in public ways and private extensions thereof. Provisions shall be made for acceptable design and

construction of storm water facilities including grading, gutter, piping, and treatment of turf and maintenance of facilities.

7. For every planned development there shall be provided a market feasibility statement that shall indicate the consumer market areas for all uses proposed in the development, the population potential of the area or areas to be served by the uses proposed and other pertinent information concerning the need or demand for such uses of land.
8. For every planned development involving twenty (20) or more dwelling units there shall be provided a traffic circulation impact study that shall show the effect of all proposed uses upon adjacent and nearby roads and highways. The study also shall show the amount and direction of all anticipated traffic flow and clearly describe what road improvements and traffic control improvements might become necessary as a result of the construction of the proposed development.

Action by the Commission

After making findings of fact as to whether or not the requested Special Use for a Planned Development with Site Development Allowances (zoning relief) meets or does not meet the aforementioned standards, the Land Use Commission may make a recommendation to the Planning & Development Committee of the City Council to approve, approve with conditions, or deny the zoning relief as requested.

The Land Use Commission is the recommending body and the City Council is the determining body.

Attachments

1. Aerial & Perspective Images
2. Zoning Map
3. Streetview Image
4. Zoning Applications
5. Purchase Contract and Contract Extensions
6. Project Narrative
7. Applicant Response to PD Approval Standards
8. Neighborhood Outreach Documents
9. Preliminary Development Schedule
10. Zoning Analysis – 2023
11. Co-Op Access Easement Legal Description
12. Court Order Summary Judgement – Easement Relocation
13. Plat of Survey
14. Architectural Plans
15. Engineering Plans & Preliminary Stormwater Report
16. Landscape & Tree Preservation Plans
17. Photometric Plan & Details
18. Traffic Study
19. IDOT Review Letter

- 20. Market Feasibility Statement
- 21. City Staff Review Comments & Applicant Responses
- 22. Public Comments
- 23. Public Notification Documents

1. Aerial & Perspective Images



Central St

Wellington Ct

ton Ct

First Williamsburg Co-Op

Subject Property

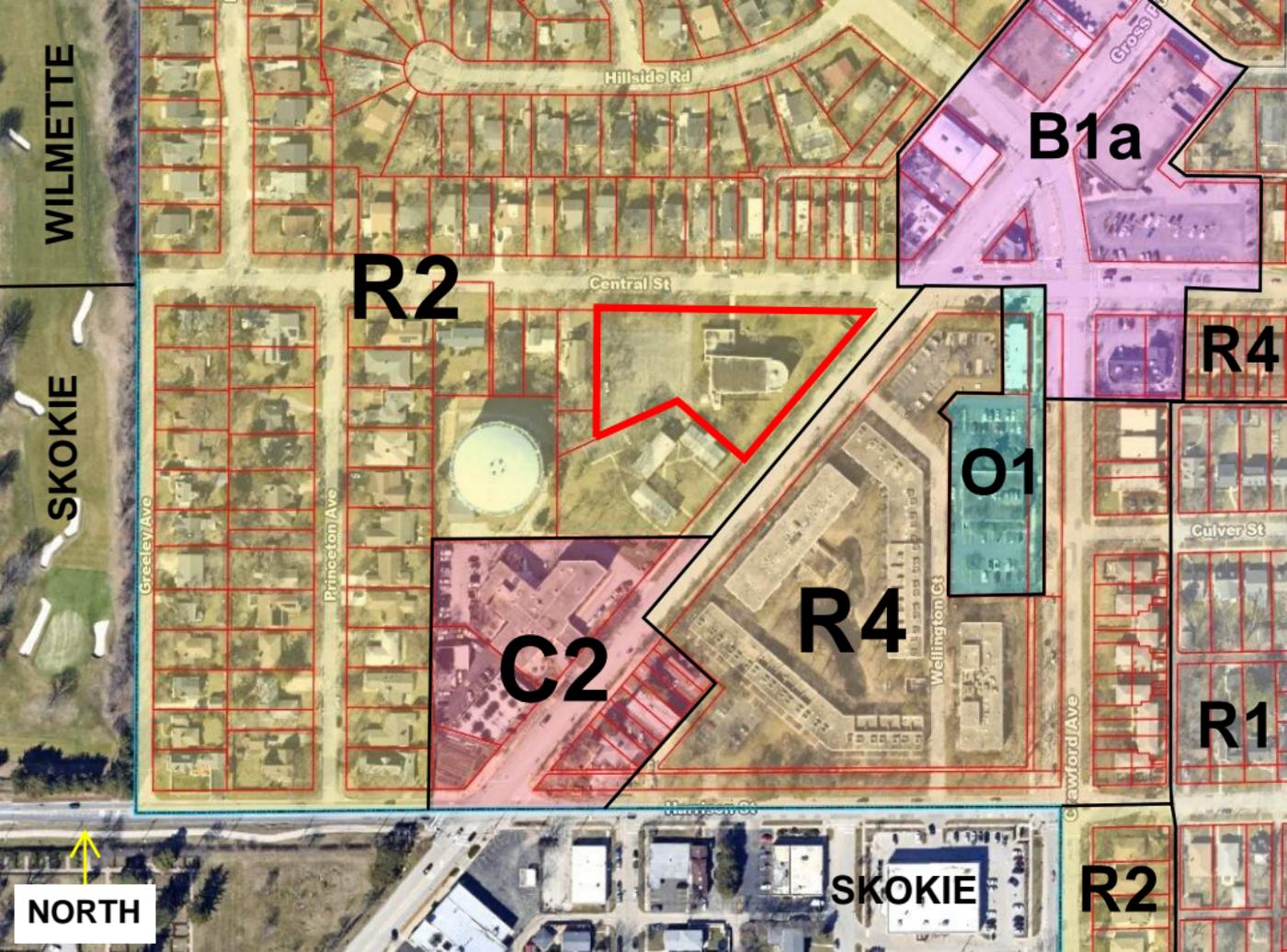
Co-Op Easement

NORTH



NORTH

2. Zoning Map



WILMETTE

SKOKIE

Greeley Ave

Princeton Ave

Hillside Rd

Central St

Wellington Ct

Crawford Ave

Culver St

R2

B1a

R4

O1

R4

C2

R1

R2

SKOKIE

NORTH



3. Streetview Image



Gross Point

Central St



30

4. Zoning Applications



PLANNED DEVELOPMENT APPLICATION



Case Number: _____

1. PROPERTY

Address(es)/Location(s)

Brief Narrative Summary of Proposal:

Multiple horizontal lines for writing the narrative summary.

2. APPLICANT

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: Work: _____ Home: _____ Cell/Other: _____

Fax: Work: _____ Home: _____

E-mail: _____

Please circle the primary means of contact.

What is the relationship of the applicant to the property owner?

- same
- builder/contractor
- potential purchaser
- potential lessee
- architect
- attorney
- lessee
- real estate agent
- officer of board of directors
- other: _____

3. SIGNATURE

"I certify that all of the above information and all statements, information and exhibits that I am submitting in conjunction with this application are true and accurate to the best of my knowledge."

Applicant Signature – **REQUIRED**

Date

4. PRE-SUBMISSION REQUIREMENTS

Prior to actually submitting an application for Planned Development, you must:

A. Complete a Zoning Analysis of the Development Plan

The Zoning Office staff must review the development plan and publish a written determination of the plan's level of compliance with the zoning district regulations. Apply at the Zoning Office.

B. Present the planned development at a pre-application conference

Contact the Zoning Office to schedule a conference with Planning & Zoning Division staff.

5. REQUIRED SUBMISSION DOCUMENTS AND MATERIALS

- (This) Completed Application Form**
- Application Fee (\$6,000)**
- Two (2) Copies of Application Binder**

Your application must be in the form of a binder with removable pages for copying. You must submit two application binders for initial review.

The Application Binder must include:

- Certificate of Disclosure of Ownership Interest Form
- Plan drawing illustrating development boundary and individual parcels and PINs
- Plat of Survey of Entire Development Site
- Zoning Analysis Results Sheet
- Preliminary Plat of Subdivision
- Pre-application Conference Materials
- Development Plan
- Landscape Plan
- Inclusionary Housing Ordinance Application
- Statement addressing how the planned development approval will further public benefits
- Statement describing the relationship with the Comprehensive Plan and other City land use plans
- Statement describing the development's compliance with any other pertinent city planning and development policies
- Statement addressing the site controls and standards for planned developments
- Statement of proposed development's compatibility with the surrounding neighborhood
- Statement of the proposed development's compatibility with the design guidelines for planned developments
- Statements describing provisions for care and maintenance of open space and recreational facilities and proposed articles of incorporation and bylaws
- Restrictive Covenants
- Schedule of Development
- Market Feasibility Statement
- Traffic Circulation Impact Study
- Statement addressing development allowances for planned developments

Notes:

- **Plats of survey** must be drawn to scale and must accurately and completely reflect the current conditions of the property.
- **Building plans** must be drawn to scale and must include interior floor plans and exterior elevations.
- **Application Fees** may be paid by cash, check, or credit card.
- **Mailing Fees** also apply and will be provided to the applicant from the City's mailing vendor.

- **Civic Engagement Website** will be set up for the duration of the planned development review process at applicant expense.

6. OTHER PROFESSIONAL REPRESENTATIVE INFORMATION

Attorney

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Architect

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Surveyor

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Civil Engineer

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Traffic Engineer

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Other Consultant

Name: _____ Organization: _____

Address: _____ City, State, Zip: _____

Phone: _____ Fax: _____ Email: _____

Application Procedure

- (A) Pre-Application Conference: Pre-Application Conference: Prior to application submittal, an applicant shall meet with the Zoning Administrator, Planning & Zoning staff, and the Alderman of the ward in which the proposed planned development is located. Where applicable, a representative of the Preservation Commission shall be present. The purpose of the conference is to present the concept of the proposed plan and discuss procedures and standards for approval. No representation made by City staff or the representative of the Preservation Commission during such conference or at any other time shall be binding upon the City with respect to the application subsequently submitted. The pre-application conference shall be scheduled within 15 calendar days after receiving the applicant's request.
- (B) Information Needed for Pre-Application Conference: The applicant shall include the following information at the time of request for the meeting:
1. Narrative summary of proposal.
 2. Conceptual site plan.
 3. Plat of survey (including the location of utilities).
 4. Proposed elevations.
 5. Photographs of the subject and surrounding properties.
 6. Description of adjacent land uses and neighborhood characteristics.
 7. Description of critical historical structures, details or characteristics (if applicable).
- (C) Zoning Analysis Application: Prior to review of the project, the applicant must submit the project for "zoning analysis." The Zoning Division's response to this application is a Zoning Analysis Review Sheet addressing specific regulatory areas, and indicating compliance or deficiency.
- (D) Results of Pre-Application Conference and Zoning Analysis: Following the pre-application conference, the Zoning Administrator shall be available to suggest modifications to the site plan as discussed during the pre-application conference. Within 7 calendar days, minutes of the pre-application conference shall be sent to the applicant and shall be available upon request to interested parties by the Zoning Administrator.

Review Procedure - Decision

- (A) Review Procedure: Upon the review of an application for a planned development the Zoning Administrator shall notify the developer of any deficiencies or modifications necessary to perfect the planned development application.
- (B) Public Hearing: After determining that the application is complete, the Zoning Administrator shall schedule a public hearing to be held by the Plan Commission at which time a formal presentation of the planned development application will be presented. The public hearing shall be held not less than 15 calendar days and no more than 30 calendar days from the date of receipt of the complete application. In addition, a sign shall be posted on the property for a minimum of 10 working days prior to the public hearing indicating the place, time and date of the hearing.
- (C) Mailed Notices Required: Notice shall also be given by first class mail to all owners of property within a 1,000-foot radius of the subject property as provided by the City. The failure of delivery of such notice, however, shall not invalidate any such hearing. The City, through its Geographic Information System, will supply the names and addresses of the owners of property within the 1,000-foot radius. A third party mailing service mails notice of the hearing to the neighboring property owners. The applicant must pay any and all fees and postage associated with mailing this notice. The City publishes a notice of the hearing in a locally circulating newspaper, generally the Evanston Review, no less than 15 days nor more than 30 days prior to hearing.
- (D) Recommendation: The Plan Commission holds a public hearing and makes a recommendation, based on findings of fact, to the City Council within 60 days of the close of the public hearing. The Planning and Development Committee of the City Council considers the Plan Commission's recommendation and forwards it to the full Council with or without a recommendation. The City Council considers the Plan Commission's recommendation and may introduce an ordinance granting the planned development. The City Council may adopt an ordinance granting the planned development at the following or any subsequent City Council meeting. The developer shall record the ordinance granting the planned development and the development plan with the Cook County Recorder.

Submittal Requirements

- 1) Planned Development Application Form.
- 2) Certificate of Disclosure of Ownership Interest Form listing each individual lot contained within the proposed development identified by parcel identification number and each owner having legal or equitable interest in each

individual parcel. Connection to the ownership interest in the property must be documented in the form of a title insurance policy, deed, lease or contract to lease or purchase.

- 3) Exhibit illustrating the boundaries of each individual parcel contained within the property(ies) proposed for development with coincide parcel identification numbers.
- 4) Plat of Survey, drawn to scale, showing dimensions and areas of the parcel(s), lot(s), block(s), or portions thereof, according to the recorded plat of the subject property(ies).
- 5) Preliminary Plat of Subdivision, if necessary, showing the development consists of, and is coterminous with, a single lot legally described in a recorded plat of subdivision or proposed subdivision or consolidation.
- 6) Pre-application Conference Materials:
 - a) Conceptual site plan, showing parking and bicycle facilities where appropriate;
 - b) Plat of survey showing location of utilities;
 - c) Elevations;
 - d) Photographs of the subject and surrounding properties;
 - e) Description of adjacent land uses and neighborhood characteristics; and
 - f) Description of critical historical structures, details or characteristics.
- 7) Zoning Analysis Results Sheet, if available.
- 8) Development Plan showing:
 - a) Location, dimensions and total area of site;
 - b) Location, dimensions, floor area, construction type and use of each structure;
 - c) Number, type and size of dwelling units, and the overall dwelling unit density;
 - d) Number and location of parking spaces and loading docks, with means of ingress and egress;
 - e) Traffic circulation pattern, location and description of public improvements, streets and access easements to be installed or created;
 - f) All existing and proposed dedications and easements;
 - g) Drainage plan;
 - h) Locations, dimensions and uses of adjacent properties, rights of way, easements and utilities serving the site;
 - i) Significant topographical or physical features, including trees;
 - j) Soil or subsurface conditions; and
 - k) Historical structures or features.
- 9) Landscape Plan, including:
 - a) Location, dimensions and total area of site;
 - b) Locations, dimensions and uses of adjacent properties, rights of way, easements and utilities serving the site;
 - c) Landscaping location and treatment, plant material types, size and quantity, open spaces, and exterior surfaces of all structures with sketches of proposed landscaping;
 - d) Topographic and physical site features including soils and existing trees and vegetation;
 - e) Location, type and size of trees to be removed, and preservation plan for existing trees to remain through construction; and
 - f) Location, type, height and material of all fences and walls.
- 10) Inclusionary Housing Ordinance Application.
- 11) Statement addressing how the planned development approval will further public benefits including:
 - a) Preservation and enhancement of desirable site characteristics, open space, topographic and geologic features, and historic and natural resources;
 - b) Use of design, landscape, and architectural features to create a pleasing environment;
 - c) Provide a variety of housing types in accordance with the City's housing goals;
 - d) Eliminate blighted structures or incompatible uses through redevelopment or rehabilitation;
 - e) Business, commercial, and manufacturing development to enhance the local economy and strengthen the tax base;
 - f) Efficiently use land resulting in more economic networks of utilities, streets, schools, public grounds, and other facilities; and
 - g) Incorporate recognized sustainable design practices and building materials to promote energy conservation and improve environmental quality.
 - h) Additional benefits related to transit alternatives, public art, public space improvements, etc.

- 12) Statement describing the relationship between the proposed development and the Comprehensive General Plan and other City land use plans.
- 13) Statement describing the developments compliance with the Zoning Ordinance and any other pertinent city planning and development policies.
- 14) Statement addressing the site controls and standards for planned developments in the subject property's zoning district regarding the following:
 - a) Minimum area
 - b) Tree preservation
 - c) Landscaped strip
 - d) Open space
 - e) Walkways
 - f) Parking and loading
 - g) Utilities
 - h) Stormwater treatment
- 15) Statement of proposed developments compatibility with the surrounding neighborhood.
- 16) Statement of proposed developments compatibility with the design guidelines for planned developments.
- 17) Statement describing provisions for care and maintenance of open space and recreational facilities and, if owned by an entity other than a government authority, proposed articles of incorporation and bylaws.
- 18) Restrictive Covenants to be recorded against proposed development.
- 19) Schedule of Development phases or stages stating beginning and completion time for each phase.
- 20) Market Feasibility Statement indicating the consumer market areas for all proposed uses in the development, the population potential of the area to be served by the proposed uses and other pertinent information concerning the demand for such uses of land.
- 21) Traffic Circulation Impact Study showing the effect of the development upon adjacent roadways, anticipated vehicular trips and traffic flow, and what road improvements and traffic control upgrading might be necessary.
- 22) Statement addressing the development allowances for planned developments in the subject property's zoning district regarding the following:
 - a) Height increases
 - b) Density increases
 - c) The location and placement of buildings varying from that otherwise permitted in the district
 - d) Floor area ratio increases

Standing

The applicant must own, lease, or have legal or equitable interest in the subject property. The Planning and Zoning Division requires the applicant to demonstrate his or her connection to the ownership interest in the property. Documentation can be in the form of a title insurance policy or a deed, and a lease or contract to lease or purchase.

5. REQUIRED DOCUMENTS AND MATERIALS

The following are required to be submitted with this application:

- (This) Completed and Signed Application Form**
- Plat of Survey** Date of Survey: _____
- Project Site Plan** Date of Drawings: _____
- Plan or Graphic Drawings of Proposal** (If needed, see notes)
- Non-Compliant Zoning Analysis**
- Proof of Ownership** Document Submitted: _____
- Application Fee** Amount \$ _____

Notes: Incomplete applications will not be accepted. Although some of these materials may be on file with another City application, individual City applications must be complete with their own required documents.

Plat of Survey

(1) One copy of plat of survey, drawn to scale, that accurately reflects current conditions.

Site Plan

(1) One copy of site plan or floor plans, drawn to scale, showing all dimensions.

Plan or Graphic Drawings of Proposal

A Special Use application requires graphic representations for any elevated proposal-- garages, home additions, roofed porches, etc. Applications for a/c units, driveways, concrete walks do not need graphic drawings; their proposed locations on the submitted site plan will suffice.

Proof of Ownership

Accepted documents for Proof of Ownership include: a deed, mortgage, contract to purchase, closing documents (price may be blacked out on submitted documents).

- **Tax bill will not be accepted as Proof of Ownership.**

Non-Compliant Zoning Analysis

This document informed you that the proposed change of use is non-compliant with the Zoning Code and requires a variance.

Application Fee

The application fee depends on your zoning district (see zoning fees). Acceptable forms of payment are: Cash, Check, or Credit Card.

6. PROPOSED PROJECT

A. Briefly describe the proposed Special Use:

KENSINGTON SCHOOL IS PROPOSING TO BUILD A NEW CHILDCARE CENTER

APPLICANT QUESTIONS

- a) Is the requested special use one of the special uses specifically listed in the Zoning Ordinance? What section of the Zoning Ordinance lists your proposed use as an allowed special use in the zoning district in which the subject property lies? (See Zoning Analysis Review Sheet)

YES. THE PROPERTY IS CURRENTLY ZONED R2 AND CHILD CARE IS A SPECIAL USE

- b) Will the requested special use interfere with or diminish the value of property in the neighborhood? Will it cause a negative cumulative effect on the neighborhood?

NO. THE CURRENT USE IS A CHURCH THAT IS LOOKING TO MOVE. THE NEW CONSTRUCTION OF A DAY CARE CENTER WILL INCREASE THE VALUE OF THE SURROUNDING AREA. THE CUMULATIVE EFFECT WILL BE POSITIVE AS THERE ARE NOT ENOUGH CHILD CARE CENTERS TO SERVE THE FAMILIES OF EVANSTON.

- c) Will the requested special use be adequately served by public facilities and services?

YES. ALL UTILITIES ARE CURRENTLY IN PLACE FOR THIS NEW SCHOOL

d) Will the requested special use cause undue traffic congestion?

NO. A NEW FULL ACCESS DRIVE IS PROPOSED ON GROSS POINT ROAD THAT WILL SERVE THE SCHOOL AND THE ADJACENT RESIDENTIAL COOPERATIVE. AN ADDITIONAL RIGHT TURN OUT ONLY ON CENTRAL STREET WILL FURTHER ALLEVIATE THOSE THAT WISH TO UTILIZE THE SIGNALLED INTERSECTION FOR ACCESS.

e) Will the requested special use preserve significant historical and architectural resources?

THERE ARE NO SIGNIFICANT HISTORICAL OR ARCHITECTURAL RESOURCES ON THIS PROPERTY

f) Will the requested special use preserve significant natural and environmental features?

THERE ARE NO SIGNIFICANT NATURAL OR ENVIRONMENTAL FEATURES ON THIS PROPERTY

g) Will the requested special use comply with all other applicable regulations of the district in which it is located and other applicable ordinances, except to the extent such regulations have been modified through the planned development process or the grant of a variation?

YES, THERE WILL BE SOME MINOR ZONING ADJUSTMENTS THAT WILL NEED TO BE CONSIDERED



City of Evanston DISCLOSURE STATEMENT

(This form is required for all Major Variances and Special Use Applications)

The Evanston City Code, Title 1, Chapter 18, requires any persons or entities who request the City Council to grant zoning amendments, variations, or special uses, including planned developments, to make the following disclosures of information. The applicant is responsible for keeping the disclosure information current until the City Council has taken action on the application. For all hearings, this information is used to avoid conflicts of interest on the part of decision-makers.

1. If applicant is an agent or designee, list the name, address, phone, fax, and any other contact information of the proposed user of the land for which this application for zoning relief is made:

Does not apply.

2. *If a person or organization owns or controls the proposed land user*, list the name, address, phone, fax, and any other contact information of person or entity having constructive control of the proposed land user. Same as number 1 above, or indicated below. (An example of this situation is if the land user is a division or subsidiary of another person or organization.)

3. List the name, address, phone, fax, and any other contact information of person or entity holding title to the subject property. Same as number 1 above, or indicated below.

4. List the name, address, phone, fax, and any other contact information of person or entity having constructive control of the subject property. Same as number 1 above, or indicated below.

If Applicant or Proposed Land User is a Corporation

Any corporation required by law to file a statement with any other governmental agency providing substantially the information required below may submit a copy of this statement in lieu of completing a and b below.

a. Names and addresses of all officers and directors.

CHARLES MARLAS 743 MCCLINTOCK DRIVE, BURR RIDGE, IL 60527

b. Names, addresses, and percentage of interest of all shareholders. If there are fewer than 33 shareholders, or shareholders holding 3% or more of the ownership interest in the corporation or if there are more than 33 shareholders.

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If Applicant or Proposed Land User is not a Corporation

Name, address, percentage of interest, and relationship to applicant, of each partner, associate, person holding a beneficial interest, or other person having an interest in the entity applying, or in whose interest one is applying, for the zoning relief.



Special Use Information

A. GENERAL INFORMATION

1. What projects are eligible for a Special Use Permit?

Projects are eligible per zoning District. Please check the Zoning District to see if your proposed project is listed as a permitted Special Use per zoning District. The Allowed Uses by Zoning District handout is also another way to access information to see if your project is eligible to apply.

2. Who can submit an application?

The applicant must either own, lease, or have legal or equitable interest in the subject property, or must be the representative of such a person. All persons or parties which have an ownership interest in the affected properties must be identified and must sign the application. The Property Owner(s) may, at their discretion, designate another person as Applicant to act on their behalf in processing this application. In that case, the designated Applicant will be considered the primary contact, until the application is closed or the Property Owner changes the designated Applicant by contacting the Zoning Office in writing. **Standing** (§6-3-8-4):

3. How do I submit an application?

Applications must be submitted in person to the Zoning Office, City of Evanston, Civic Center Room 3700, 2100 Ridge Avenue. Our office hours are Monday through Friday (excluding Holidays) from 8:30 am until 5:00 pm. Evanston.

Applications must be complete, including all required documentation and fee.
Applications are not accepted by mail or e-mail.
Application materials cannot be returned.

4. What forms of payment are accepted?

Cash, Credit Card, Check.

5. Can I withdraw my application?

Yes, an application may be withdrawn any time prior to a vote.

6. Who has access to my application materials?

The application is a public document, and as such, may be reviewed by the general public upon request.

B. INFORMATION ABOUT SPECIAL USES

What is a Special Use Permit?

For each zoning district, the Zoning Ordinance identifies **permitted uses** (also called “by right” uses) and **special uses** which may be allowed depending upon the circumstances. In order to legally operate a special use, a property owner must apply for a Special Use Permit from the Zoning Office. The application is reviewed at a public hearing by the Zoning Board of Appeals (ZBA), which makes a recommendation to the City Council. The ZBA can also recommend conditions on a granted special use. The City Council is the deciding body for all Special Uses in the City of Evanston.

The Special Use Application Process

- The City reviews the project through a Zoning Analysis (applied for separately) and determines it is eligible to apply for a special use
- The Applicant files a Special Use Application
- The City publishes a notice of the hearing in the Evanston Review, between 15 and 30 days prior to hearing.
- The City posts a sign describing the public hearing on the property no less than 10 working days before the hearing.
- The City must mail notification of the public hearing to all properties that are within 500 feet of any point on the subject property. (The applicant is responsible for the accuracy of the list used by the City for mailing this notice. The applicant can either rely on a list the City produces through its Geographic Information System or produce his or her own list of the names and addresses of property owners within 500 feet of the subject property. The Zoning Office will send to the applicant its generated mailing list. The applicant should inform the Zoning Office if any names and addresses are missing.
- The City encourages all applicants to discuss their proposal with their neighbors prior to the public hearing.
- The Zoning Division will schedule the applicant to meet with the Site Plan & Appearance Review Committee; (SPAARC) which provides a recommendation to the Zoning Board of Appeals.
- The ZBA recommends denial, approval, or approval with conditions of the application to City Council;
- The Planning and Development Committee of the City Council considers the ZBA recommendation and forwards it to the full City Council with or without a recommendation;
- City Council considers the ZBA recommendation and may introduce an ordinance granting the requested zoning relief;
- City Council may adopt an ordinance granting the requested zoning relief at the following or any subsequent City Council meeting.

The approximate time from when the Zoning Office receives a complete application to a decision is **three to four months**.

To recommend approval for a special use, the ZBA must find that the proposed special use meets all of the following criteria:

- a) is one of the listed special uses for the zoning district in which the property lies;
- b) complies with the purposes and policies of the Comprehensive General Plan and the Zoning Ordinance;
- c) does not cause a negative cumulative effect in combination with existing special uses or as a category of land use;
- d) does not interfere with or diminish the value of property in the neighborhood;
- e) is adequately served by public facilities and services;
- f) does not cause undue traffic congestion;
- g) preserves significant historical and architectural resources;
- h) preserves significant natural and environmental resources; and
- i) complies with all other applicable regulations.

Expiration

Within one year of obtaining a special use permit, the recipient must either obtain a building permit and commence construction, or obtain a certificate of occupancy and commence the use. City Council may extend this one-year limitation upon request.

CONTACT INFORMATION

Community Development Department – Planning and Zoning Division
2100 Ridge Avenue, Room 3202 Evanston, Illinois 60201
P. 847-448-4311 F. 847-448-8126 E. zoning@cityofevanston.org
www.cityofevanston.org/zoning



City of Evanston
DISCLOSURE STATEMENT
FOR ZONING HEARINGS

OFFICE USE	
CASE #	
DATE RECEIVED	

The Evanston City Code, Title 1, Chapter 18, requires any persons or entities who request the City Council to grant zoning amendments, variations, or special uses, including planned developments, to make the following disclosures of information. The applicant is responsible for keeping the disclosure information current until the City Council has taken action on the application. For all hearings, this information is used to avoid conflicts of interest on the part of decision-makers.

1. Address or location of property for which zoning relief is sought, if applicable: 3434 CENTRAL Zip: 60201

2. Name of applicant for zoning relief: Charles Marlas

3. If applicant is an agent or designee, list the name, address, phone, fax, and any other contact information of the proposed user of the land for which this application for zoning relief is made: Does not apply.

4. *If a person or organization owns or controls the proposed land user*, name, address, phone, fax, and any other contact information of person or entity having constructive control of the proposed land user. Same as number _____ above, or indicated below. (An example of this situation is if the land user is a division or subsidiary of another person or organization.)

Charles Marlas
743 McClintock Drive, Burr Ridge, IL 60527
630-990-8000
cmarlas@kensingtonschool.com

5. Name, address, phone, fax, and any other contact information of person or entity holding title to the subject property. Same as number _____ above, or indicated below.

Unity Church
3434 Central, Evanston, IL 60201
Kurt Condra
847-864-8977

6. Name, address, phone, fax, and any other contact information of person or entity having constructive control of the subject property. Same as number ⁴_____ above, or indicated below.

If Applicant or Proposed Land User is a Corporation



7.

Any corporation required by law to file a statement with any other governmental agency providing substantially the information required below may submit a copy of this statement in lieu of completing a and b below.

a. Names and addresses of all officers and directors.

b. Names, addresses, and percentage of interest of all shareholders, if there are fewer than 33 shareholders, or shareholders holding 3% or more of the ownership interest in the corporation, if there are more than 33 shareholders.

If Applicant or Proposed Land User is not a Corporation



8.

Name, address, percentage of interest, and relationship to applicant, of each partner, associate, person holding a beneficial interest, or other person having an interest in the entity applying, or in whose interest one is applying, for the zoning relief.

Charles Marlas 100%

I certify that all of the above statements and all statements, information and exhibits that I am submitting in conjunction with this application for relief from the requirements of the Zoning Ordinance or for an appeal from the Zoning Administrator's decision are true to the best of my knowledge.

2.1.2022

Applicant's signature

Date

Applicant's signature

Date

Applicant's signature

Date

PURCHASE AND SALE AGREEMENT

THIS PURCHASE AND SALE AGREEMENT ("Agreement") made and entered into as of August 13, 2021 ("Execution Date"), by and among **UNITY CHURCH ON THE NORTH SHORE**, an Illinois not for profit corporation ("Seller") and **CHARLES MARLAS**, or his related assignee, an LLC to be formed ("Buyer").

Purchase and Sale of the Property. Subject to the terms, provisions and conditions set forth herein, Seller hereby agrees to sell the Property to Buyer, and Buyer hereby agrees to purchase the Property from Seller. As used herein, the term "Property" shall mean, collectively, all of Seller's right, title and interest in: (a) that certain real estate commonly known as 3434 N. Central Avenue, Evanston, Illinois and legally described on **EXHIBIT A**, consisting of approximately 57,942.76 square feet of land (the "Land"); (b) the building(s), parking improvements and other improvements located on the Land (the "Improvements"); (c) all tangible personal property owned by Seller which is identified on **EXHIBIT A-1** attached hereto (the "Personal Property");

1. **Purchase Price for Property.** The Purchase Price for the Property shall be One Million Nine Hundred Thousand Dollars (\$1,900,000.00) to be paid in readily available in U.S. Dollars Within two (2) business days of the Execution Date of this Agreement, Buyer shall deposit with Old Republic Title Insurance Company, 20 South Clark Street, Chicago, Illinois (the "Title Company") a deposit in the amount of Seventy Thousand Dollars (\$70,000.00) (the "Deposit") pursuant to the terms of the joint order escrow instructions in the form attached hereto as **EXHIBIT D** (the "Earnest Money Instructions"). The Deposit shall be credited against the Purchase Price at the Closing of the transaction contemplated hereby (the "Closing"). The balance of the Purchase Price increased or decreased by any closing costs and prorations provided below, shall be paid in full by a wire transfer at Closing.

2. **Effective Date.** For purposes of this Agreement, the term "Effective Date" shall mean the first (1st) business day after the date upon which Seller has obtained the approval of Seller's Board of Directors but in no event shall be later than August 31, 2021.

3. **Approvals.**

(a) **Zoning Governmental Approval.** The Buyer and Seller hereby acknowledge that (i) Buyer is purchasing the Property for the purpose of developing a childcare center materially consistent with the site plan (the "Intended Purpose") attached hereto as **EXHIBIT C** (the "Site Plan"); and (ii) Buyer shall be granted 180 days from the Effective Date (the "Zoning Approval Period") to obtain approval for a change in the zoning of the Property from the City of Evanston, Illinois (the "Zoning Approval"). In the event Buyer is not granted Zoning Approval prior to the expiration of the Zoning Approval Period, Buyer shall provide Seller with written notice of the same and shall have the right to either (a) request an additional sixty (60) day extension; or (b) terminate this Agreement.

(b) **Co-Op Easement Agreement.** Buyer hereby acknowledges that the Property used as a means of ingress and egress by the adjacent land owner to its property and may be subject to an easement agreement, recorded as Document No. {TBD} (the "Existing Easement Agreement")

related to such easement rights. As a condition to Closing, Seller shall cause the Existing Easement Agreement to be relocated to the area specified on the attached easement area plan as attached hereto as **EXHIBIT F** (the "Relocation Easement Area") and shall cause the delivery of a mutually (as between Seller and the adjacent owner) acceptable new easement agreement which shall be executed, delivered and recorded at Closing (the "New Easement Agreement"). In the event Seller is unable to obtain a New Easement Agreement acceptable to Buyer and Seller by not later than October 1, 2021, Seller shall have the right to terminate this Agreement and the Deposit shall be returned to Buyer.

(c) **Seller's Board of Directors and Voting Membership Approval.** Within sixty (60) days of the Effective Date (the "Seller Internal Approval Period"), Seller shall use its best efforts to obtain the approval of its voting membership (the "Seller Internal Approval"). Buyer hereby acknowledges and agrees that this Agreement shall be subject to Seller's Internal Approval, in the event Seller does not obtain the requisite Seller's Internal Approval within the Seller Internal Approval Period, Seller shall, upon written notice to Buyer, have the right to terminate this Agreement and the Deposit shall be returned to Buyer.

4. **Title and Survey Review.**

(a) Within ten (10) business days after the Execution Date, Seller shall (at its expense) obtain and deliver to Buyer a commitment for title insurance issued by the Title Company dated and including copies of all documents of record referred to therein (collectively the "Title Commitment").

(b) Within fifteen (15) days after Buyer receives the last of the Title Commitment and the Updated Survey (as hereinafter defined), Buyer shall notify Seller in writing of: (i) any objection Buyer may have to any exceptions reported in the Title Commitment or any matter shown on or absent from the Updated Survey or (ii) any endorsements required to insure over title and survey objections or closing deliveries to be made by Seller in connection with the issuance of the Title Policy (defined herein) as a condition to Closing (the "Buyer's Objections"). Within ten (10) business days after receipt of the Buyer's Objections, Seller shall then notify Buyer in writing (a "Defect Response") that it will, by Closing, either cure or cause the Title Company to cure the Buyer's Objections in a manner reasonably satisfactory to Buyer or that Seller is unable or unwilling, in its sole discretion, to cure or cause the Title Company to cure such Buyer's Objections.

(c) If Seller is unable or unwilling to cure any or all of the Buyer's Objections as disclosed by the Defect Response, Buyer shall have the right to either: (i) terminate this Agreement by sending written notice of such termination to Seller within five (5) business days following Buyer's receipt of Seller's Defect Response, in which event, the Deposit shall be refunded to Buyer promptly as Buyer's sole and exclusive remedy, and, except as explicitly stated herein, thereafter neither Seller nor Buyer shall have any further obligations under this Agreement, except such obligations as explicitly survive the expiration or earlier termination of this Agreement; or (ii) waive such Buyer's Objections and accept title to the Property subject thereto. Buyer's failure to provide written notice of termination within such five (5) business day period shall be deemed as Buyer's election to proceed with the transaction as contemplated herein.

(d) Within ten (10) days after the Execution Date, the Seller shall (at its expense) obtain an updated land Survey (the "Updated Survey") which Updated Survey shall be prepared in accordance with current ALTA Survey standards and certified to Seller, Buyer, Buyer's lender (if any) and the Title Company. In the event the Updated Survey shall not comply with the foregoing or shall result in the Title Company's addition of any exceptions to title which are unacceptable to Buyer, then Buyer may deliver to Seller a Buyer's Objection with respect to the Updated Survey provided that such Buyer's Objection is made within five (5) business days of receiving the Updated Survey, and if the Buyer's Objection is timely made then the process identified in subsections 4(b) and 4(c) shall be adhered to and each party reserves its rights with respect thereto (including, without limitation, Buyer's right under subsection 4(c)(i) above to terminate if such Buyer's Objections are not remedied by the Seller prior to Closing).

(e) Notwithstanding the foregoing, at or prior to Closing, Seller at Seller's expense shall remove or insure over any exceptions on the Title Commitment that relate to (collectively "Monetary Liens"): (i) mortgages, financing statements, financing liens, mechanics' and materialmen's liens encumbering the Property to the extent caused by Seller, (ii) delinquent tax liens relating to the Property, or (iii) other liens or encumbrances which secure other monetary obligations of Seller which are due and payable and are of a definite, undisputed and ascertainable amount. On the Closing Date, Seller shall convey title to the Property free from all defects, exceptions and encumbrances, except for the Permitted Exceptions (defined below).

(f) The term "Permitted Exceptions" as used herein shall collectively mean all of the following: (i) general real estate taxes and assessments which are not yet due and payable as of the Closing Date; (ii) exceptions to title relating to any matters created by, through, or under the acts of Buyer or its agents; (iii) all title exceptions and survey matters which Buyer agrees to take title subject to pursuant to the provisions of this Section 3 above; (iv) covenants, conditions, restrictions and easements recorded against the title and which run with the land; and (v) any other exceptions to title approved in writing by Buyer. Permitted Exceptions shall not include any matters of title or survey first arising after the effective date of the Title Commitment or the Updated Survey that are not approved by Buyer.

5. Due Diligence Contingency.

(a) Within five (5) business days from the Effective Date of this Agreement, Seller shall deliver to Buyer copies (in Seller's possession) of the items set forth on **EXHIBIT B** (collectively, the "Due Diligence Materials").

(b) The term "Contingency Time" shall mean 6:00 p.m. (CST) on that date which is ninety (90) days after delivery by Seller of the last of the Due Diligence Materials and the Updated Survey; it being understood that Buyer shall have 180 days from the Effective Date (subject to the extension set forth in Section 3(a)) for the sole purpose of securing the Zoning Approval. During the Contingency Time and upon the delivery by Buyer to Seller of a certificate of insurance as required in Section 5(d) below, Buyer, at its sole cost and expense, through its agents, employees and independent contractors ("Buyer's Agents"), has the right, upon twenty-four (24) hours' prior notice to Seller, to enter the Land and Improvements, for the purpose of inspecting the same and performing, a Phase I environmental assessment, an asbestos inspection, , engineering and other

inspection or tests thereon; PROVIDED, HOWEVER, BUYER IS NOT PERMITTED TO PERFORM ANY SAMPLING, BORING, DRILLING OR OTHER PHYSICALLY INTRUSIVE TESTING INTO ANY STRUCTURES OR GROUND COMPRISING THE PROPERTY, INCLUDING, WITHOUT LIMITATION, A PHASE II ENVIRONMENTAL ASSESSMENT, OR ANY TESTING OF THE GROUND WATER WITHOUT (I) SUBMITTING TO SELLER THE SCOPE FOR SUCH TESTING; AND (II) OBTAINING THE PRIOR WRITTEN CONSENT OF SELLER FOR SUCH TESTING, WHICH CONSENT SELLER MAY WITHHOLD IN ITS SOLE AND ABSOLUTE DISCRETION. BUYER SHALL UTILIZE ONLY THOSE ENVIRONMENTAL CONTRACTORS APPROVED BY SELLER, IN SELLER'S SOLE DISCRETION. IN THE EVENT SELLER DETERMINES THAT IT DESIRES BUYER TO TERMINATE OR LIMIT ANY INSPECTION, STUDY OR TEST THAT BUYER IS PERFORMING, SELLER SHALL NOTIFY BUYER OF SELLER'S DESIRE AND BUYER SHALL CAUSE SUCH INSPECTION, STUDY OR TEST TO BE IMMEDIATELY TERMINATED OR SO LIMITED. SELLER HEREBY DISCLAIMS THE ACCURACY OR COMPLETENESS OF ANY INSPECTIONS, STUDIES OR TESTS THAT BUYER CAUSES TO BE CONDUCTED ON THE PROPERTY.

(c) **Confidentiality**. Prior to Closing, Buyer agrees and covenants with Seller not to disclose to any third party (other than lenders, accountants, attorneys and other professionals and consultants in connection with the transaction contemplated herein, whom Buyer shall cause to keep such information confidential) without Seller's prior written consent, unless Buyer is obligated by law to make such disclosure any of the reports or any other documentation or information obtained by Buyer which relates to the Property or Seller in any way, all of which shall be used by Buyer and its agents solely in connection with the transaction contemplated hereby. In the event that this Agreement is terminated, Buyer agrees that all such information will continue to be held in strict confidence.

(d) **Indemnity**. Buyer agrees to indemnify, protect, defend and hold Seller and its employees, officers, directors, shareholders, subsidiaries, affiliates, parent, advisors and other agents and their respective employees, officers, directors and, shareholders (collectively, the "Indemnified Parties") harmless from and against any and all liabilities, demands, actions, causes of action, suits, claims, losses, damages, costs and expenses (including, without limitation, reasonable attorneys' fees, court costs and litigation expenses) suffered or incurred by any of the Indemnified Parties as a result of or in connection with any activities of Buyer (including activities of any of Buyer's employees, consultants, contractors or other agents) relating to the Property, including, without limitation, mechanics' liens, damage to the Property, or injury to persons or property resulting from such activities in connection therewith. In the event that the Property is disturbed or altered in any way as a result of such activities, Buyer shall promptly restore the Property to its condition existing prior to the commencement of such activities which disturb or alter the Property. Furthermore, Buyer agrees to maintain and cause any of its representatives or agents conducting any Due Diligence to maintain and have in effect commercial general liability insurance with (i) limits of not less than One Million and 00/100 Dollars (\$1,000,000.00) for personal injury, including bodily injury and death, and property damage, (ii) UNITY ON THE NORTH SHORE named as an additional insured party and (iii) waiver of subrogation. Buyer shall deliver to Seller a copy of the certificate of insurance effectuating the insurance required hereunder prior to the

commencement of such activities which certificate shall provide that such insurance shall not be terminated or modified without at least thirty (30) days' prior written notice to Seller.

(e) **Buyer's Termination Right.** Prior to the expiration of the Contingency Time, Buyer may terminate this Agreement for no reason or for any reason by delivery of written notice of such termination to Seller. If Buyer terminates this Agreement at or before the expiration of the Contingency Time pursuant to this Section 5(e), Buyer shall be entitled to a return of the Deposit, and any interest thereon, and all obligations under this Agreement shall terminate, except those obligations which specifically survive the expiration or earlier termination of this Agreement. If Buyer does not timely give notice of termination as aforesaid, Buyer shall be deemed to have waived its right to terminate this Agreement on account thereof and Buyer shall be deemed to have elected to purchase the Property in accordance with the remaining terms and conditions of this Agreement and this Agreement shall continue in full force and effect.

(f) **Survival.** The obligations set forth in Sections 5(b), 5(c), 5(d) and 6 and such other provisions in this Agreement designated as surviving shall survive Closing or any termination of this Agreement (collectively, the "Surviving Obligations").

6. Buyer elects to terminate all service contracts relating to the Property. At or prior to Closing, Seller shall make commercially reasonable efforts to terminate all service contracts. Buyer shall not be liable for any costs associated with terminating such service contracts.

7. **Purchase As-Is.** EXCEPT FOR THE REPRESENTATIONS OF SELLER EXPRESSLY SET FORTH IN SECTION 12 OF THIS AGREEMENT, BUYER WARRANTS AND ACKNOWLEDGES TO AND AGREES WITH SELLER THAT BUYER IS PURCHASING THE PROPERTY IN ITS "AS-IS, WHERE IS" CONDITION "WITH ALL FAULTS" AND DEFECTS AS OF THE CLOSING DATE AND SPECIFICALLY AND EXPRESSLY WITHOUT ANY WARRANTIES, REPRESENTATIONS OR GUARANTEES, EITHER EXPRESS OR IMPLIED, AS TO ITS CONDITION (INCLUDING, WITHOUT LIMITATION, ITS PHYSICAL CONDITION OR ENVIRONMENTAL MATTERS OR CONDITIONS), FITNESS FOR ANY PARTICULAR PURPOSE, MERCHANTABILITY, OR ANY OTHER REPRESENTATION OR WARRANTY OF ANY KIND, NATURE, OR TYPE WHATSOEVER FROM OR ON BEHALF OF SELLER. EXCEPT FOR THE REPRESENTATIONS OF SELLER EXPRESSLY SET FORTH IN SECTION 12 OF THIS AGREEMENT, SELLER SPECIFICALLY DISCLAIMS ANY WARRANTY, GUARANTY OR REPRESENTATION, ORAL OR WRITTEN, PAST OR PRESENT, EXPRESS OR IMPLIED, CONCERNING (A) THE VALUE, NATURE, QUALITY OR CONDITION OF THE PROPERTY, INCLUDING, WITHOUT LIMITATION, THE WATER, STRUCTURAL INTEGRITY, SOIL AND GEOLOGY; (B) THE INCOME TO BE DERIVED FROM THE PROPERTY; (C) THE SUITABILITY OF THE PROPERTY FOR ANY AND ALL ACTIVITIES AND USES WHICH BUYER MAY CONDUCT THEREON, INCLUDING THE POSSIBILITIES FOR FUTURE DEVELOPMENT OF THE PROPERTY; (D) THE COMPLIANCE OF OR BY THE PROPERTY OR ITS OPERATION WITH ANY LAWS, RULES, ORDINANCES OR REGULATIONS OF ANY APPLICABLE GOVERNMENTAL AUTHORITY OR BODY; (E) THE HABITABILITY, MERCHANTABILITY, MARKETABILITY, PROFITABILITY OR FITNESS FOR A PARTICULAR PURPOSE OF

THE PROPERTY; (F) THE MANNER OR QUALITY OF THE CONSTRUCTION OR MATERIALS, IF ANY, INCORPORATED INTO THE PROPERTY; (G) THE MANNER, QUALITY, STATE OF REPAIR OR LACK OF REPAIR OF THE PROPERTY; (H) THE PRESENCE OR ABSENCE OF HAZARDOUS MATERIALS AT, ON, UNDER, OR ADJACENT TO THE PROPERTY OR ANY OTHER ENVIRONMENTAL MATTER OR CONDITION OF THE PROPERTY; OR (I) ANY OTHER MATTER WITH RESPECT TO THE PROPERTY. BUYER HEREBY RELEASES SELLER FROM ALL RESPONSIBILITY AND LIABILITY REGARDING THE PROPERTY, INCLUDING, WITHOUT LIMITATION, THE CONDITION (INCLUDING, BUT NOT LIMITED TO, THE CONDITION OF THE SOIL, WATER, GROUND WATER, PRESENCE OF HAZARDOUS MATERIALS AND ALL OTHER PHYSICAL CHARACTERISTICS), VALUATION OR UTILITY OF THE PROPERTY. THIS SECTION 7, INCLUDING WITHOUT LIMITATION THE RELEASE SET FORTH ABOVE SHALL SURVIVE THE CLOSING. BUYER ACKNOWLEDGES AND AGREES THAT, EXCEPT FOR THE REPRESENTATIONS AND WARRANTIES OF SELLER CONTAINED IN SECTION 12 OF THIS AGREEMENT, ANY INFORMATION PROVIDED BY OR ON BEHALF OF SELLER WITH RESPECT TO THE PROPERTY WAS OBTAINED FROM A VARIETY OF SOURCES AND THAT SELLER HAS NOT MADE ANY INDEPENDENT INVESTIGATION OR VERIFICATION OF SUCH INFORMATION AND MAKES NO REPRESENTATIONS AS TO THE ACCURACY OR COMPLETENESS OF SUCH INFORMATION. SELLER IS NOT LIABLE OR BOUND IN ANY MANNER BY ANY ORAL OR WRITTEN STATEMENTS, REPRESENTATIONS OR INFORMATION PERTAINING TO THE PROPERTY, OR THE OPERATION THEREOF, FURNISHED BY ANY REAL ESTATE BROKER, AGENT, EMPLOYEE, SERVANT OR OTHER PERSON EXCEPT FOR THE EXPRESS REPRESENTATIONS SET FORTH IN SECTION 12 OF THIS AGREEMENT. BUYER FURTHER ACKNOWLEDGES AND AGREES THAT PURCHASER IS A SOPHISTICATED AND EXPERIENCED PURCHASER OF PROPERTIES SUCH AS THE PROPERTY AND HAS BEEN DULY REPRESENTED BY COUNSEL IN CONNECTION WITH THE NEGOTIATION OF THIS AGREEMENT. BUYER ACKNOWLEDGES THAT PURCHASER HAS NOT RELIED, AND IS NOT RELYING UPON ANY INFORMATION, DOCUMENT, SALES BROCHURES OR OTHER LITERATURE, MAPS OR SKETCHES, PROJECTION, PRO FORMA, STATEMENT, REPRESENTATION, GUARANTEE OR WARRANTY (WHETHER EXPRESS OR IMPLIED, OR ORAL OR WRITTEN, MATERIAL OR IMMATERIAL) THAT MAY HAVE BEEN GIVEN BY OR MADE BY OR ON BEHALF OF THE SELLER. EXCEPT AS MAY OTHERWISE BE PROVIDED HEREIN, SELLER HAS MADE NO AGREEMENT TO ALTER, REPAIR OR IMPROVE ANY OF THE PROPERTY. FOR PURPOSES OF THIS SECTION 7, THE PROPERTY SHALL INCLUDE ANY PERSONAL PROPERTY BEING TRANSFERRED TO BUYER HEREUNDER.

8. **Conditions Precedent to Closing.**

The following are conditions of Closing, unless they have been waived by the applicable party per (in such party's sole and absolute discretion) in writing prior to Closing:

- (i) Neither party shall be in material default of any covenant or obligation under this Agreement,

- (ii) Each party's representations and warranties set forth in this Agreement shall remain true and correct in all material respects as of the Closing Date;
- (iii) the Title Company shall be committed to issue at the Closing with respect to the Property an original extended coverage ALTA owner's title insurance policy in the amount of the Purchase Price, subject only to the Permitted Exceptions and containing such endorsements as may be required by Buyer (the "Title Policy");
- (iv) no material violation of any applicable law with respect to the Property (which was not known to the Buyer as of the Contingency Time) shall exist as of the Closing Date;
- (v) Seller shall be in a position to execute and deliver the New Easement Agreement; and
- (vi) the Property is vacant and free from all tenancies.

9. **Closing.**

- (a) Provided all conditions to Closing have been satisfied, the Closing shall occur no later than sixty (60) days after Buyer obtains all necessary approvals from the City of Evanston but not later than March 1, 2022, (the "Closing Date") through a so-called "New York style" escrow at the Title Company's Chicago office. Possession of the Property will be transferred to Buyer at the conclusion of the Closing, subject only to the Permitted Exceptions.
- (b) On or before the Closing, Seller shall deliver or cause to be delivered to Title Company the following, fully executed and acknowledged by Seller as applicable:
 - (i) a Special Warranty Deed in a form set forth on Exhibit E ("Deed");
 - (ii) a quitclaim bill of sale conveying title to the Personal Property (the "Bill of Sale");
 - (iii) FIRPTA affidavit in a form accepted by the Title Company;
 - (iv) a comprehensive settlement statement prepared by the Title Company (the "Settlement Statement");
 - (v) such transfer tax declarations as may be required by applicable state and local law in connection with the conveyance of real property;
 - (vi) evidence of the existence, organization and authority of Seller satisfactory to the Title Company;
 - (vii) such other documents, certificates and instruments reasonably necessary (as determined by the Title Company) in order to effectuate the transaction described herein including: (A) an ALTA Statement sufficient to remove all general

exceptions, (B) all required "gap" indemnities and (C) other documents reasonably required by the Title Company in order to issue the Title Policy;

(c) On or before the Closing, Buyer shall deliver, or cause to be delivered, to Title Company, as applicable, the following, fully executed and acknowledged by Buyer as applicable:

- (i) the balance of the Purchase Price (less the Deposit previously delivered and plus or minus any credits or prorations hereunder) by wire transfer;
- (ii) a countersigned Settlement Statement;
- (iii) an executed ALTA Statement;
- (iv) such other documents, certificates and instruments reasonably necessary (as determined by the Title Company and Buyer's lender (if any)) in order to effectuate the transaction described herein;

(d) *Closing Costs.* At Closing, Seller shall pay: (i) the real estate conveyance tax, intangible taxes and other transfer taxes imposed by the State, County and City in connection with the conveyance of the Property contemplated hereby; (ii) all of the costs and expenses of its counsel; (iii) one-half of all closing fees charged by the Title Company, including escrow and New York Style closing fees; (iv) the costs of the Existing Survey, the Updated Survey and the Title Commitment; (v) the premium for the Title Policy and an extended coverage endorsement; and (vi) such other costs related to the sale of the Property as are normally paid by a Seller in Cook County, Illinois. In addition, Seller shall pay any and all costs and fees associated with and/or resulting from any inspections required by local authorities with respect to or arising out of the transfer of the Property to Buyer, and shall comply therewith at its sole cost.

(e) Buyer shall pay: (i) all of the costs and expenses of its counsel; (ii) one-half of all closing fees charged by the Title Company, including escrow and New York Style closing fees; (iii) Lender's escrow, if any; (iv) the premium for any lender's title insurance policy, if any; (v) the cost of any studies or inspections conducted by Buyer during Buyer's due diligence; (vi) the cost of all title endorsements required by Buyer beyond extended coverage; and (vii) Lender's legal fees, if any; (viii) all costs pertaining to Buyer's financing of the purchase; (ix) all fees charged for the recording of the Deed and any loan documents; and (x) such other costs related to the sale of the Property as are normally paid by a Buyer in Cook County, Illinois.

10. **Prorations.** At Closing, in the event that Buyer and Seller mutually determine that any costs or expenses related to the Property require a proration, , the following prorations shall be computed and apportioned to: (i) Buyer; as a fraction, the numerator of which shall be the number of days in the period for which such charges are paid (the "Pay Period") after and including the Closing date, over the number of days in the Pay Period (date of Closing is an income and expense to Buyer); and, (ii) Seller; as a fraction, the numerator of which shall be the number of days in the Pay Period before the Closing date, over the number of days in the Pay Period.

(a) *Water, sewer and utility charges.* Seller shall have final meter readings performed prior with respect to any utility services billed to Seller as of the day prior to Closing and shall pay all of its utility charges..

(b) *Real Estate Taxes.* Intentionally omitted.

11. **Seller's Escrow Period Covenants.** Beginning on the Effective Date and up until Closing or the earlier termination of the Agreement, Seller shall: (a) insure and maintain the Property consistent with its current operating practices; (b) not cause or expressly permit any document to be recorded adversely against title to the Property, or, if any such document is so recorded then Seller shall be permitted reasonable time to cure the same, and shall not modify, amend or terminate any existing documents of record without Buyer's express written consent; (c) promptly furnish Buyer copies of all written notices received by Seller: (i) from a governmental entity alleging a violation by Seller or the Property of federal, state or municipal laws, ordinances, regulations, orders, or requirements of departments of housing, buildings, fire, labor, health, or other federal, state or municipal departments or other governmental authorities having jurisdiction over or materially affecting the Property or the use or operation thereof or (ii) from a third party to any record documents alleging a material Seller default under such record document; and (d) not actively market the Property for sale and shall not actively negotiate or enter into any letter of intent or purchase agreement with respect to the Property (other than this Agreement).

12. **Seller Representations and Warranties.** Seller makes the following representations and warranties as of the Effective Date and again as of the Closing (all of which shall survive the Closing for a period of twelve (12) months (the "Survival Period") and shall not be merged into the Deed):

(a) Seller is an Illinois not for profit corporation and prior to the Closing Date, Seller will have obtained approval of its Board of Directors and its voting membership (collectively, the "Seller Approvals") and will have obtained all necessary authorizations and consents to enable it to execute and deliver this Agreement and to consummate the transaction contemplated hereby. Subject to the Seller Approvals, this Agreement and the other documents to be executed by Seller hereunder will have been duly entered into by Seller and will constitute legal, valid and binding obligations of Seller enforceable in accordance with their respective terms.

(b) Seller is the owner of the Property.

(c) The execution, delivery and performance of this Agreement and the Closing hereunder will not conflict with any agreement, contract or, to the best of Seller's knowledge, law applicable to Seller or constitute a default under any agreement or instrument to which Seller is a party or by which Seller or the Property are bound.

(d) There are no leases with respect to the Property.

(e) Seller has received no written notice from any governmental authority with jurisdiction over the Property: (i) alleging any violation of any laws applicable to the Property which remains uncured to the full satisfaction of the governmental authority issuing said notice; (ii) of any action

or governmental proceeding for Condemnation, or for a zoning change; or (iii) alleging a violation of any law, rule or regulation with respect to the presence of hazardous materials on the Property.

(f) Seller has received no written notice from any adjoining property owner alleging: (i) any boundary disputes with such adjoining landowners respecting the Property; (ii) a dispute relating to impairments of access to and from the Property; or (iii) a default by Seller under any record agreement.

(g) To the best of Seller's knowledge, Seller has received no written notice from any adjoining property owner alleging: (i) any boundary disputes with such adjoining landowners respecting the Property; or (ii) a dispute relating to impairments of access to and from the Property.

(h) To the best of Seller's knowledge, there is no material litigation, arbitration or administrative proceeding pending with respect to the Property.

(i) To the best of Seller's knowledge, there is not the presence of any hazardous materials at the Property or the existence of any violation of any law, rule or regulation relating to hazardous materials with respect to the Property.

(j) For all purposes, "Seller's knowledge" shall be deemed to mean only the actual, present knowledge of Pastor Kurt Condra, and shall not include the knowledge, actual, implied, imputed or constructive, of any partner, related entity, agent, attorney, contractor, consultant, or other employee of Seller, or of any other person, or entity.

13. **Buyer Representations and Warranties.** Buyer hereby represents, warrants and covenants as follows:

(a) **Power.** Buyer has the legal power, right and authority to enter into this Agreement and the instruments referenced herein and to consummate the transactions contemplated hereby.

(b) **Requisite Action.** All requisite action (corporate, trust, partnership or otherwise) has been taken by Buyer in connection with entering into this Agreement and the instruments referenced herein and the consummation of the transactions contemplated hereby. No consent of any partner, shareholder, member or investor is required which has not been obtained to permit Purchaser to enter into this Agreement and consummate the transaction contemplated hereby.

(c) **Authority.** The individuals executing this Agreement and the instruments referenced herein on behalf of Buyer have the legal power, right and actual authority to bind Buyer to the terms and conditions hereof and thereof.

(d) **Validity.** This Agreement and all documents required hereby to be executed by Buyer are and shall be valid, legally binding obligations of and enforceable against Buyer in accordance with their terms.

(e) **Conflicts.** Neither the execution and delivery of this Agreement and documents referenced herein, nor the incurrence of the obligations set forth herein, nor the consummation of the transactions herein contemplated, nor referenced herein conflict with or result in the material

breach of any terms, conditions or provisions of or constitute a default under, any bond, note, or other evidence of indebtedness or any contract, lease or other agreements or instruments to which Buyer is a party.

(f) Litigation. There is no action, suit or proceeding pending or threatened against Buyer in any court or by or before any other governmental agency or instrumentality which would materially and adversely affect the ability of Buyer to carry out the transactions contemplated by this Agreement.

14. Defaults. If Buyer commits a breach hereof or defaults hereunder, and such breach or default is not cured within five (5) business days following the date of Seller's written notice to Buyer alleging such default ("Buyer's Default"), Seller will have the right to terminate this Agreement by further written notice thereof to Buyer, effective immediately, and Seller shall have the right to cause the delivery of the Deposit by the Escrowee as full and final liquidated damages in connection with Buyer's Default. Seller and Buyer agree that the Deposit reflects their best estimate of the actual damages that will be incurred by Seller in the event of a Buyer Default hereunder.

In the event that Seller defaults under the terms and provisions of this Agreement (Seller's Default") and such Seller's Default is not cured within five (5) business days following the date of Buyer's written notice to Seller alleging such Seller's Default, Buyer shall be entitled, as its sole remedy either (a) to receive the return of the Deposit, plus an amount equal to \$15,000.00); or (b) to enforce specific performance of Seller's obligation to convey the Property to Buyer pursuant to the terms and conditions of this Agreement. Buyer shall be deemed to have elected to terminate this Agreement and receive back the Deposit if Buyer fails to file suit for specific performance against Seller in a court having jurisdiction in the county and state in which the Property is located, on or before three (3) months following the date upon which Closing was to have occurred.

15. Risk of Loss.

(a) Except as may be caused by Buyer, all risk of loss to the Property shall remain upon Seller until the conclusion of the Closing. If, before Closing, the Property is or if any portion of the Property is taken or formally threatened by eminent domain or condemnation (a "Condemnation"), Seller shall, upon the earlier of one (1) business day before Closing or five (5) business days after the occurrence of such or Condemnation, notify Buyer thereof and Buyer shall have the option to do any one of the following, by providing written notice to Seller within ten (10) days after Seller provides notice to Buyer of such Condemnation: (i) terminate this Agreement; or (ii) proceed with the purchase of the Property.

(b) In the event of a casualty loss to the Property Buyer shall have the option to do any one of the following, by providing written notice to Seller within ten (10) days after Seller provides notice to Buyer of such Casualty: (i) terminate this Agreement; or (ii) proceed with the purchase of the Property.

instruments as are reasonably necessary to implement such an exchange. Each of the parties hereto shall be solely responsible for assuring that the structure of any proposed exchange is effective for such party's tax purposes. Furthermore, each of the parties hereto specifically agrees that the other party hereto may assign this Agreement and any of its rights or obligations hereunder, in whole or in part, as necessary or appropriate in furtherance of effectuating a Section 1031 like-kind exchange for the Property, provided that such assignment shall not serve to relieve the assigning party of any liability for the assigning party's obligations hereunder. The other party hereto shall have no obligation to pay costs or expenses of effectuating such exchange, no such exchange shall alter the time for performance set forth herein, and the other party hereto shall not be required to take title to any exchange property or (except for customary consent to assignment of this Agreement to an exchange intermediary) to incur obligations to third parties.

19. **Brokers' Commission.**

Each of the parties hereto represents to the other that it has not entered into any agreement for the payment of any fees, compensation or expenses to any person, firm or corporation in connection with the transactions provided for herein except Seller has entered into such an agreement with Cresa Global and Greystone Bel Advisors. Each party agrees to indemnify and hold and save the other harmless from any such fees, compensation, damages or expenses which may be suffered by reason of such indemnifying party's representation above being incorrect. Seller shall pay the broker's commission under a separate agreement.

20. **Miscellaneous Provisions.**

(c) This Agreement shall be binding upon and shall inure to the benefit of Seller and Buyer and their respective successors and assigns.

(d) The several headings and captions of the sections and subsections used herein are for convenience of reference only, and shall in no way be deemed to limit, define or restrict the substantive provisions of this Agreement.

(e) This Agreement constitutes the entire agreement of Buyer and Seller with respect to the purchase and sale of the Property, is intended to be an integration of all prior negotiations and understandings and supersedes any prior or contemporaneous agreement with respect thereto. Buyer, Seller and their agents shall not be bound by any terms, conditions, statements, warranties or representations, oral or written, not contained herein.

(f) No amendment or modification of this Agreement shall be binding upon the parties unless made in writing and signed by both Seller and Buyer.

(g) Time is of the essence with respect to the performance of all the terms, conditions and covenants of this Agreement.

(h) Unless otherwise specified, in computing any period of time described herein, the day of the act or event after which the designated period of time begins to run is not to be included and the last day of the period so computed is to be included at, unless such last day is a Saturday, Sunday or legal holiday for national banks in the location where the Property is located, in which

event the period shall run until the end of the next day which is neither a Saturday, Sunday, or legal holiday.

(i) This Agreement and the rights of the parties hereunder shall be governed by and construed in accordance with the laws and customs of the State of Illinois.

(j) This Agreement may be executed in any number of counterparts and by the different parties hereto on separate counterparts, each of which, when so executed, shall be deemed an original, but all such counterparts shall constitute but one and the same instrument. This Agreement and any amendments may be executed and then delivered by fax or scanned email which shall constitute effective execution and delivery for all purposes.

(k) If any action is instituted between Seller and Buyer in connection with the enforcement of this Agreement or any provision hereof, the party prevailing in such action shall be entitled to recover from the other party all of its reasonable costs in bringing such action, including, but without limitation, reasonable attorney fees.

(l) If any provision of this Agreement is held to be illegal, invalid or unenforceable under present or future laws, and if such provision is not essential to the effectuation of the basic purposes of this Agreement, such provision shall be fully severable, this Agreement shall be construed and enforced as if such illegal, invalid or unenforceable provision had never been a part of this Agreement, and the remaining provisions of this Agreement shall remain in full force and effect.

(m) The waiver by either party of the performance of any covenant, condition or promise shall not invalidate this Agreement, nor shall it be construed as a waiver of any other covenant, condition or promise herein. Each party reserves the right to waive any of the terms or conditions of this Agreement which are for their respective benefit and to consummate the transaction contemplated by this Agreement in accordance with the terms and conditions of this Agreement which have not been so waived.

(N) The respective parties hereto shall and hereby do waive trial by jury in any action, proceeding or counterclaim brought by either of the parties hereto against the other on any matters whatsoever arising out of or in any way connected with this Agreement, or for the enforcement of any remedy under any statute.

[signatures follow on next page]

IN WITNESS WHEREOF, the parties hereto have executed this Purchase and Sale Agreement as of the Effective Date.

SELLER:

UNITY CHURCH ON THE NORTH SHORE,
an Illinois not for profit corporation

By: Kurt Condra
Name: Kurt Condra
Its: Senior Minister

By: June Lash
Name: June Lash
Its: Board of Trustees President

BUYER:

By: _____
Name: **CHARLES MARLAS**

IN WITNESS WHEREOF, the parties hereto have executed this Purchase and Sale Agreement as of the Effective Date.

SELLER:

UNITY CHURCH ON THE NORTH SHORE,
an Illinois not for profit corporation

By: _____
Name: _____
Its: _____

BUYER:

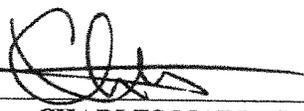
By:  _____
Name: CHARLES MARLAS

EXHIBIT A

Legal Description

LOT 1 (EXCEPT THAT PART OF THE WEST 60 FEET OF LOT 1 LYING SOUTH OF A LINE 150 FEET SOUTH OF THE NORTH LINE OF AFORESAID LOT 1 IN PLAT OF CONSOLIDATION RECORDED SEPTEMBER 11, 1974 AS DOCUMENT 22843735) OF LOT 1 IN WILLIAMSBURG AND LOT 2 IN UNITY RESUBDIVISION OF LOT 2 IN WILLIAMSBURG, A SUBDIVISION OF LOT 1 IN EVERT AND SCHAFER'S SUBDIVISION OF PART OF THE NORTHEAST ¼ OF FRACTIONAL SECTION 10, TOWNSHIP 41 NORTH, RANGE 13 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

PIN: 10-10-200-073-0000

COMMON ADDRESS: 3434 N. CENTRAL AVENUE, EVANSTON, ILLINOIS 60201

EXHIBIT A-1

LIST OF PERSONAL PROPERTY

EXHIBIT B

Due Diligence Deliveries

Property Financials
Property is Exempt from Real Estate Taxes
Tenant Leases and Correspondences
Executed Leases; if any—None
Insurance
Insurance Claims and Correspondence—None (last 3 years)
Miscellaneous
Current ALTA Survey
Current Title Report
Real Estate Tax Attorney Correspondences—None
Any existing Building Plans and Specs
Any existing Site Plans
Written Notice of Code Violations, if any—None
Warranties and Guarantees (roof warranty and HVAC)
Existing Phase I, if any
Existing Roof Reports within last 3 years (Power)

EXHIBIT C

Site Plan

TO BE COMPLETED PRIOR TO CLOSING

EXHIBIT D

Earnest Money Instructions



OLD REPUBLIC NATIONAL COMMERCIAL TITLE SERVICES

20 South Clark Street, Suite 2900, Chicago, IL 60603 | T: 312.641.7799

ESCROW NUMBER: NCT21011155

STRICT JOINT ORDER ESCROW- EARNEST MONEY DEPOSIT

Property Address: 3434 Central Street, Evanston, IL

Deposits: SEVENTY THOUSAND AND NO/100TH DOLLARS (the "Deposit") as delivered by Buyer pursuant to that certain Purchase and Sale Agreement, by and between Unity Church on the North Shore ("Seller"), and Charles Marlas ("Buyer") dated August 13, 2021.

Certified, uncertified, cashier check(s) or wire(s) in the amount of \$70,000.00 is hereby deposited with Old Republic National Title Insurance Company as ESCROWEE to be delivered by it only upon the joint written order of the undersigned or their respective legal representatives or assigns.

Old Republic National Title Insurance Company, as ESCROWEE, is hereby expressly authorized to disregard, in its sole discretion, any and all unilateral notices or warnings given by any of the parties hereto, or by any other person or corporation, but said ESCROWEE is hereby expressly authorized to regard and to comply with and obey any and all orders, judgments or decrees entered or issued by any court with or without jurisdiction, and in case the said ESCROWEE obeys or complies with any such order, judgment or decree of any court it shall not be liable to any of the parties hereto or any other person, firm or corporation by reason of such compliance, notwithstanding any such order, judgment or decree being entered without jurisdiction or being subsequently reversed, modified, annulled, set aside or vacated.

In case of any suit or proceeding regarding this escrow, to which said ESCROWEE is or may at any time become a party, it shall have a lien on the contents hereof for any and all costs, attorneys' and solicitors' fees, whether such attorneys or solicitors shall be regularly retained or specially employed, and any other expenses which it may have incurred or become liable for on account thereof, and it shall be entitled to reimburse itself therefore out of said deposit, and the undersigned jointly and severally agree to pay said ESCROWEE upon demand all such costs, fees and expenses so incurred.

In no case shall the above-mentioned deposits be surrendered except on an order signed by the parties hereto, their respective legal representatives or assigns, or in obedience of the process or order to court as aforesaid.

Deposits made pursuant to these instructions may be invested on behalf of any party or parties thereto: Provided, that any direction to ESCROWEE for such investment shall be expressed in writing and contain the consent of all other parties to these escrow and also provided that you are in receipt of the tax payer's identification number and investment forms as required. ESCROWEE will, upon request furnish information concerning its procedures and fee schedules for investment.

Except as to deposits of funds for which ESCROWEE has received express written direction concerning investment to other handling, the parties hereto agree that the ESCROWEE shall be under no duty to invest or reinvest any deposits at any time held by it thereunder: and, further that ESCROWEE may commingle such deposits with other deposits or with its own funds in the manner provided for the administration of funds under Section 2-8 of the Corporate Fiduciary Act (ILL. Rev. State 1992 205ILES 620/2-8) and may use any part or all such funds for its own benefit without obligation to any party for interest or earning derived thereby, if any. Provided, however, nothing herein shall diminish Escrowee's obligation to apply the full amount of the deposits in accordance with the terms of the Agreement.

In the event the ESCROWEE is requested to invest deposits hereunder, Old Republic National Title Insurance Company is not to be held responsible for any loss of principal or interest which may be incurred as a result of making the investments or redeeming said investment for the purposes of these escrow instructions.

ESCROW NUMBER: NCT21011155

PURCHASER:

CHARLES MARLAS

Signed By: _____

Jeffrey Sanchez, Attorney

Address: Zabel Law, LLC
55 W. Monroe, Suite 3330
Chicago, IL 60603

Phone: 312 201-9800

Fax: jsanchez@zabellaw.com

SELLER:

Unity Church on the North Shore, an
Illinois not for profit

Signed By: _____

LaVon M. Johns, Attorney

Address: Miller Canfield Paddock & Stone
225 W. Washington, Suite 2600
Chicago, IL 60606

Phone: (312) 882-5463

Fax: johns@millercanfield.com

ACCEPTED:

Old Republic National Title Insurance Company

By: _____

EXHIBIT E
FORM OF DEED

THIS INSTRUMENT PREPARED BY
AND AFTER RECORDING RETURN TO:

Miller, Canfield, Paddock and Stone, P.L.C.
225 W. Washington, Suite 2600
Chicago, Illinois 60606
Attention: LaVon M. Johns

SEND SUBSEQUENT TAX BILLS TO:

Property Pin # _____

Address: 3434 N. Central Avenue, Evanston, IL 60201

SPECIAL WARRANTY DEED

THAT UNITY CHURCH ON THE NORTH SHORE, an Illinois not for profit corporation (“**Grantor**”), for and in consideration of the sum of Ten and 00/100 Dollars (\$10.00) and other good and valuable consideration paid in cash to Grantor by the Grantee herein named, by these presents does GRANT, BARGAIN, SELL, REMISE, RELEASE, ALIENATE, CONVEY AND CONFIRM unto [CHARLES MARLAS] (“**Grantee**”), whose mailing address is _____, that certain real property situated in Cook County, Illinois, as more particularly described on Exhibit A attached hereto and made a part hereof for all purposes, together with all improvements thereon and appurtenances relating thereto (“**Property**”).

This Special Warranty Deed is expressly made subject to taxes and assessments for the 2021 tax year and all subsequent years and the matters set forth on Exhibit B attached hereto (collectively, the “**Permitted Exceptions**”).

TO HAVE AND TO HOLD the Property subject the Permitted Exceptions and all improvements located thereon, together with all and singular the rights and appurtenances thereto and in any wise belonging unto the said Grantee, its legal representatives, successors and assigns, forever; and Grantor does hereby bind itself, its legal representatives and successors, to Warrant and Forever Defend all and singular the Property subject to the Permitted Exceptions unto the said Grantee, its legal representatives, successors and assigns, against every person whomsoever lawfully claiming or to claim the same or any part thereof by, through or under Grantor, but not otherwise.

And without limiting the generality of the foregoing conveyances, Grantor does hereby quit claim unto Grantee and Grantee’s successors and assigns forever all right, title and interest of Grantor, if any, in and to all rights of way, streets, gaps and gores within and/or adjacent to the Property.

[Signature appears on following page.]

IN WITNESS WHEREOF, Grantor has hereunto caused this instrument to be executed on _____, 202____, to be effective as of _____, 202____.

GRANTOR:

UNITY CHURCH ON THE NORTH SHORE,
An Illinois not for profit corporation

By: _____
Name: _____
Title: _____

STATE OF ILLINOIS §
 §
COUNTY OF COOK §

This instrument was acknowledged and executed before me this _____ day of _____, 202____ by _____, the _____ of Unity Church on the North Shore, an Illinois not for profit corporation, known to me to be the person whose name is subscribed to the foregoing instrument, and acknowledged to me that he executed the same for the purposes and consideration therein expressed on behalf of said limited liability company.

GIVEN UNDER MY HAND AND SEAL OF OFFICE, this the ___ day of _____, 202____.

Notary Public – State of Illinois

My commission expires:

EXHIBIT A

LEGAL DESCRIPTION

EXHIBIT B

PERMITTED EXCEPTIONS

EXHIBIT F
RELOCATION EASEMENT AREA DEPICTION

37991976.1/160419.00001
08/10/21



OLD REPUBLIC NATIONAL COMMERCIAL TITLE SERVICES

20 South Clark Street, Suite 2900, Chicago, IL 60603 | T: 312.641.7799

ESCROW NUMBER: NCT21011155

STRICT JOINT ORDER ESCROW- EARNEST MONEY DEPOSIT

Property Address: 3434 Central Street, Evanston, IL

Deposits: SEVENTY THOUSAND AND NO/100TH DOLLARS (the "Deposit") as delivered by Buyer pursuant to that certain Purchase and Sale Agreement, by and between Unity Church on the North Shore ("Seller"), and Charles Marlas ("Buyer") dated August 13, 2021.

Certified, uncertified, cashier check(s) or wire(s) in the amount of \$70,000.00 is hereby deposited with Old Republic National Title Insurance Company as ESCROWEE to be delivered by it only upon the joint written order of the undersigned or their respective legal representatives or assigns.

Old Republic National Title Insurance Company, as ESCROWEE, is hereby expressly authorized to disregard, in its sole discretion, any and all unilateral notices or warnings given by any of the parties hereto, or by any other person or corporation, but said ESCROWEE is hereby expressly authorized to regard and to comply with and obey any and all orders, judgments or decrees entered or issued by any court with or without jurisdiction, and in case the said ESCROWEE obeys or complies with any such order, judgment or decree of any court it shall not be liable to any of the parties hereto or any other person, firm or corporation by reason of such compliance, notwithstanding any such order, judgment or decree being entered without jurisdiction or being subsequently reversed, modified, annulled, set aside or vacated.

In case of any suit or proceeding regarding this escrow, to which said ESCROWEE is or may at any time become a party, it shall have a lien on the contents hereof for any and all costs, attorneys' and solicitors' fees, whether such attorneys or solicitors shall be regularly retained or specially employed, and any other expenses which it may have incurred or become liable for on account thereof, and it shall be entitled to reimburse itself therefore out of said deposit, and the undersigned jointly and severally agree to pay said ESCROWEE upon demand all such costs, fees and expenses so incurred.

In no case shall the above-mentioned deposits be surrendered except on an order signed by the parties hereto, their respective legal representatives or assigns, or in obedience of the process or order to court as aforesaid.

Deposits made pursuant to these instructions may be invested on behalf of any party or parties thereto: Provided, that any direction to ESCROWEE for such investment shall be expressed in writing and contain the consent of all other parties to these escrow and also provided that you are in receipt of the tax payer's identification number and investment forms as required. ESCROWEE will, upon request furnish information concerning its procedures and fee schedules for investment.

Except as to deposits of funds for which ESCROWEE has received express written direction concerning investment to other handling, the parties hereto agree that the ESCROWEE shall be under no duty to invest or reinvest any deposits at any time held by it thereunder: and, further that ESCROWEE may commingle such deposits with other deposits or with its own funds in the manner provided for the administration of funds under Section 2-8 of the Corporate Fiduciary Act (ILL. Rev. State 1992 205 ILES 620/2-8) and may use any part or all such funds for its own benefit without obligation to any party for interest or earning derived thereby, if any. Provided, however, nothing herein shall diminish Escrowee's obligation to apply the full amount of the deposits in accordance with the terms of the Agreement.

In the event the ESCROWEE is requested to invest deposits hereunder, Old Republic National Title Insurance Company is not to be held responsible for any loss of principal or interest which may be incurred as a result of making the investments or redeeming said investment for the purposes of these escrow instructions.

ESCROW NUMBER: NCT21011155

PURCHASER:

CHARLES MARLAS

Signed By: 

Jeffrey Sanchez, Attorney

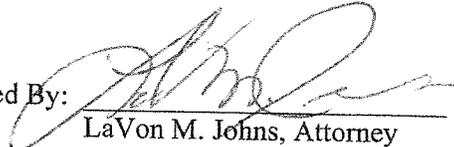
Address: Zabel Law, LLC
55 W. Monroe, Suite 3330
Chicago, IL 60603

Phone: 312 201-9800

Fax: jsanchez@zabellaw.com

SELLER:

Unity Church on the North Shore, an
Illinois not for profit

Signed By: 

LaVon M. Johns, Attorney

Address: Miller Canfield Paddock & Stone
225 W. Washington, Suite 2600
Chicago, IL 60606

Phone: (312) 882-5463

Fax: johns@millercanfield.com

ACCEPTED:

Old Republic National Title Insurance Company

By: _____

5. Purchase Contract and Contract Extensions

AMENDMENT TO PURCHASE AND SALE AGREEMENT

THIS AMENDMENT TO PURCHASE AND SALE AGREEMENT ("Amendment") dated as of the 24th day of April, 2024 by and between **UNITY CHURCH ON THE NORTH SHORE** (the "Seller") and **CHARLES MARLAS, on behalf of an LLC to be formed** (the "Buyer").

WHEREAS, the Seller and Buyer entered into a Purchase and Sale Agreement dated August 13, 2021, which has been previously amended and/or restated (the "Agreement"); and

WHEREAS, Seller is engaged in litigation in the Cook County Circuit Court bearing Case No. 2023 CH 06757 (the "Litigation"); and

WHEREAS, Purchaser requires an extension of the Zoning Government Approval period beyond the time permitted under the Agreement; and

WHEREAS, Buyer and Seller now desire to amend the terms of the Agreement upon the terms and conditions set forth in this Amendment;

NOW THEREFORE, for the sum of ONE (\$1.00) DOLLAR and other valuable consideration, the parties hereto agree as follows:

1. Zoning Government Approval. The entirety of Section 3(a) of the Agreement is deleted in its entirety, and replaced as follows:

Zoning Governmental Approval. The Buyer and Seller hereby acknowledge that (i) Buyer is purchasing the Property for the purpose of developing a childcare center materially consistent with the site plan (the "Intended Purpose") attached hereto as EXHIBIT C (the "Site Plan"); and (ii) Buyer shall be granted until one hundred and twenty (120) days after the conclusion of the Litigation (the "Zoning Approval Period") to obtain all City of Evanston and IDOT governmental approvals necessary, including approval of site access to Gross Point Road, to obtain the special use authorization and to utilize the Property for the Intended Purpose (the "Zoning Approval"). Zoning Approval does not include any permits necessary with regards to actual construction of improvements. In the event Buyer is not granted Zoning Approval prior to the expiration of the Zoning Approval Period, Buyer shall provide Seller with written notice of the same and shall have the right to either (a) request an additional sixty (60) day extension; or (b) terminate this Agreement.

2. Exhibit C. Exhibit C of the Agreement shall be replaced with the document attached hereto as Exhibit 1.
3. Closing. Section 9(a) of the Agreement is deleted in its entirety and replaced as follows:

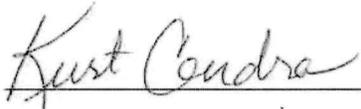
Provided all conditions to Closing have been satisfied, the Closing shall occur no later than sixty (60) days after the end of the Zoning Approval Period (the "Closing Date") through a so-called "New York style" escrow at the Title Company's Chicago office. Possession of the Property will be transferred to Buyer at the conclusion of the Closing subject only to the Permitted Exceptions.

4. Counterparts. This Amendment may be executed in multiple counterparts (including by facsimile transmission or electronic delivery of a "pdf" copy), each of which shall constitute an original, but all of which shall constitute one document. Each party agrees that the electronic signatures, whether digital or encrypted (including Adobe Sign or similar digital signature programs), of the parties included in this Amendment are intended, and shall be deemed, to authenticate this writing and to have the same force and effect as manual physical signatures.
5. Except as modified herein, all terms and conditions set forth in the Agreement shall continue in full force and effect.

[SIGNATURES ON FOLLOWING PAGE]

IN WITNESS WHEREOF, the parties hereto have set their hands and seals the day and year first above written.

UNITY CHURCH ON THE NORTH SHORE,
An Illinois not for profit corporation

By: 
Name: Kurt Condra
Its: Minister
Date: 4.25.24

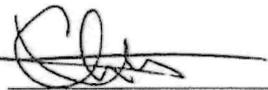

Charles Marlas
Date: 4.24.24

EXHIBIT 1

PRELIMINARY SITE IMPROVEMENT PLANS
for
KENSINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS
PROJECT NO:11641



KENSINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS

224 N. Liberty Street, Morris, Illinois 60450
Phone: (815) 941-0260 Fax: (815) 941-0263

CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

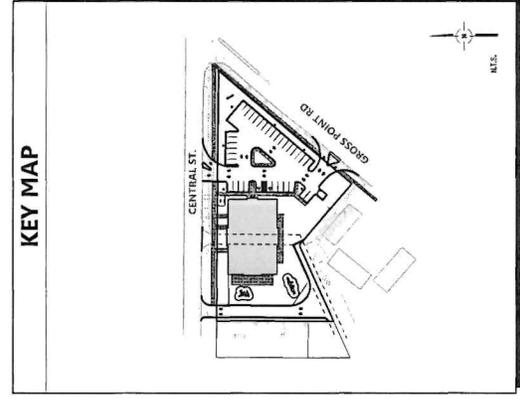


PREPARED BY	DATE	DRAWN BY	CHECKED BY
DATE	DATE	DATE	DATE
DATE	DATE	DATE	DATE
DATE	DATE	DATE	DATE

SHEET C1
1 OF 5

CALL J.U.L.I.E. 1-800-892-0123
WITH THE FOLLOWING:
PROJECT NO. 11641
SHEET NO. C1
48 HOURS BEFORE YOU DIG.
EXCLUDING SAT., SUN. & HOLIDAYS.

SHEET #	SHEET I.D.	SHEET DESCRIPTION
1	C1	COVER SHEET
2	P-01	PRELIMINARY EXISTING CONDITIONS
3	P-02	PRELIMINARY GEOMETRIC PLAN
4	P-03	PRELIMINARY GRADING PLAN
5	P-04	PRELIMINARY UTILITY PLAN



#	SHEET #	REMARKS	DATE
1	5	PER CITY OF EVANSTON	03/09/23
		04-10-23 LAND USE COMMISSION REVIEW	

REVISIONS

ORIGINAL PLAN DATE	REVISION DATE
FEBRUARY 27, 2023	

NO.	DATE	REMARKS

NO.	DATE	REMARKS

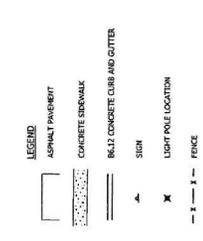
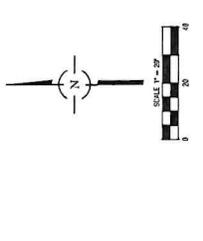
PRELIMINARY GEOMETRIC PLAN
3434 CENTRAL STREET
KENSINGTON SCHOOL
 EVANSTON, ILLINOIS

9279 W. Kedzie Road, Suite 200,
 Rosemead, IL 60450
 Phone: (815) 896-0850 Fax: (815) 896-0855



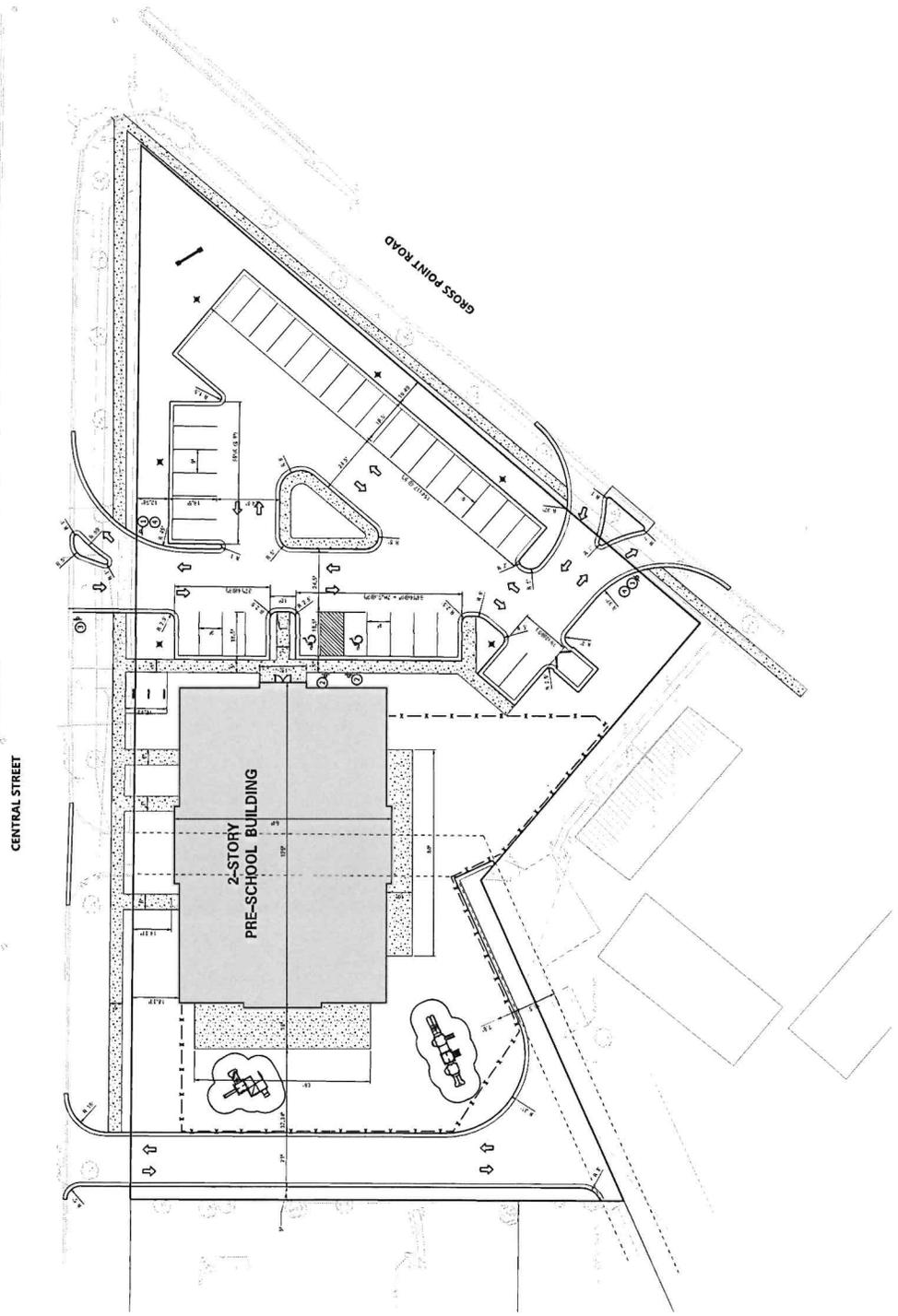
CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

DATE: 02/27/23
 JOB NO.: 111541
 SHEET: P-GM
 3 OF 5



PARKING SUMMARY:

ASPHALT	2
ACCESSIBLE	2
TOTAL:	35



UNITS: 1000000

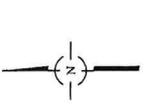
NO.	DATE	REMARKS

NO.	DATE	REMARKS

PRELIMINARY GRADING PLAN
3434 CENTRAL STREET
KENSINGTON SCHOOL
 EVANSTON, ILLINOIS

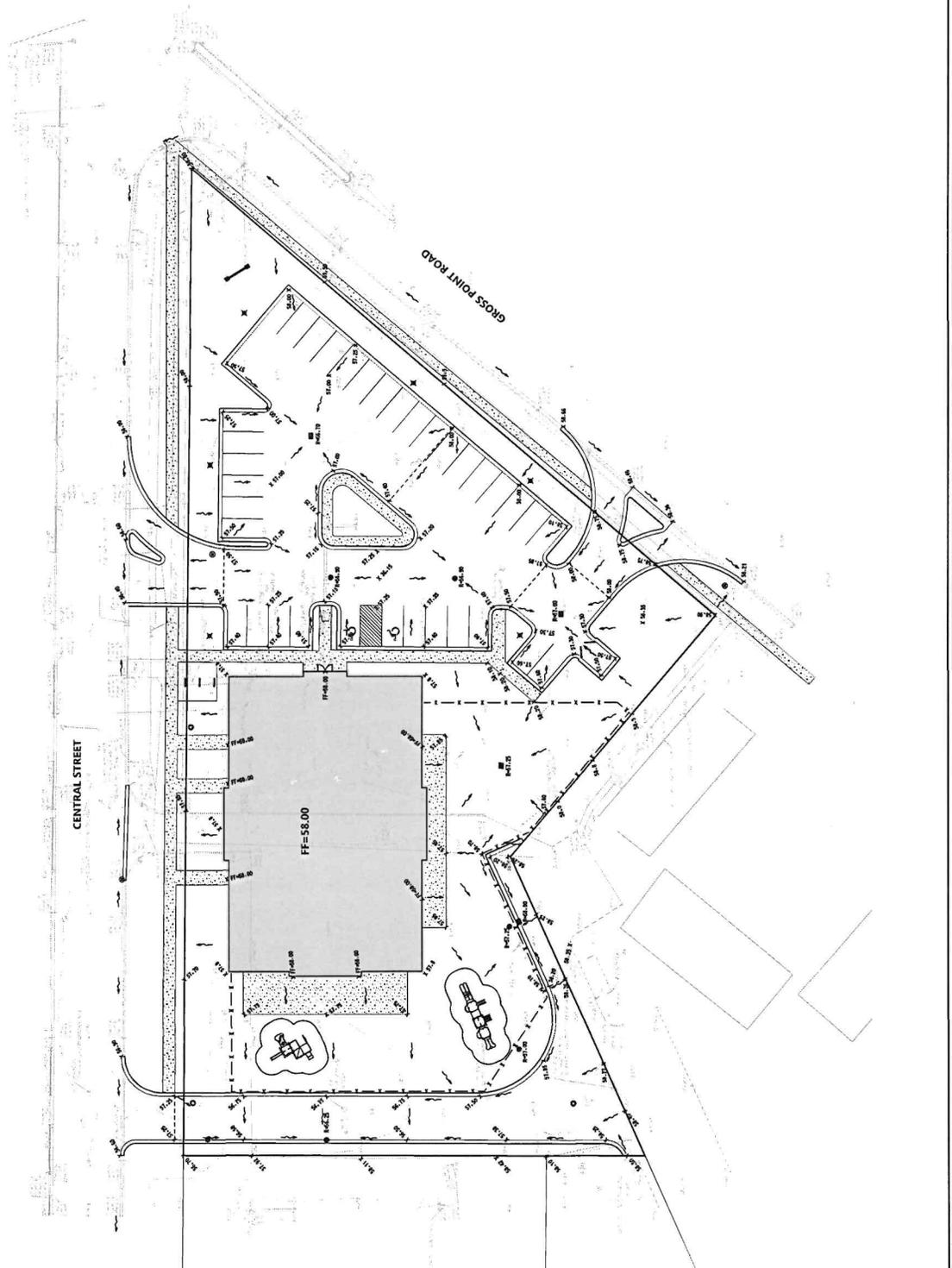
CONSULTING ENGINEERS
 LAND SURVEYORS
 5575 N. PULASKI ROAD, SUITE 100
 CHICAGO, ILLINOIS 60630
 PHONE: (847) 696-0000 FAX: (847) 696-1000

SPACECO INC.
 PROJECT: 3434-CENTRAL
 DATE: 02/20/03
 JOB NO.: 03001
 SHEET: P-GR
 4 OF 5



- LEGEND**
- FF FINISHED FLOOR
 - LOCAL DRAINAGE
 - OUTFALL FLOW ROUTE
 - LOCAL DRAINAGE DIVIDE

NOTES:
 1. ALL DIMENSIONS GIVEN IN FEET AND INCHES UNLESS NOTED OTHERWISE.
 2. ALL SPOUT GRABBER JUNCTIONS COME LINE AND ARE TO BE LOCATED AT THE END OF PARALLEL LINES UNLESS NOTED OTHERWISE.

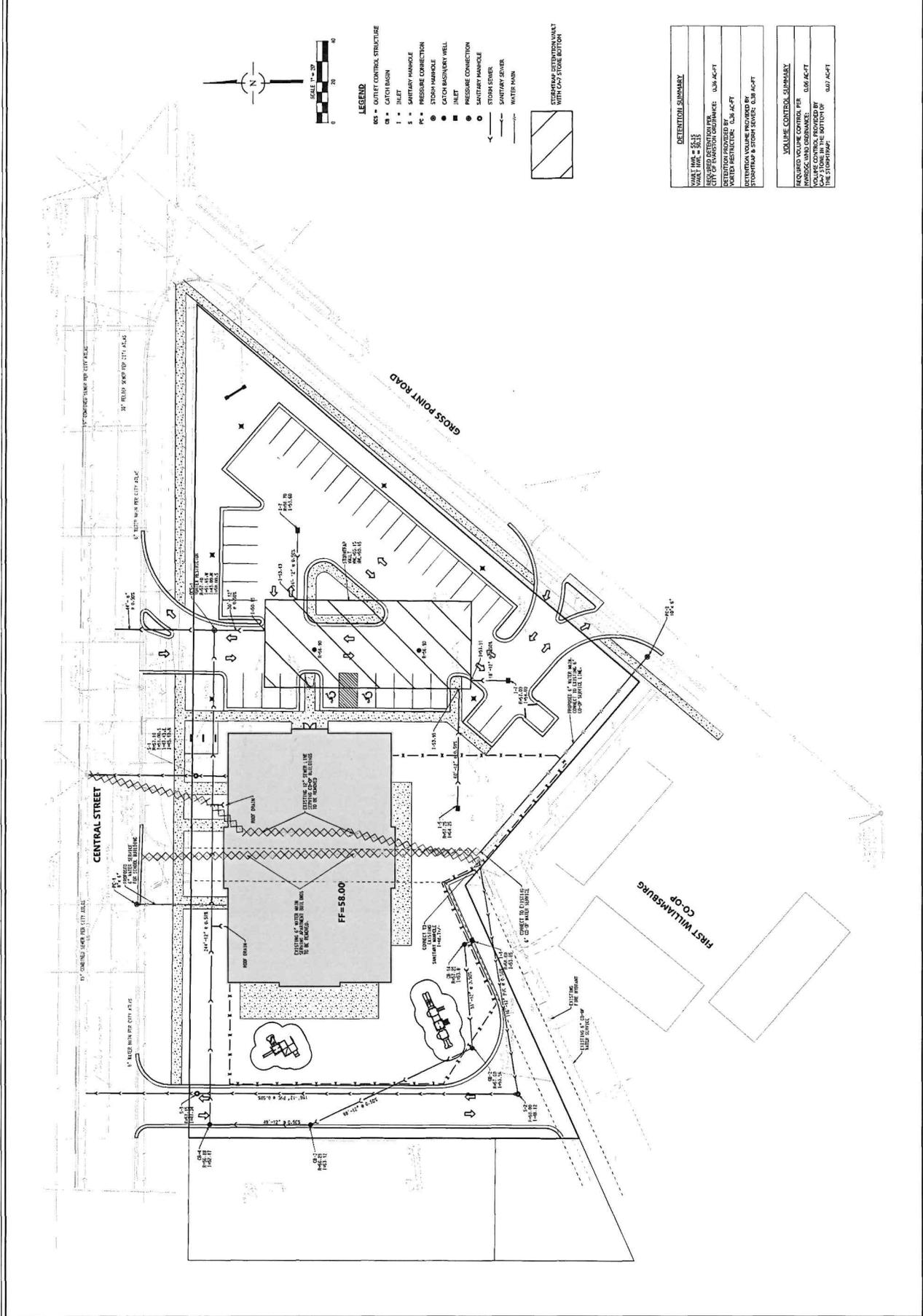


SPRAGGS INC.
 CONSULTING ENGINEERS
 11411 P-UT
 DATE: 10/20/23
 SHEET: 5 OF 10

PRELIMINARY UTILITY PLAN
3434 CENTRAL STREET
KENSINGTON SCHOOL
 EVANSTON, ILLINOIS

NO.	DATE	REMARKS
1	10/20/23	PER CITY OF EVANSTON COMMISSION REVIEW
2		4-10-23 LAND USE

NO.	DATE	REMARKS



DETECTION SUMMARY

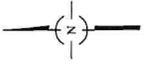
VALVE MARK = 50-15	0.00 ACFT
REQUIRED DETENTION PFR	0.00 ACFT
CITY OF EVANSTON ORDINANCE	0.00 ACFT
DESIGN DETENTION PFR	0.00 ACFT
ACTED DETENTION PFR	0.00 ACFT
STORMWATER VOLUME REQUIRED BY STUDYAREA & STORM SEWER	0.00 ACFT

VOLUME CONTROL SUMMARY

REQUIRED VOLUME CONTROL PFR	0.00 ACFT
PROPOSED VOLUME CONTROL PFR	0.00 ACFT
DESIGN VOLUME CONTROL PFR	0.00 ACFT
ACTED VOLUME CONTROL PFR	0.00 ACFT



- LEGEND**
- 12" = OUTLET CONTROL STRUCTURE
 - 12" = CATCH BASIN
 - 12" = SANITARY MANHOLE
 - 12" = JACKET
 - 12" = PRESSURE CONNECTION
 - 12" = STORM MANHOLE
 - 12" = JACKET BURNOUT WELL
 - 12" = PRESSURE CONNECTION
 - 12" = SANITARY MANHOLE
 - 12" = STORM SEWER
 - 12" = SANITARY SEWER
 - 12" = WATER MAIN



STORMWATER DETENTION VAULT WITH 100% STORAGE CAPACITY

12" SANITARY SEWER MAIN

12" STORM SEWER MAIN

12" WATER MAIN

12" GAS MAIN



Re: [EXTERNAL EMAIL] RE: Unity v. First Williamsburg

From Melissa Colson <mcolson@zabellaw.com>

Date Thu 5/15/2025 4:06 PM

To LaVon M. Johns <LJohns@rshc-law.com>

Cc Jeff Sanchez <jsanchez@zabellaw.com>

Good afternoon,

This email shall serve as notice that Purchaser has elected to exercise their option to extend the Zoning Approval Period to July 20, 2025.

Please do not hesitate to contact me if you have any questions.

Warm regards,

Melissa Colson

Attorney

E-mail Address – mcolson@zabellaw.com

Zabel Law LLC.

55 W Monroe, Ste 3330

Chicago, IL 60603

[312-201-9800](tel:312-201-9800)

[FAX 312-201-1436](tel:312-201-1436)

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This e-mail or any attachments may contain viruses or other illicit code, may have been modified without our consent, and except as may be provided above, does not reflect an intention to conduct a transaction or make an

From: Melissa Colson mcolson@zabellaw.com
Subject: 3434 Central St, Evanston, IL
Date: October 6, 2025 at 6:40 PM
To: LaVon M. Johns LJohns@rshc-law.com

Good afternoon,

This email shall serve as notice that Purchaser has elected to exercise their option to extend the Zoning Approval Period by sixty (60) days, through December 6, 2025.

Kindly confirm receipt and acknowledgement of this notice when available.

Warm regards,

Melissa Colson
Attorney

E-mail Address – mcolson@zabellaw.com

Zabel Law LLC.
55 W Monroe, Ste 3330
Chicago, IL 60603

312-201-9800

FAX 312-201-1436

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Verification of Zoning Approval Period Extension – 3434 Central Street

1 message

Kurt Condra <revkurt@unityns.org>

Tue, Dec 30, 2025 at 3:20 PM

To: shubbard@cityofevanston.org

Cc: Melissa Smith <MSmith@rshc-law.com>, Chuck Marlas <cmarlas@kensingtonschool.com>, "Montana, Mark (Avison Young - US)" <mark.montana@avisonyoung.com>, "LaVon M. Johns" <ljohns@rshc-law.com>

Dear Mr. Hubbard,

This correspondence pertains to Item 65 in your letter dated December 22 to Chuck Marlas concerning the Planned Development Application for 3434 Central Street (Kensington School), attached for reference.

Also attached is the Dec. 5 email from Melissa Colson confirming that the purchaser, Kensington, has exercised its option to extend the Zoning Approval Period by sixty (60) days, through February 3, 2026.

Please accept this message as formal verification that Unity on the North Shore, as seller, has reviewed and approved this extension.

Kindly confirm receipt and acknowledgment of this notice at your convenience. Should any additional information or documentation be required from Unity on the North Shore, please advise.

Respectfully, kdc

--

Rev. Kurt Condra (he/him)

Senior Minister

Email revkurt@unityns.org

Website unityns.org

Cell 214-403-5933 (text or call)

Office 3434 Central St., Evanston, IL 60201

Schedule a time to connect with me. [CLICK HERE](#)

----- Forwarded message -----

From: Melissa Colson <mcolson@zabellaw.com>

To: "LaVon M. Johns" <LJohns@rshc-law.com>

Cc:

Bcc:

Date: Fri, 5 Dec 2025 16:58:01 -0600

Subject: RE: 3434 Central St, Evanston, IL

Good afternoon,

The Purchaser has not been granted Zoning Approval at this time, as such this email shall service as notice that Purchaser has elected to exercise their option to extend the Zoning Approval Period by sixty (60) days, through February 3, 2026.

Kindly confirm receipt and acknowledgement of this notice when available.

Warm regards,

Melissa Colson

Attorney

E-mail Address – mcolson@zabellaw.com

Zabel Law LLC.

55 W Monroe, Ste 3330

Chicago, IL 60603

312-201-9800

FAX 312-201-1436

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This e-mail or any attachments may contain viruses or other illicit code, may have been modified without our consent, and except as may be provided above, does not reflect an intention to conduct a transaction or make an agreement, does not satisfy the requirements for contract formation or a writing, and does not constitute a contract or electronic signature under any applicable law governing electronic transactions.

From: Melissa Colson
Sent: Monday, October 6, 2025 6:40 PM
To: 'LaVon M. Johns' <LJohns@rshc-law.com>
Subject: 3434 Central St, Evanston, IL

Good afternoon,

This email shall service as notice that Purchaser has elected to exercise their option to extend the Zoning Approval Period by sixty (60) days, through December 6, 2025.

Kindly confirm receipt and acknowledgement of this notice when available.

Warm regards,

Melissa Colson

Attorney

E-mail Address – mcolson@zabellaw.com

Zabel Law LLC.

55 W Monroe, Ste 3330

Chicago, IL 60603

312-201-9800

FAX 312-201-1436

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does not satisfy the requirements for contract formation or a writing, and does not constitute a contract or electronic signature under any applicable law governing electronic transactions.

2 attachments



3434 Central - Round 3 PRO Review Comment Letter_threat_extracted[98].pdf
321K



RE_ 3434 Central St, Evanston, IL .eml
15K

Law Offices of

ZABEL LAW, LLC

55 WEST MONROE • SUITE 3330 • CHICAGO, ILLINOIS 60603 • TELEPHONE: 312/201-9800 • FAX: 312/201-1436

MELISSA J. COLSON

Email Address: mcolson@zabellaw.com

December 30, 2025

RE: UNITY CHURCH ON THE NORTH SHORE to CHARLES MARLAS; SALE OF 3434 N. CENTRAL AVE, EVANSTON, ILLINOIS

To Whom It May Concern,

This letter serves to confirm that, as of the date hereof, the contract referenced above:

1. Remains in full force and effect.
2. The Purchaser has elected to extend the Zoning Approval Period through February 3, 2026.
3. Neither party has made any claims that the contract is void or attempted to terminate the contract for any reason.

Please feel free to contact my office should you require any additional information or clarification.

Sincerely,

Melissa Colson

Melissa Colson

6. Project Narrative



Section 1 **Project Narrative**

Kensington School of Evanston
3434 Central Street

Kensington School Administration Office

743 McClintock Drive
Burr Ridge, IL

Kensington School LaGrange, IL

Kensington School Of Hinsdale Hinsdale, IL

Kensington School Lincoln Park Chicago, IL

Kensington School of Glenview Glenview, IL

Kensington School of the Highlands LaGrange, IL

Kensington School of Western Springs Western Springs, IL

Kensington School of St. Charles St. Charles, IL

Kensington School of Geneva Geneva, IL

Kensington School of Naperville Naperville, IL

Kensington School of Wheaton Wheaton, IL

Kensington School of South Naperville Naperville, IL

Kensington School of Elmhurst Elmhurst, IL

Kensington School Arlington Heights Arlington Heights, IL

Kensington School of Evanston Evanston, IL

Introduction

Founded in 1969 and nationally recognized for our excellence in early childhood education, Kensington School is a private nursery, preschool and kindergarten serving more than 2,000 children and families throughout the Chicago area. Throughout thirteen locations, our innovative curriculum, inspirational teaching and beautiful classroom environments make Kensington School a distinctive first school experience for each child.

Curriculum at Kensington School, at every developmental level and in every classroom, is at the heart of what we do. We are entrusted with children during the most receptive period of their development, their first six years, and have developed unique and comprehensive curriculum components to stimulate and educate young minds at every age.

“Growing Up Kensington” sets the stage for a joyful journey of learning. Children’s laughter and excitement, their anticipation of what each day at Kensington School will bring, is what sets Kensington School’s curriculum apart. Kensington School’s curriculum is traditional and academic at its core, innovative in creation and forward thinking in educational philosophy. Barbara Marlas, Kensington School’s founder, has set a standard in early childhood education for purposeful learning at its best.

From the drawing board to completion, a Kensington School building provides the highest standards of safety, comfort, and security for each child. Our school buildings are a well-blended combination of cheerful, individual classrooms and common areas such as the gym, library, roundabout and play yards enjoyed by all ages. Above all, it is a welcoming and home-like place that children instantly recognize as their own.

We are very pleased to be offered the opportunity to bring Kensington School to the City of Evanston and look forward to being a part of the community for many years to come.

Proposed Development

The proposed development at 3434 Central Street, is a 2-story, 19,952 square foot, Day Care Center for children. The site, located at the corner of Central Street and Gross Point Road, is 57,942 square feet. Off-street surface parking is provided at the northeast corner of the property. The number of parking spaces proposed meets the zoning requirement of 38.

Requested Zoning Approvals

The site is in the R2 district. A special use approval is required for Day Care Center – Child. The proposed development will also require site development allowances for: parking in a front yard, and an elimination of one landscape strip.

7. Applicant Response to PD Approval Standards

Statement of Public Benefits (revised 1-02-26)

Kensington School does request a variety of zoning relief and therefore proposes public benefits to ensure the best possible outcome for the community of Evanston. Following discussions with neighboring property owners, community members, city staff, and elected officials, the below list of public benefits is proposed.

In addition to these public benefits, the applicant team notes the property is currently tax exempt with ownership by a religious institution, and the redevelopment for Kensington estimates the return of a six-figure property tax bill per year. While the Kensington School property will then use limited city services such as sewer and water utilities, a significant portion of the new tax bill will go to the school districts without adding any district services/students.

Also, Kensington School aims to provide a needed service for the Evanston community. As nearly any local parent of a preschooler or toddler in the Evanston area can tell you, daycare and preschool spots are competitive and in short supply. This may be exacerbated in the near future if the Head-Start program at District 65 is reduced or eliminated due to funding shortfalls.

Public Benefits:

1. A new public sidewalk will be constructed along the south side of Central Street from the traffic intersection to the western property edge.
2. \$25,000 contribution for new/repared public sidewalks in the area.
3. A new private drive, dedicated solely to the residential Co-op to the south, will be installed within an access easement so that a shared drive is no longer needed.
4. Utility upgrades will reposition underground utilities onto each correct property and separate the connections so that shared connections across property lines no longer exist.
5. Creation of an ongoing scholarship program for reduced tuition for existing Kensington families who encounter economic hardships.
6. Reduced tuition for sustainable drop-off/pick-ups to discourage vehicle usage in the area (\$40 off per month when a vehicle is used less than 25% of the time).

If the community feels different public benefits are preferred and voice those suggestions during the Land Use Commission public hearing or after, the Kensington team is amenable to modification of the public benefits listed above.

Statement of Relationship to Comprehensive General Plan (revised 10-17-25)

The development is in compliance with the *Evanston Comprehensive General Plan*, adopted in 2000.

In compliance with the guiding principles of the Comp Plan, the development will positively contribute to the overall social, economic and physical quality of Evanston, as set forth below.

Enhance Evanston Neighborhoods

The development is located in an R2 district with single family homes to the north and west, and multi-unit housing to the south and east. A business district is directly northeast of the property. The child care center offers early childhood education for local families, thus providing a vital neighborhood service supporting Evanston's working families the ability to be more productive in the workforce by providing an innovative curriculum, inspirational teaching and beautiful classroom environments each day while parents are at their place of business or working from home.

The church structure is nearing the end of its life cycle. Along with the new Kensington School building, site improvements including new public sidewalks, street trees, and new curb cuts with appropriate traffic calming such as left-turn in only and right-turn out only raised curbs along Central Street ensure the existing neighborhood is further enhanced. Additionally, the proposed public benefit of a \$25,000 contribution for public sidewalks will further aid in enhancing the surrounding neighborhood.

Another noted objective of the Comp Plan is the mixing of residential and compatible institutional uses such as daycare facilities or schools within neighborhoods. With residential in nearly all directions, 3434 Central Street is an exceptional location for Kensington School.

Traffic Management, Bicycles and Pedestrians

A traffic study was performed to consider any traffic impacts in consideration of the new facility.

Careful consideration was given to determine the best vehicular access to the site both along Green Bay Road and Central Street.

The restricted curb cuts will ensure proper circulation in and out of the site and will restrict vehicles from navigating west of the property along Central Street. Building operations are weekday daytime hours only, and traffic to and from the site is generated at staggered times in the morning and late afternoon as family drop-off and pick-up times vary.

Although the development is not located in a high density area, the design of the project ensures that pedestrians and bicyclists have easy access to the site and building. Bike parking is available, and sidewalks connect all parking areas and parkway sidewalks to the building.

Statement of Compliance with Zoning Ordinance and Other Pertinent City Policies (revised 10-17-25)

The development is in substantial compliance with the Zoning Ordinance of Evanston and other pertinent planning and development policies. The Purpose and Intent of the Evanston Zoning Code (6-1-2) is:

Promoting the public health, safety, comfort, morals, convenience, general welfare, and the objectives and policies of the comprehensive general plan, as adopted and amended, from time to time, by the City Council; Kensington School is an institutional use that provides a needed service to the area.

Securing adequate light, pure air, and safety from fire and other dangers; The development will meet all fire and life-safety requirements including a building sprinkler system. The building is buffered by land on all sides.

Conserving and enhancing the taxable value of land and buildings throughout the City; Kensington School is a for-profit institution that will put the current non-profit (Unity Church) property back on the property tax roll thereby substantially increasing the taxable value of land and buildings.

Dividing the entire City into districts and restricting and regulating therein the location, construction, reconstruction, alteration, and use of buildings, structures, and land, whether for residential, university, business, industrial, or other specified uses; The use of the site for Kensington School and the proposed site layout are appropriate when considering operations and the best overall development plan for the community.

Minimizing or lessening congestion in the public streets; Substantial consideration has been given to the surrounding traffic given the intense intersection at Gross Point and Central Street. Therefore, the Gross Point entry point is restricted by raised curb cut to allow a right-in and right-out only. Additionally, the Central Street entry point is restricted by raised curb cut to allow a left-in and right-out only to deter vehicles from continuing further west on Central Street. With these traffic controls, vehicles of Kensington families are unlikely to travel through neighborhood streets to get to the facility. Additionally, Kensington School will include language in their family handbook that prohibits traveling west on Central Street to cut through neighborhood streets.

Preventing the overcrowding of land by regulating and limiting the height and bulk of buildings hereafter erected, as said buildings relate to land area; The development complies with height and bulk zoning requirements.

Establishing, regulating, and limiting the building or setback lines on or along streets, alleys, and property lines; The development complies with building setbacks.

Regulating and limiting the intensity of the use of lot areas, and regulating and determining the area of open spaces between and among the surrounding buildings; The intensity of the Kensington School use is appropriate given the substantial lot size and distance from other residential areas. The intensity is further controlled by the Planned Development and Special Use. The area of open space is compliant in relation to building setbacks and building lot coverage.

Statement Addressing Site Controls and Standards (revised 10-17-25)

Minimum Area:

The proposed gross building area 19,952 SF, above the required 10,000 SF minimum for a planned development in the R2 district. The lot area of the planned development is 57,942 SF, above the required minimum lot size of 5,000 SF for a planned development in the R2 district.

Tree Preservation:

There are twenty (20) existing trees on the property. All of these trees cannot be preserved as a few fall within the proposed building and site improvement area. As part of the proposed landscape design, approximately 13 new trees will be planted on the property. See Tree Preservation Plan for details.

There are sixteen (16) existing parkway trees in the right-of-way. Four of the parkway trees will not be preserved as they fall within the curb cut/parking access locations. The remaining 12 parkway trees will be at least 12 feet beyond the perimeter construction fence and will be protected as necessary during construction.

Landscaped Strip:

Landscape strips 10 feet wide are required on the west (rear yard) and south (interior side yard) sides. The west landscape strip is provided, but is only 5 feet wide because the area along that west property line is needed to accommodate the Co-op's new 20 foot wide private driveway that is on Kensington's property. The south landscape strip is provided. A development allowance is sought for the elimination/reduction of the west side landscape strip.

Walkways:

The existing public sidewalks in the parkway along Central Street and Gross Point Road will remain and will be repaired/replaced as necessary. New sidewalks will connect the building's main entrance and other ground floor exits. Sidewalks are also planned between the building and parking areas. A new sidewalk extension will be provided along Central Street to the west as a public benefit.

Parking and Loading:

The parking area has been planned with the City's input and is located at the east side of the property within the pie shaped area. Access at Gross Point Road is right-in and right-out only; and access at Central Street is right/left-in and right-out only. The parking area will serve both the families of the children attending the school and employees. A development allowance is sought for the elimination of the loading berth since a loading berth is not needed for the operation of Kensington School.

Utilities:

The proposed planned development will provide underground utilities, including water, storm sewer, sanitary sewer, natural gas, electric, and data.

Stormwater Treatment:

The development will comply with all stormwater regulations and MWRD restrictions. Stormwater management will include a stormwater detention vault under the parking area with a permeable bottom to further absorb stormwater on site, grading, gutters, and French-drain piping to contain as much stormwater on-site as possible.

Statement of Compatibility with the Surrounding Neighborhood (revised 10-17-25)

Kensington School's building will be new construction and state of the art utilizing the most current energy and environmental practices while also utilizing architectural elements and building materials that complement the neighborhood. Its facade will incorporate red brick, limestone and white trim elements designed to reflect traditional school buildings that currently are seen throughout Evanston.

The building will be designed and built to LEED Gold or equivalent, and will be a mostly-electric building to comply with the Stretch Energy Code and Evanston's Healthy Buildings Ordinance and lead the way among building sustainability in the neighborhood.

The school's placement on a corner with strong vehicular access along with controlled access at Central Street will allow families to come and go with ease without cutting through neighborhood streets.

Parking has been relocated to the corner of the property to ensure there is no impact on the surrounding residential properties to the south and west. A new access easement and driveway will be installed along the western edge of the property for the sole use of the Cooperative to the south that also provides access to the Co-op's existing parking area.

It is appropriate for compatible institutional uses such as daycare facilities to locate adjacent to residential neighborhoods since the proximity and location is often a primary factor when a family chooses a daycare.

Statement of Compatibility with Design Guidelines for Planned Developments (revised 10-17-25)

The proposed development, a 19,952 SF 2-story child day care center, is compatible with the “Design Guidelines for Planned Developments”, adopted in 2006.

The project is designed in accordance with the Design Guidelines and will contribute to the City of Evanston’s tradition in design excellence. The project will deliver a high-quality design that compliments the neighborhood.

The building is sited in a logical position, bounded by Central Street to the north and Gross Point Road to the east, with the main entrance facing east. Special attention is given to the Central Street facade as well so that there are no large expanses or voids. The parking is situated at the pie shaped portion of the property to the east so that it is as far from the residential uses as possible, and so that the building is sited away from the traffic intersection to ensure appropriate vision clearance.

The building’s massing is broken by several vertical planes on all facades. The materials of these planes alternate between the two main exterior materials, red brick and white synthetic wood panels. Along with the detailing of exterior materials, the scale of the building is further enhanced by the window size and fenestration.

The project has been thoroughly planned and designed in relation to parking, landscaping, utilities, underground stormwater detention, security, access and circulation.

The building and site design are currently under review by Design Evanston and an endorsement from the group is expected.

Statement of Development Allowances for Planned Developments (revised 10-17-25)

Off-Street Parking within the Front Yard and (Street) Side Yard, and not within the Rearmost 30 Feet

Parking is not allowed within a front or street side yard in the R2 District. The Kensington School parking lot is located in the triangular area of the property because that is the most appropriate location for the parking. The parking area is situated as far away from the residential uses as possible, and the parking lot allows for greater vision clearance around the Gross Point and Central traffic intersection. Additionally, this site layout allows for a safe and semi-private playground area in the rear yard.

Relief is also requested for the parking lot location. Historically, individual open off-street parking spaces shall be within the rearmost 30 feet of a residential lot, but parking lots were not considered within that regulation since a single parking module is larger than 30 feet which makes compliance impossible. Regardless, the Kensington School parking lot is not within the rear yard since the rear yard is the area closest to residential neighbors. In this instance, the front yard parking lot is more appropriate than a rear yard parking lot, so relief is requested.

Landscape Strip

A 10 foot landscape strip is required along the west rear yard. A landscape strip that is generally 5 feet wide is proposed. The landscaping is minimized because of the new private drive for the Co-op that is located adjacent to the landscape strip and west property line. The easement for the private drive is 20 feet wide so altogether there is a buffering distance of 25 feet that is on Kensington property but is not used by Kensington School. A 6 foot tall solid fence between the drive and the playground is planned for safety. Existing trees along the west property line will remain.

Impervious Surface Lot Coverage

A maximum of 55% impervious surface lot coverage is allowed. The site design requests to exceed that specifically because of the substantial private drive along the west side of the property that is for the sole use of the Co-op. The existing impervious surface lot coverage for Unity Church is 69% where 55% is allowed. Kensington School proposes to reduce that to 62% by utilizing less hardscape and incorporating brick pavers where feasible. Furthermore, underground stormwater detention is provided in compliance with MWRD requirements to restrict the stormwater flow to the storm sewer connection. However, the underground stormwater vault will feature a permeable bottom so that held stormwater can further infiltrate into the ground. When considering the 4,350 sq. ft. underground vault's permeable base, the overall impervious surface lot coverage reduces to 55% (fully compliant)..

Furthermore, if the property is reviewed without the western portion of the lot where the private driveway for the Co-op exists, the remaining Kensington area's impervious surface lot coverage is 58% at grade, and reduces further to 50% (fully compliant) when adding in the permeable underground vault.

Accessory Structures

The "southern patio" is a compliant at-grade terrace that is considered part of the principal structure and should be permitted since it is within the building envelope and attached to the building. The patio/terrace is not within any setback. However, if this use/structure is deemed noncompliant within that location where a 2.5 story building could be instead, then relief is requested. The patio/terrace is needed for the most efficient operation of Kensington School. The "refuse enclosure" is not an accessory use and is not listed as an accessory use in 6-4-6-3, but is a fence as noted below for relief. If this fence is also considered an accessory use that is not permitted, the enclosure could be eliminated to achieve compliance or the relief could be granted.

Fencing

Fencing is not permitted at 6 feet in height within a front or street side yard. 6 foot tall fencing is proposed around the playground for safety and security, and as a dumpster enclosure for improved aesthetics of the property, though the dumpster enclosure could be eliminated to reduce the zoning relief.

Statement of Development Allowances for Planned Developments (revised 12-08-25)

Drive aisle width adjacent to First Williamsburg Co-op

A 16.5' wide drive aisle for the Co-op parking stalls matches the existing condition of a 16' wide drive aisle. The drive aisle does not lead to anywhere else, and is an access drive to the parking, more so than a parking lot. Since the vehicles previously maneuvered into and out of the parking spaces and along the driveway without difficulty, maintaining the 16' width is appropriate and is akin to a typical alley width where such maneuvering is regularly accomplished. Increasing to a 24' width is not necessary for functionality and would further increase the impervious surface coverage at the property, increase stormwater detention/runoff, and decrease greenspace/landscaping.

8. Neighborhood Outreach Documents

MK Development Advisor, LLC

Melissa Klotz
(815) 354-7959

August 8, 2025

I'M SORRY I MISSED YOU!

I walked your neighborhood today to speak to surrounding residents about the pending planned development request for Kensington School at 3434 Central Street. I would love to hear from you and get your feedback!

The Past: Kensington School attempted to gain approval to construct a daycare at 3434 Central Street but was unsuccessful, mostly due to a variety of traffic circulation issues.

The Present: A variety of changes and improvements to traffic circulation have been identified and Kensington School seeks input from the neighbors surrounding 3434 Central Street.

The Future: Kensington School hopes to proceed through Evanston's planned development process successfully with conditions that ensure traffic circulation is appropriate.

As a past long-time City of Evanston employee and the former Zoning Administrator, I am familiar with the history of 3434 Central Street. Kensington School is a moderate-intensity proposal on a large property that is surrounded by high traffic streets. Kensington is likely to be far less intense compared to a future high-intensity use the Envision Evanston process has identified for that exact site - which allows for subdivision into 12 lots, each with a 50 foot tall apartment building with as many dwelling units as possible (easily 10 or more). In total that is 12 new 50 foot tall buildings and 120 or more new dwelling units - plus all of those vehicles, deliveries, dumpsters, etc.

Kensington School has identified the following solutions for traffic circulation:

- 1. Restricted vehicular access from Central Street: a raised landscaped curb that prohibits vehicles from entering or exiting on Central from/to the west via Princeton or Greeley Streets.**
- 2. Additional traffic control including signage, ground striping, and cameras.**
- 3. Wording within the Kensington School Handbook that clarifies any family who does not follow the restricted access from Central Street is subject to disenrollment from the daycare.**

What other concerns - about traffic circulation or anything else related to the request do you have? Are there solutions that have not been identified? Neighbor input matters and I would like to hear your thoughts. Please connect with me!

I am assisting Kensington School so that 3434 Central Street redevelops appropriately and with proper conditions that ensure the greater Central Street neighborhood is impacted in a positive way. Please connect with me any time via email at MelissaKlotz1983@gmail.com, or call or text me at 815.354.7959. I look forward to connecting with you.

Thank you,

Melissa Klotz
Development Advisor



Dear Neighbors,

I want to take a moment to thank you for your continued engagement and thoughtful feedback as we explore the opportunity to bring a Kensington School to your neighborhood. I'm writing today to specifically address a concern that many of you have raised — traffic and congestion around the proposed site, particularly regarding turn patterns into and out of the school's parking lot.

We've heard your concerns about the potential for vehicles traveling on Harrison/Old Orchard Rd to turn onto Greeley or Princeton to access the parking lot from the west via Central Street. To mitigate this, we will implement restricted turn access at the lot entrance: **only left turns into the lot from Central Street (coming from the east) and only right turns out of the lot will be permitted. There will be no allowable right turns into the lot from the west.**

We understand that some neighbors are concerned that these restrictions alone may not be enough. Please know that we take this issue very seriously. What distinguishes Kensington School is our deep, longstanding relationship with our families — these are not transient customers like those visiting a coffee shop or convenience store. Our parents are members of a committed school community who understand and respect the expectations we set, and traffic and parking procedures are no exception.

We have **very clear and strict guidelines** in place around drop-off and pick-up procedures at all our schools, and we take compliance seriously. These policies are part of our family handbook and are reviewed regularly with our families. We want to be clear: **any parent found violating our traffic protocols, including making an unsafe right-hand turn from Central Street against the posted restrictions, would be subject to disciplinary action, including possible disenrollment of their child.**

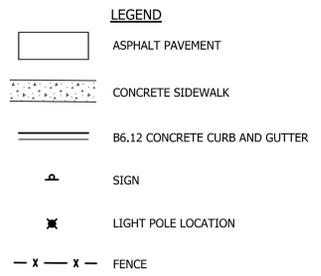
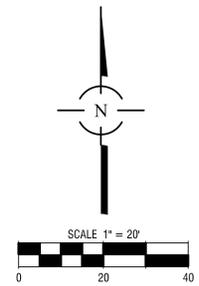
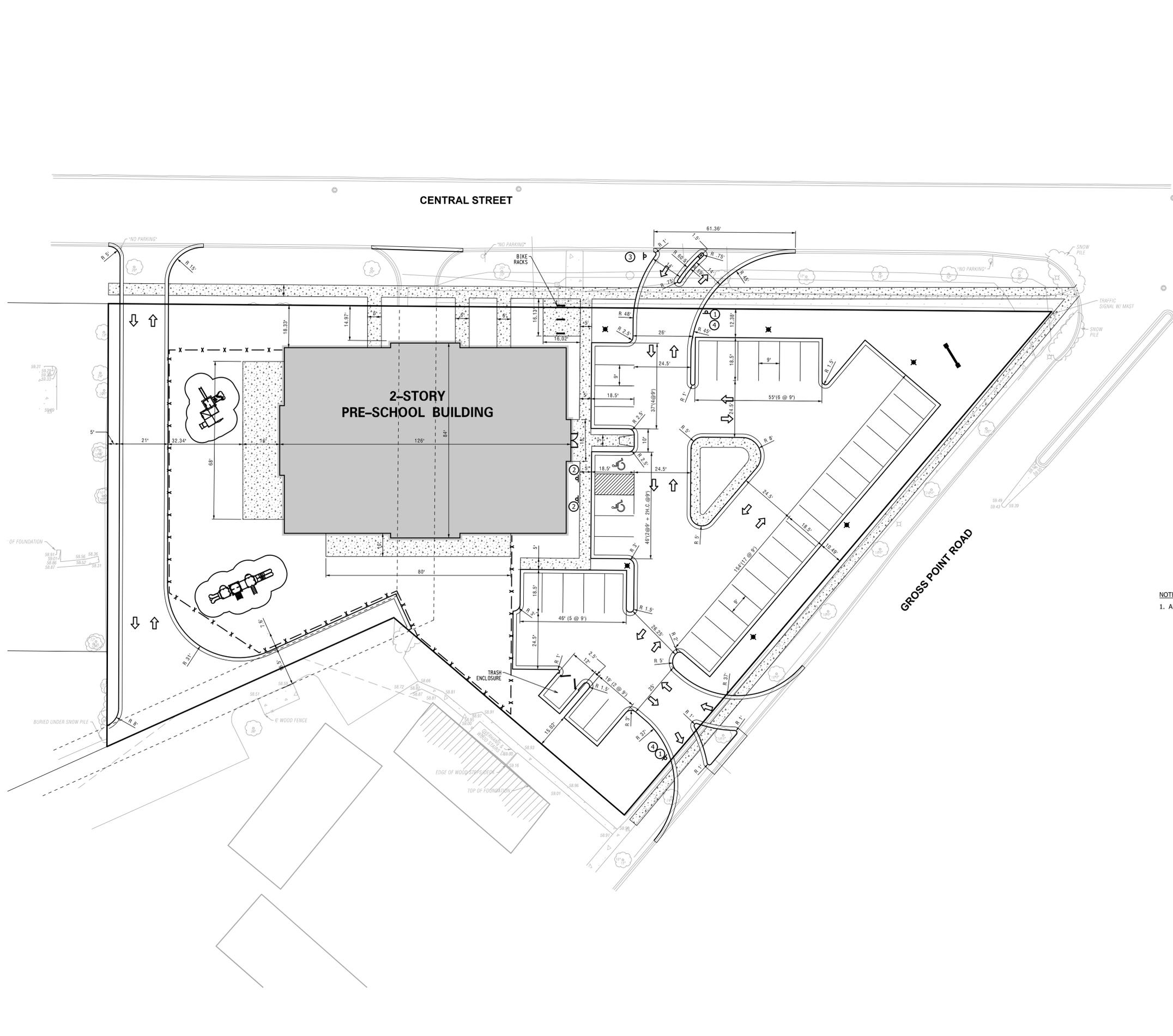
We consider the safety of all children and families, ours and yours, paramount. We are committed to working together to be respectful neighbors, and we will continue to seek input and ensure that our school is a positive addition to the community.

Thank you again for your time and engagement. Please feel free to reach out if you have any further questions or concerns.

Sincerely,

A handwritten signature in black ink that reads 'Charles Marlas'. The signature is fluid and cursive, with a long horizontal stroke at the end.

Charles Marlas
Owner
Kensington School



PARKING SUMMARY

STANDARD:	36
ACCESSIBLE:	2
TOTAL:	38

NOTES:
1. ALL DIMENSIONS ARE TO THE BACK OF CURB UNLESS OTHERWISE NOTED.

NO.	DATE	REMARKS

NO.	DATE	REMARKS
4	07/24/25	PER CITY OF EVANSTON
3	06/16/25	PER CITY OF EVANSTON
2	04/16/25	PER CLIENT

PRELIMINARY GEOMETRIC PLAN
KESINGTON SCHOOL
3434 CENTRAL STREET
EVANSTON, ILLINOIS



FILENAME:	11641P-GM
DATE:	02/27/23
JOB NO.:	11641
SHEET	P-GM
	3 OF 5



9. Preliminary Development Schedule

ID	Task Mode	Task Name	Duration	Start	Finish	Predecess
1	→	ZONING ANALYSIS AND APPROVAL PROCESS	71 days	Mon 5/12/25	Mon 8/18/25	
2	→	Special Zoning Approval Submittal	1 day	Mon 5/12/25	Mon 5/12/25	
3	→	Planned Development and Rezoning Request	10 days	Tue 5/13/25	Mon 5/26/25	2
4	→	Interdepartmental Staff Review	10 days	Tue 5/27/25	Mon 6/9/25	3
5	→	DARP Review and Recommendation for PUD	10 days	Tue 6/10/25	Mon 6/23/25	4
6	→	Plan Commission Recommendation	1 day	Mon 7/14/25	Mon 7/14/25	
7	→	Public Hearing	1 day	Mon 7/14/25	Mon 7/14/25	6SS
8	→	City Council Approval - Option #1	1 day	Mon 7/28/25	Mon 7/28/25	
9	→	City Council Approval - Option #2	1 day	Mon 8/18/25	Mon 8/18/25	
10	→?	Assume total 3 month process				
11	→?	Plan Commission meetings 2nd & 4th Mondays of each month				
12	→?	City Council meetings 2nd & 4th Mondays of each month				
13	→					
14	→	BUILDING PERMIT APPLICATION	27 days	Tue 8/19/25	Wed 9/24/25	
15	→	Complete and submit building permit	1 day	Tue 8/19/25	Tue 8/19/25	9
16	→	DARP (Design and Project Review Committee)	10 days	Wed 8/20/25	Tue 9/2/25	15
17	→	Permit Review	15 days	Wed 9/3/25	Tue 9/23/25	16
18	→	Permit Issuance	1 day	Wed 9/24/25	Wed 9/24/25	17
19	→					
20	→	Land Development	187 days	Thu 9/25/25	Fri 6/12/26	
34	→					
35	→	Foundation	41 days	Thu 10/30/25	Thu 12/25/25	
49	→					
50	→	Framing	110 days	Fri 12/26/25	Thu 5/28/26	
62	→					
63	→	Mechanicals	36 days	Fri 3/6/26	Fri 4/24/26	
71	→					
72	→	Finishes	163 days	Mon 4/27/26	Wed 12/9/26	
101	→					
102	→	Occupancy	46 days	Thu 12/10/26	Thu 2/11/27	

10. Zoning Analysis (2023)



**Zoning Analysis
Summary**

Review Date: 04-12-23
Michael Griffith, Planner

Case Number:

Case Status/Determination:

21ZONA-0188 – 3434 CENTRAL STREET	Non-compliant
-----------------------------------	---------------

Applicant: Charles Marlas

Plans prepared by: The Lauder Group

Plans dated: 04-10-23

Survey dated: 06-01-21

District: R2

Proposal:

Demolish the existing Unity Church North Shore building and other site improvements, construct a new 2-story, 19,952 square foot building for a Daycare Center-Child, Kensington School

Review Comments:

	Property zoned R2 Single-Family Residential
	Zoning lot size: 57,942 sf
6-8-1-10.D.1	A Planned Development is required for new construction, a new structure, on a zoning lot greater than 30,000 sf.
6-8-3-3, 6-3-5-3	A Planned Development is a special use in the R2 zoning district. Special uses require City Council approval.
6-8-3-3, 6-3-5-3	A Daycare Center-Child is a special use in the R2 zoning district. Special uses require City Council approval.
	Both special uses, Planned Development and Daycare Center-Child, are considered together.
	Yards have been determined to be as follows: <ul style="list-style-type: none"> ▪ Front yard: Along Gross Point Road ▪ Street side yard: Along Central Street ▪ Interior side yard: Along south property line ▪ Rear yard: Along west property line
	Illinois Department of Transportation (IDOT) has jurisdiction over the Gross Point Road right-of-way. A permit from IDOT is required for any work in their right-of-way.
	Plat of survey shows an existing ingress/egress easement on the property off Central Street providing access to the First Williamsburg Co-Op dwellings to the south fronting onto Gross Point Road.

	This easement should be vacated and a new easement provided covering the new proposed access of Central Street, and agreed to by the First Williamsburg Co-Op.
6-16-2-8	The photometric plan needs to measure light levels at the property line. Exterior lighting is to be directed so that there is no glare onto abutting public streets and properties. Illumination cannot spill over the property line.
6-8-1-10.B.7	The planned development shall provide, if possible, for underground installation of utilities (including electricity and telephone) both in public ways and private extensions thereof.
	Signage requires a separate permit from a building permit.
	Additional comments may be provided as the review/zoning entitlement process moves forward.

Principal Use and Structure: New principal structure for a Daycare Center-Child	
6-8-3-3	Use: Special Use required Proposed: Daycare Center- Child
6-8-3-5	Lot width: Compliant Standard: 35.0' Existing: 285.3' Proposed: 285.3'
6-8-3-4	Lot size: Compliant Standard: 5,000 sf Existing: 57,942.8 sf Proposed: 57,942.8 sf
	Dwelling Units #: NA
6-8-3-6	Building Lot Coverage: Compliant Standard: 40.0% of lot area, 23,177.1 sf Existing: 12.7%, 7,375.6 sf Proposed: 17.2%, 9,976 sf
6-8-3-9	Impervious Surface Coverage: Non-compliant Standard: 55% of lot area, 31,868.5 sf Existing: 69%, 40,283 Proposed: 60.5%, 3,5071 sf
	Accessory Structure Rear Yard Coverage: NA
	Gross Floor Area: NA

6-8-3-8	<p>Building Height: Compliant</p> <p>Standard: 35.0' not to exceed 2.5 stories Proposed: 28.0' at 2 stories</p> <ul style="list-style-type: none"> ▪ 35.0' to top of parapet
6-8-3-7	<p>Yards (Setbacks):</p> <p>Front, southeast, Gross Point Road: Compliant</p> <ul style="list-style-type: none"> ○ Standard: 27.0' ○ Proposed: 27.0'+ <p>Street Side, north, Central Street: Compliant</p> <ul style="list-style-type: none"> ○ Standard: 15.0' ○ Proposed: 15.0' <p>Interior Side, south: Compliant</p> <ul style="list-style-type: none"> ○ Standard: 15.0' ○ Proposed: 15.0'+ <p>Rear, west: Compliant</p> <ul style="list-style-type: none"> ○ Standard: 30.0' ○ Proposed: 30.0'+
Accessory Use and Structure 1: Fence	
6-4-6-7.F.2	<p>Location (Yard): Compliant</p> <p>Standard: Street side, interior side, and rear yards, located at least 3' behind the front street facing façade.</p> <p>Proposed: Interior side (south) and rear (west) yards and located 3' behind the front street facing façade.</p>
6-4-6-7.F.3	<p>Height: Compliant</p> <p>Standard: 6.0' Proposed: 5'-6.0'</p>
	Distance from Principal Building: NA
	<p>Yards (Setbacks): NA</p> <p>Front:</p> <p>Street Side:</p> <p>Interior Side:</p> <p>Interior Side:</p> <p>Rear:</p>

Accessory Use and Structure 2: Playground Equipment	
6-4-6-3, Table 4-A #23	Location (Yard): Standard: Rear yard Proposed: Rear yard
	Height: NA
	Distance from Principal Building: NA
6-4-6-3, Table 4-A #23	Yards (Setbacks): Front, southeast: NA Street Side, north: Compliant <ul style="list-style-type: none"> ○ Standard: 5.0' ○ Proposed: 5.0'+ Interior Side, south: Compliant <ul style="list-style-type: none"> ○ Standard: 5.0' ○ Proposed: 5.0'+ Rear, west: Compliant <ul style="list-style-type: none"> ○ Standard: 5.0' ○ Proposed: 5.0'+
Accessory Use and Structure 3: Refuse enclosure (detached accessory use/structure)	
6-4-6-3	Location (Yard): Non-compliant Standard: Rear yard Proposed: Front yard
6-4-6-7.F.3	Height, fence/wall enclosing refuse area: Compliant Standard: 6.0' Proposed: 6.0' ▪ Enclosure is a masonry wall on three sides. Material includes modular face brick (red) and limestone cap (gray)
	Distance from Principal Building: NA
	Yards (Setbacks): NA Front: Street Side: Interior Side: Rear:

Parking Requirement	
6-16-2, Table 16-B	<p>Use 1: Daycare Center-Child</p> <ul style="list-style-type: none"> ○ Standard: 1 per employee, 1 per company vehicle, and 1 per 15 children = 34 ○ Proposed: 35 <p>▪ Per applicant: 23 employees, 0 company vehicles, and 165 children</p> <p>Total Required: Compliant</p> <ul style="list-style-type: none"> ○ Standard: 34 ○ Proposed: 35
6-16-2-6, 2018 IL Accessibility Code Sec. 205.1, Table 208.2	<p>Accessible Spaces (ADA): Compliant</p> <p>Standard: 2 Proposed 2</p>
6-8-3-11, 6-16-2-2	<p>Access: Compliant</p> <p>Standard: Street, no alley access Existing: Street, Central Street Proposed: Street, Central Street and Gross Point Road</p>
6-16-2-5	<p>Vertical Clearance: Compliant</p> <p>Standard: 7.0' Proposed: Open to the sky</p>
6-16-2-8	<p>Surface: Compliant</p> <p>Standard: Hard surface Proposed: Asphalt</p>
6-8-3-7.B	<p>Location:</p> <p>Front yard, southeast: Non-compliant</p> <ul style="list-style-type: none"> ○ Standard: Parking prohibited ○ Proposed: Parking located within front yard <p>Street Side yard, north: Compliant</p> <ul style="list-style-type: none"> ○ Standard: Parking prohibited ○ Proposed: Parking not located within street side yard <p>Interior Side yard, south: Non-compliant</p> <ul style="list-style-type: none"> ○ Standard: Parking prohibited ○ Proposed: Parking located within interior side yard <p>Rear yard, west: NA</p>
6-16-2-7, 2018 IL Accessibility Code Sec. 502.2 and 502.3	<p>Parking Angle 1: 90-degree</p> <p>Parking Space Size, 90-degree: Compliant</p> <ul style="list-style-type: none"> ○ Width: 8.5'; 9.0' proposed ○ Length: 18'; 18.0' proposed

	<p>Accessible Spaces (ADA): Compliant</p> <ul style="list-style-type: none"> ○ Parking space width: 8'-11'; 9' proposed ○ Access aisle width: 5'-8'; 9' proposed <ul style="list-style-type: none"> ▪ Two 90-degree accessible parking spaces permitted to share a common access aisle. ▪ Minimum required width for accessible parking space + access aisle is 16'; 18' proposed. <p>Drive Aisle Width, 2-way: Non-compliant</p> <ul style="list-style-type: none"> ○ Standard: 24.0' ○ Proposed: 17.0' <ul style="list-style-type: none"> ▪ 17.0' proposed at driveway aisle adjacent to existing Co-Op parking. ▪ 20.0' wide driveway proposed off Central Street to existing Co-Op parking.
Loading Requirements: NA	
	<p>Use 1:</p> <p>Use 2:</p> <p>Use 3:</p> <p>Total Number of Short/Long Loading Berths:</p>
	Long Berth Size:
	Short Berth Size:
	Vertical Clearance:
	Location:
Miscellaneous:	
6-8-1-10.B.3	<p>Transition landscape strip: Non-compliant</p> <p>Standard: A minimum 10' wide landscape transition strip required along all boundaries not abutting dedicated and improved public streets, required along west and south property lines.</p> <p>Proposed: Landscape strip along west property line 5.5' wide, required landscape strip not indicated along south property line.</p>

11. Co-Op Access Easement Legal Description

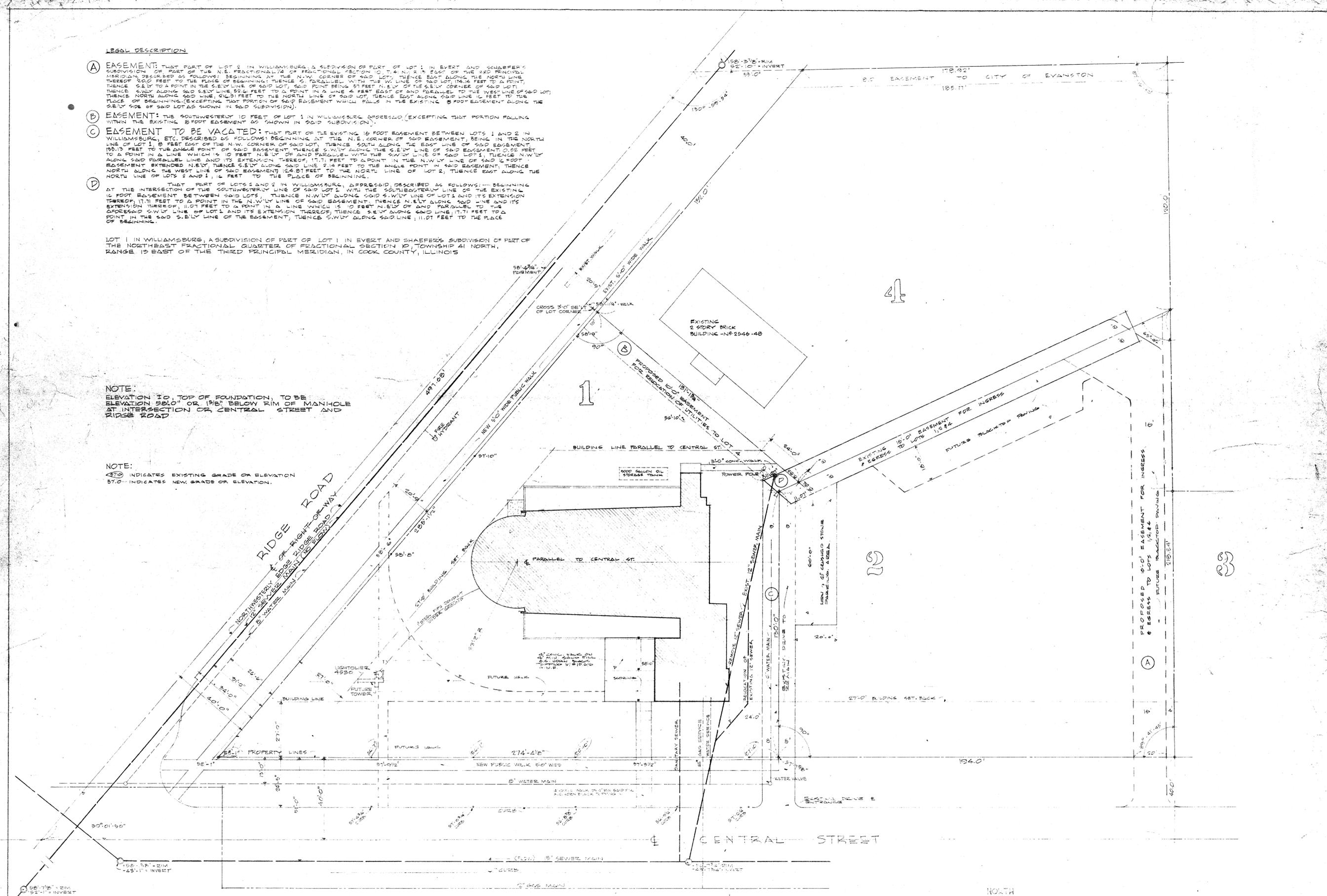
LEGAL DESCRIPTION

- (A) EASEMENT: THAT PART OF LOT 2 IN WILLIAMSBURG, A SUBDIVISION OF PART OF LOT 1 IN EVERT AND SCHAFFER'S SUBDIVISION, BE PART OF THE N.E. 1/4 SECTION 14 OF T.4 N. R.18 E. OF THE 2ND PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS: BEGINNING AT THE N.W. CORNER OF SAID LOT, THENCE EAST ALONG THE NORTH LINE THEREOF 50.0 FEET TO THE PLACE OF BEGINNING; THENCE S. PARALLEL WITH THE W. LINE OF SAID LOT 114.4 FEET TO A POINT; THENCE S.E. TO A POINT IN THE S.E. LINE OF SAID LOT, SAID POINT BEING 31 FEET N.E. OF THE S.E. CORNER OF SAID LOT; THENCE S.W. ALONG SAID S.E. LINE 52.6 FEET TO A POINT IN A LINE 4 FEET EAST OF AND PARALLEL TO THE WEST LINE OF SAID LOT; THENCE NORTH ALONG SAID LINE 16.31 FEET TO THE NORTH LINE OF SAID LOT; THENCE EAST ALONG SAID LINE 1/2 FEET TO THE PLACE OF BEGINNING. (EXCEPTING THAT PORTION OF SAID EASEMENT WHICH FALLS IN THE EXISTING 8 FOOT EASEMENT ALONG THE S.E. LINE OF SAID LOT AS SHOWN IN SAID SUBDIVISION).
- (B) EASEMENT: THE SOUTHWESTERLY 10 FEET OF LOT 1 IN WILLIAMSBURG, A SUBDIVISION OF PART OF LOT 1 IN EVERT AND SCHAFFER'S SUBDIVISION, (EXCEPTING THAT PORTION FALLING WITHIN THE EXISTING 8 FOOT EASEMENT AS SHOWN IN SAID SUBDIVISION).
- (C) EASEMENT TO BE VACATED: THAT PART OF THE EXISTING 16 FOOT EASEMENT BETWEEN LOTS 1 AND 2 IN WILLIAMSBURG, ETC. DESCRIBED AS FOLLOWS: BEGINNING AT THE N.E. CORNER OF SAID EASEMENT, BEING IN THE NORTH LINE OF LOT 1 8 FEET EAST OF THE N.W. CORNER OF SAID LOT; THENCE SOUTH ALONG THE EAST LINE OF SAID EASEMENT 15.13 FEET TO THE ANGLE POINT OF SAID EASEMENT; THENCE S.W. ALONG THE S.E. LINE OF SAID EASEMENT 10.22 FEET TO A POINT IN A LINE WHICH IS 10 FEET N.E. OF AND PARALLEL WITH THE S.W. LINE OF SAID LOT 1; THENCE N.W. ALONG SAID PARALLEL LINE AND ITS EXTENSION, THEREOF, 11.71 FEET TO A POINT IN THE N.W. LINE OF SAID 16 FOOT EASEMENT EXTENDED N.E. TO THE ANGLE POINT; THENCE S.E. ALONG SAID LINE 14 FEET TO THE ANGLE POINT IN SAID EASEMENT; THENCE NORTH ALONG THE WEST LINE OF SAID EASEMENT 12.81 FEET TO THE NORTH LINE OF LOT 2; THENCE EAST ALONG THE NORTH LINE OF LOTS 2 AND 1, 16 FEET TO THE PLACE OF BEGINNING.
- (D) AT THE INTERSECTION OF THE SOUTHWESTERLY LINE OF SAID LOT 2 WITH THE SOUTHWESTERLY LINE OF THE EXISTING 16 FOOT EASEMENT BETWEEN SAID LOTS, THENCE N.W. ALONG SAID S.W. LINE OF LOT 1 AND ITS EXTENSION THEREOF, 11.71 FEET TO A POINT IN THE N.W. LINE OF SAID EASEMENT; THENCE N.E. ALONG SAID LINE AND ITS EXTENSION THEREOF, 11.01 FEET TO A POINT IN A LINE WHICH IS 10 FEET N.E. OF AND PARALLEL TO THE PROPOSED S.W. LINE OF LOT 1 AND ITS EXTENSION, THEREOF; THENCE S.E. ALONG SAID LINE 11.71 FEET TO A POINT IN THE SAID S.E. LINE OF THE EASEMENT; THENCE S.W. ALONG SAID LINE 11.01 FEET TO THE PLACE OF BEGINNING.

LOT 1 IN WILLIAMSBURG, A SUBDIVISION OF PART OF LOT 1 IN EVERT AND SCHAFFER'S SUBDIVISION OF PART OF THE NORTHEAST QUARTER OF SECTION 14, T.4 N. R.18 E. OF THE 2ND PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS

NOTE:
ELEVATION TO TOP OF FOUNDATION TO BE ELEVATION 58.0' OR 5/8" BELOW RIM OF MANHOLE AT INTERSECTION OF CENTRAL STREET AND RIDGE ROAD

NOTE:
ELEVATION INDICATES EXISTING GRADE OR ELEVATION
57.0 INDICATES NEW GRADE OR ELEVATION.



PLOT PLAN SCALE 1/8" = 1'-0"
UNITY CENTER OF CHRISTIANITY - EVANSTON ILLINOIS
 REVEREND MAUD KELLOGG - MINISTER

12. Court Order Summary Judgment - Easement Relocation

IN THE CIRCUIT COURT OF COOK COUNTY, ILLINOIS
COUNTY DEPARTMENT, CHANCERY DIVISION

UNITY CHURCH ON THE NORTH SHORE,

Plaintiff,

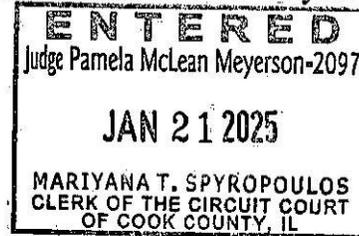
vs.

FIRST WILLIAMSBURG CORPORATION,

Defendant.

Case No. 2023-CH-06757

Honorable Pamela McLean Meyerson

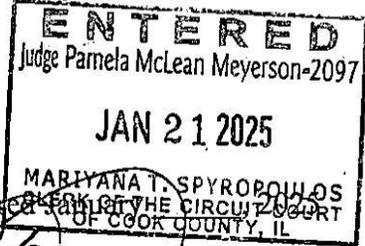


ORDER

This matter came before the Court on January 8, 2025, pursuant to Plaintiff's Motion for Summary Judgment, and the Court being fully advised, entered a final order dated January 16, 2025. Within 30 days, the parties by agreement submitted a revised order for the Court. Having reviewed the proposed revision, the Court revises its January 16, 2025 Order as follows (with amendments in italics). IT IS HEREBY ORDERED:

1. For the reasons stated on the record in open court, Plaintiff's Motion for Summary Judgment is hereby GRANTED. The Easement described in Plaintiff's Complaint and further depicted on Exhibit 1 of Plaintiff's Complaint may be relocated to the cross-hatched area as depicted on Exhibit 1, Site Plan Drawing No. A1-0 of the Complaint, which is approximately eighty feet to the west of its current location (the "Relocated Easement"), because such relocation would not substantially alter Defendant's use of the Easement.

2. Prior to the construction of the Relocated Easement, Plaintiff shall cause the recording of the Relocated Easement in the Cook County Recorder's Office.
During construction, Defendant's right of access to Central will not be interrupted.
3. Judgment as matter of law in favor of Plaintiff is hereby ENTERED.
4. This is a final order disposing of all matters.



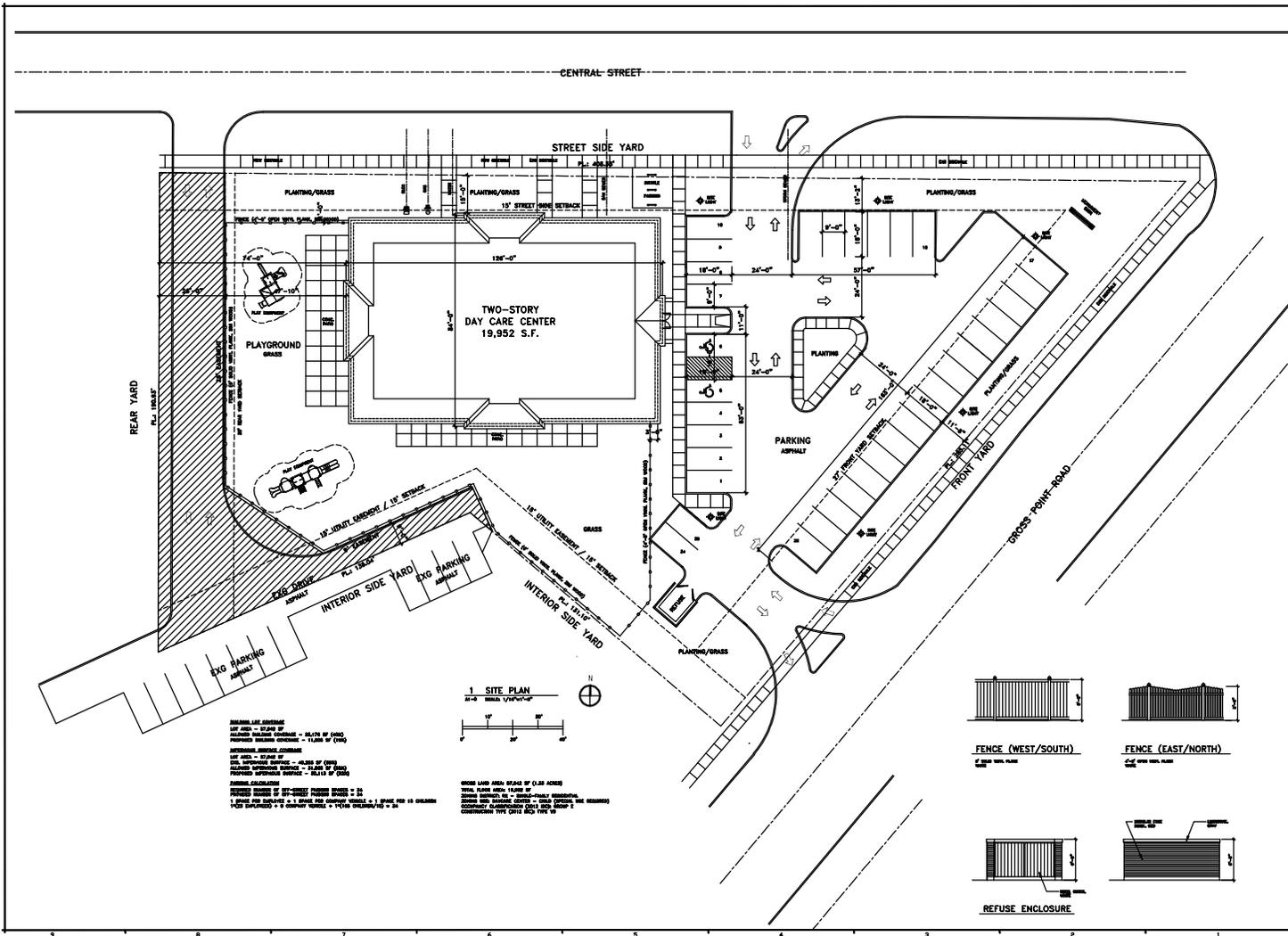
Entered by

A handwritten signature in black ink, appearing to be "Pamela McLean Meyerson", written over the stamp and extending to the left.

Honorable Pamela Mclean Meyerson

Submitted by:

John K Theis
Riley Safer Holmes and Cancila LLP.
70 W. Madison St., Ste 2900
Chicago, IL 60602
Phone: 312.471.8700
Fax: 312.471.8701
Firm I.D. 60128



THE LAUDER GROUP
 743 McCLENDON DRIVE
 BURBANK, IL 60137
 T. 708.666.6666

KENSINGTON SCHOOL OF EVANSTON
 3434 CENTRAL STREET
 EVANSTON, IL 60201

NO. OF SHEETS	1
SHEET NO.	1
DATE	07-11-23
SCALE	AS SHOWN
PROJECT NO.	19-001
CLIENT	LAUDER GROUP
DESIGNER	LAUDER GROUP
DATE	07-11-23
SCALE	AS SHOWN
PROJECT NO.	19-001
CLIENT	LAUDER GROUP

SITE PLAN

A1-0

13. Plat of Survey

LEGEND OF SYMBOLS & ABBREVIATIONS

○ UTILITY POLE	○ MANHOLE	P.O.C. POINT OF COMMENCEMENT
⊛ LIGHT POLE	⊙ SANITARY MANHOLE	P.O.B. POINT OF BEGINNING
⊞ TRANSFORMER	⊙ STORM STRUCTURE (CLOSED)	° DEGREES
⊞ UTILITY PEDESTAL	⊙ STORM STRUCTURE (OPEN)	' FEET/MINUTES
⊞ TRAFFIC SIGNAL	⊙ CURB INLET	" INCHES/SECONDS
⊞ SIGNAL BOX	⊙ VALVE VAULT	S.F. SQUARE FEET
⊞ GAS VALVE	⊙ FLARED END SECTION	(R) RECORD BEARING/DISTANCE
⊞ WATER VALVE	—W— WATER LINE	IF TOP OF FOUNDATION
⊞ ELECTRIC METER	—T— TELEPHONE/CATV LINE	FF FINISHED FLOOR
⊞ GAS METER	—G— GAS LINE	TP TOP OF PIPE
⊞ FIRE HYDRANT	—E— ELECTRIC LINE	B.S.L. BUILDING SETBACK LINE
⊞ AUTO SPRINKLER	—OH— OVERHEAD WIRES	P.U.E. PUBLIC UTILITY EASEMENT
⊞ MONITORING WELL	—STM— STORM SEWER	D.E. DRAINAGE EASEMENT
⊞ GROUND LIGHT	—SAN— SANITARY SEWER	L ARC LENGTH
● BOLLARD	—X— CHAIN LINK FENCE	R RADIUS LENGTH
● 8/BOX	—□— STOCKADE FENCE	C CHORD LENGTH
— SIGN	—G— GUARD RAIL	CB CHORD BEARING
— FLAG POLE	CONCRETE SURFACE	CMP CORRUGATED METAL PIPE

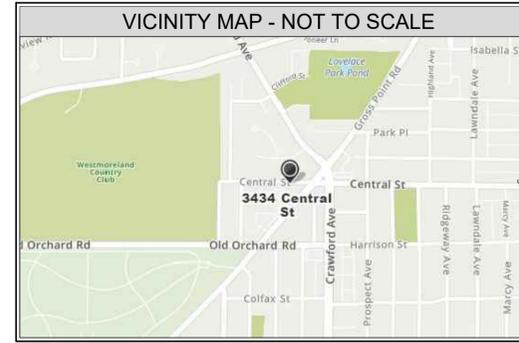
EXISTING PARKING SPACE TABLE

TYPE OF SPACE	TOTAL EXISTING
REGULAR	42
ADA	3
TOTAL	45

MISCELLANEOUS NOTES

- MN1 SURVEY PREPARED BY: JLH LAND SURVEYING INC. 910 GENEVA STREET SHOREWOOD, IL 60404 815.729.4000 INFO@JLHSURVEY.COM
- MN2 ALL FIELD MEASUREMENTS MATCH RECORD DIMENSIONS WITHIN THE PRECISION REQUIREMENTS OF ALTA/NSPS SPECIFICATIONS.
- MN3 THIS PROFESSIONAL SERVICE CONFORMS TO THE CURRENT ILLINOIS MINIMUM STANDARDS FOR A BOUNDARY SURVEY.
- MN4 ALL STREETS SHOWN ARE PUBLIC RIGHT OF WAY, UNLESS OTHERWISE NOTED.
- MN5 ASSUMED BEARING: THE SOUTH RIGHT OF WAY LINE OF CENTRAL STREET TO BE NORTH 89 DEGREES 30 MINUTES 15 SECONDS EAST.
- MN6 AT THE TIME OF THIS SURVEY THERE IS NO RECORD OR OBSERVED EVIDENCE OF A CEMETERY OR BURIAL GROUND.
- MN7 AT THE TIME OF THIS SURVEY, THE ADDRESS WAS POSTED AS 3434 CENTRAL STREET.
- MN8 THE SUBJECT PROPERTY HAS ACCESS TO AND FROM CENTRAL STREET WHICH IS GOVERNED BY THE VILLAGE OF EVANSTON.
- MN9 IN REGARDS TO TABLE "A" ITEM 11(a), AT THE TIME OF THIS SURVEY, NO PLANS AND/OR REPORTS WERE PROVIDED BY CLIENT. EVIDENCE OF UNDERGROUND UTILITIES SHOWN PER SECTION 5.E.IV.
- MN10 IN REGARDS TO TABLE "A" ITEM 16, AT THE TIME OF THIS SURVEY, THERE WAS NO VISIBLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OR BUILDING ADDITIONS.
- MN11 IN REGARDS TO TABLE "A" ITEM 17, AT THE TIME OF THIS SURVEY, THERE WAS NO RECENT STREET OR SIDEWALK CONSTRUCTION OR PROPOSED RIGHT OF WAY CHANGES PROVIDED.

VICINITY MAP - NOT TO SCALE



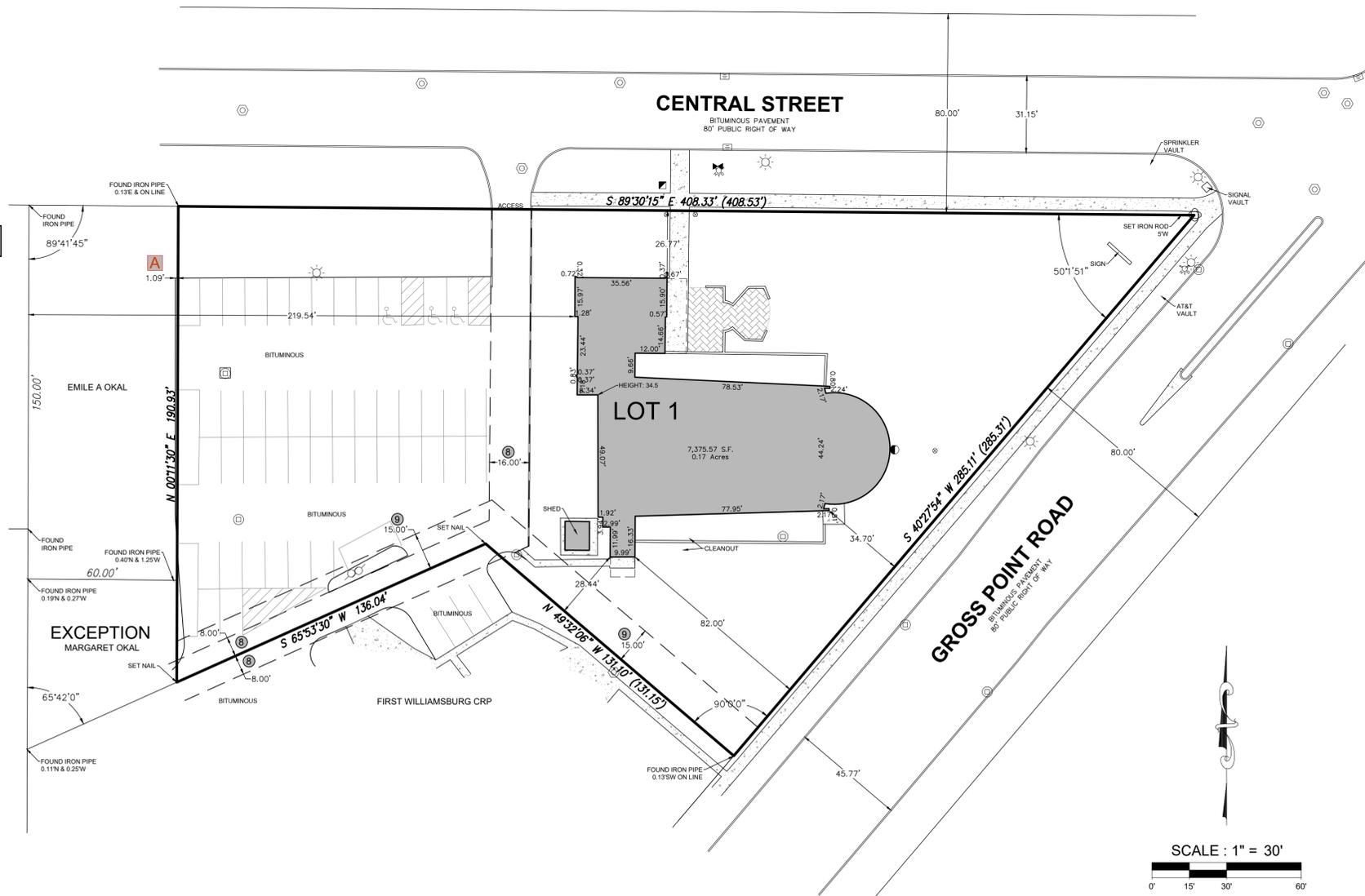
AREA: 57,942.76 SF± OR 1.33 ACRES±

SIGNIFICANT OBSERVATIONS

- A BITUMINOUS ASPHALT IS 1.09' WEST OF PROPERTY LINE

ITEMS CORRESPONDING TO SCHEDULE B-II

- B EASEMENT FOR INGRESS AND EGRESS FOR LOTS 1, 2 & 4 AND FOR PUBLIC UTILITIES AND DRAINAGE OVER, UPON AND UNDER A STRIP OF LAND 16 FEET IN WIDTH AS SHOWN ON THE PLAT OF SUBDIVISION RECORDED AS DOCUMENT 22843735. (FOR EXACT LOCATION SEE RECORD). ITEM IS SHOWN.
- B GRANT OF EASEMENT RECORDED MARCH 28, 2000 AS DOCUMENT NUMBER 00218014, MADE BY AND BETWEEN UNIT CHURCH OF THE NORTH SHORE AND COMMONWEALTH EDISON COMPANY RELATING TO THE RIGHT TO CONSTRUCT, RECONSTRUCT, ADD TO, REMOVE, OPERATE AND MAINTAIN ELECTRICAL TRANSMISSION SYSTEMS CONSISTING OF SUCH POLES, ANCHORS, GUYS, WIRES, CABLES, BURIED CABLES, CONDUITS, TERMINALS, MANHOLES OR OTHER RELATED FIXTURES AND APPURTENANCES THERETO ALONG WITH THE RIGHT OF INGRESS AND EGRESS THERETO AND TERMS AND CONDITIONS CONTAINED THEREIN. ITEM IS SHOWN.



FLOOD NOTE:
BY GRAPHIC PLOTTING ONLY, THIS PROPERTY IS IN ZONE(S) _____ OF THE FLOOD INSURANCE RATE MAP, COMMUNITY PANEL No. 17031C0253J, WHICH BEARS AN EFFECTIVE DATE OF 08/19/2008 AND IS NOT IN A SPECIAL FLOOD HAZARD AREA, BY CONTACT DATED 06/07/2021 TO THE NATIONAL FLOOD INSURANCE PROGRAM <http://www.fema.gov/>. WE HAVE LEARNED THIS COMMUNITY DOES CURRENTLY PARTICIPATE IN THE PROGRAM. NO FIELD SURVEYING WAS PERFORMED TO DETERMINE THIS ZONE AND AN ELEVATION CERTIFICATE MAY BE NEEDED TO VERIFY THIS DETERMINATION OR APPLY FOR A VARIANCE FROM THE FEDERAL EMERGENCY MANAGEMENT AGENCY.

RECORD DESCRIPTION

LOT 1 (EXCEPT THAT PART OF THE WEST 60 FEET OF LOT 1 LYING SOUTH OF A LINE 150 FEET SOUTH OF THE NORTH LINE OF AFORESAID LOT 1 IN PLAT OF CONSOLIDATION RECORDED SEPTEMBER 11, 1974 AS DOCUMENT 22843735) OF LOT 1 IN WILLIAMSBURG AND LOT 2 IN UNITY RESUBDIVISION OF LOT 2 IN WILLIAMSBURG, A SUBDIVISION OF LOT 1 IN EVERT AND SCHAFER'S SUBDIVISION OF PART OF THE NORTHEAST 1/4 OF FRACTIONAL SECTION 10, TOWNSHIP 41 NORTH, RANGE 13 EAST OF THE THIRD PRINCIPAL MERIDIAN, IN COOK COUNTY, ILLINOIS.

THE LANDS SURVEYED, SHOWN AND DESCRIBED HEREON ARE THE SAME LANDS AS DESCRIBED IN THE TITLE COMMITMENT PROVIDED BY OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY, COMMITMENT NO. NCT21011155, DATED FEBRUARY 01, 2021.

PROJECT REVISION RECORD

DATE	DESCRIPTION	DATE	DESCRIPTION
06/07/2021	FIRST DRAFT		
06/08/2021	COMMENTS		
FIELD WORK: SM & KH		DRAFTED: JLH	
		CHECKED BY: JLH	
		FB & PG: N/A	

LEGAL

THIS SURVEY WAS PREPARED FOR THE PURPOSE OF THIS REAL ESTATE TRANSACTION ONLY AND NO FURTHER PARTIES OTHER THAN THOSE CERTIFIED ABOVE SHALL RELY ON IT FOR ANY OTHER PURPOSE OR TRANSACTION.

ALTA/NSPS LAND TITLE SURVEY

for
UNITY ON THE N SHORE
NV5 PROJECT NO. 202100071-001
3434 CENTRAL STREET, EVANSTON, IL

BASED UPON TITLE COMMITMENT NO. NCT21011155
OF OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY
BEARING AN EFFECTIVE DATE OF FEBRUARY 01, 2021

SURVEYOR'S CERTIFICATION

To: UNITY ON THE NORTH SHORE; OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY; AND BOCK & CLARK CORPORATION, AN NV5 COMPANY;

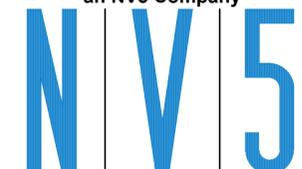
THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2021 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/NSPS LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 7a, 7b1, 7c, 8, 9, 11a, 13, 16, 17, AND 20 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON JUNE 01, 2021.

James L. Harpole
JAMES L. HARPOLE, PLS
REGISTRATION NO. 035-4046
IN THE STATE OF ILLINOIS, EXPIRES 11-30-2022
ILLINOIS PROFESSIONAL DESIGN FIRM NO. 184.007120
NETWORK PROJECT NO. 202100071-001



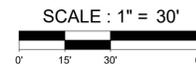
SHEET 1 OF 1

Bock & Clark Corporation
an NV5 Company

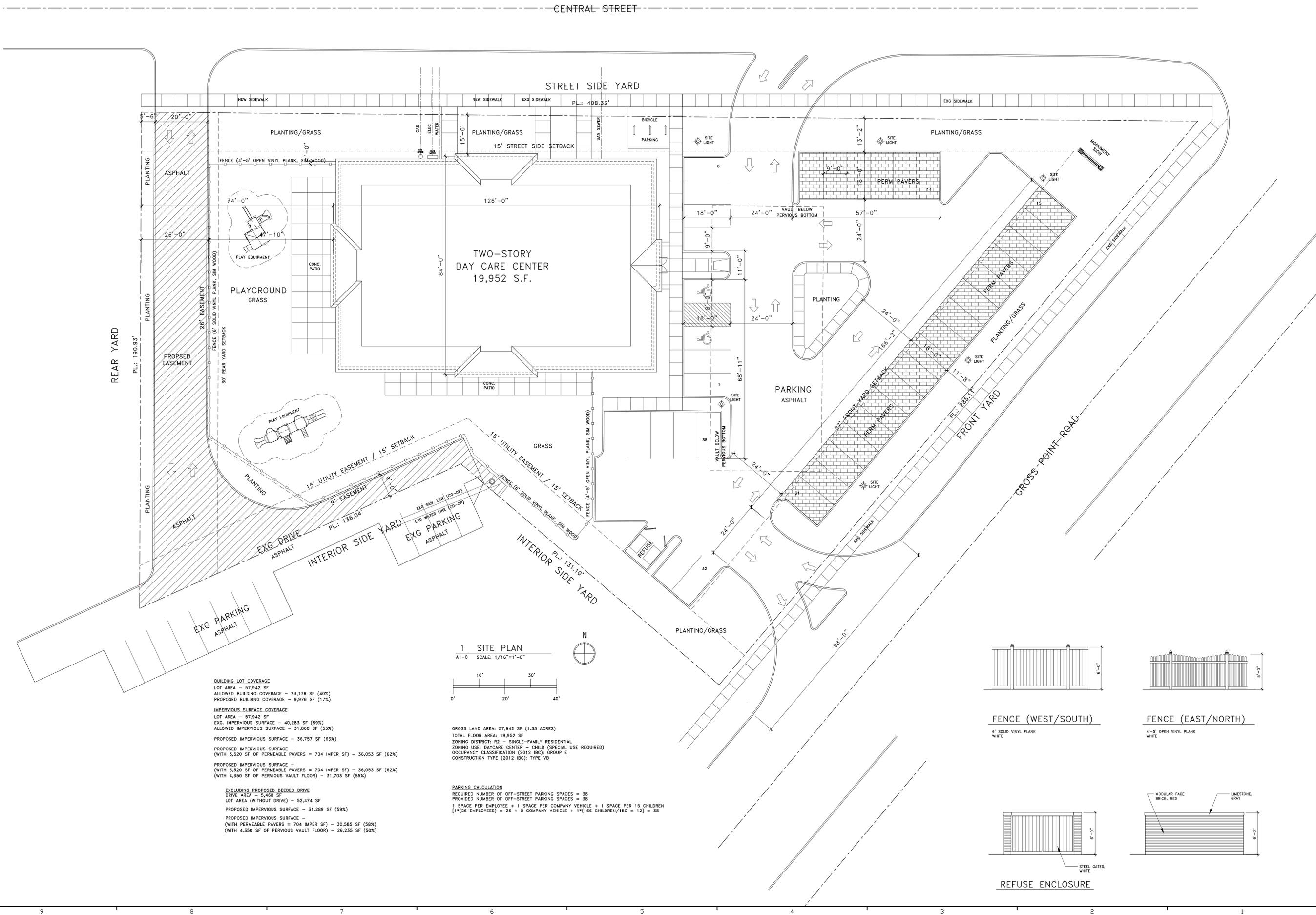


Transaction Services 1-800-SURVEYS (787-8397)
3550 W. Market Street, Suite 200, Akron, Ohio 44333
www.BockandClark.com maywehelpyou@bockandclark.com www.NV5.com

SURVEY • ZONING • ENVIRONMENTAL • ASSESSMENT



14. Architectural Plans



THE LAUDER GROUP
743 MCCLINTOCK DRIVE
BURR RIDGE, IL 60527
T 312-907-7977

KENSINGTON SCHOOL OF EVANSTON
3434 CENTRAL STREET
EVANSTON, IL 60201

DATE	DESCRIPTION
12-08-25	PER EVANSTON COMMENTS
10-17-25	PER EVANSTON COMMENTS
05-09-25	PLANNED DEVELOPMENT SUBMISSION

DRAWING TITLE

SITE PLAN

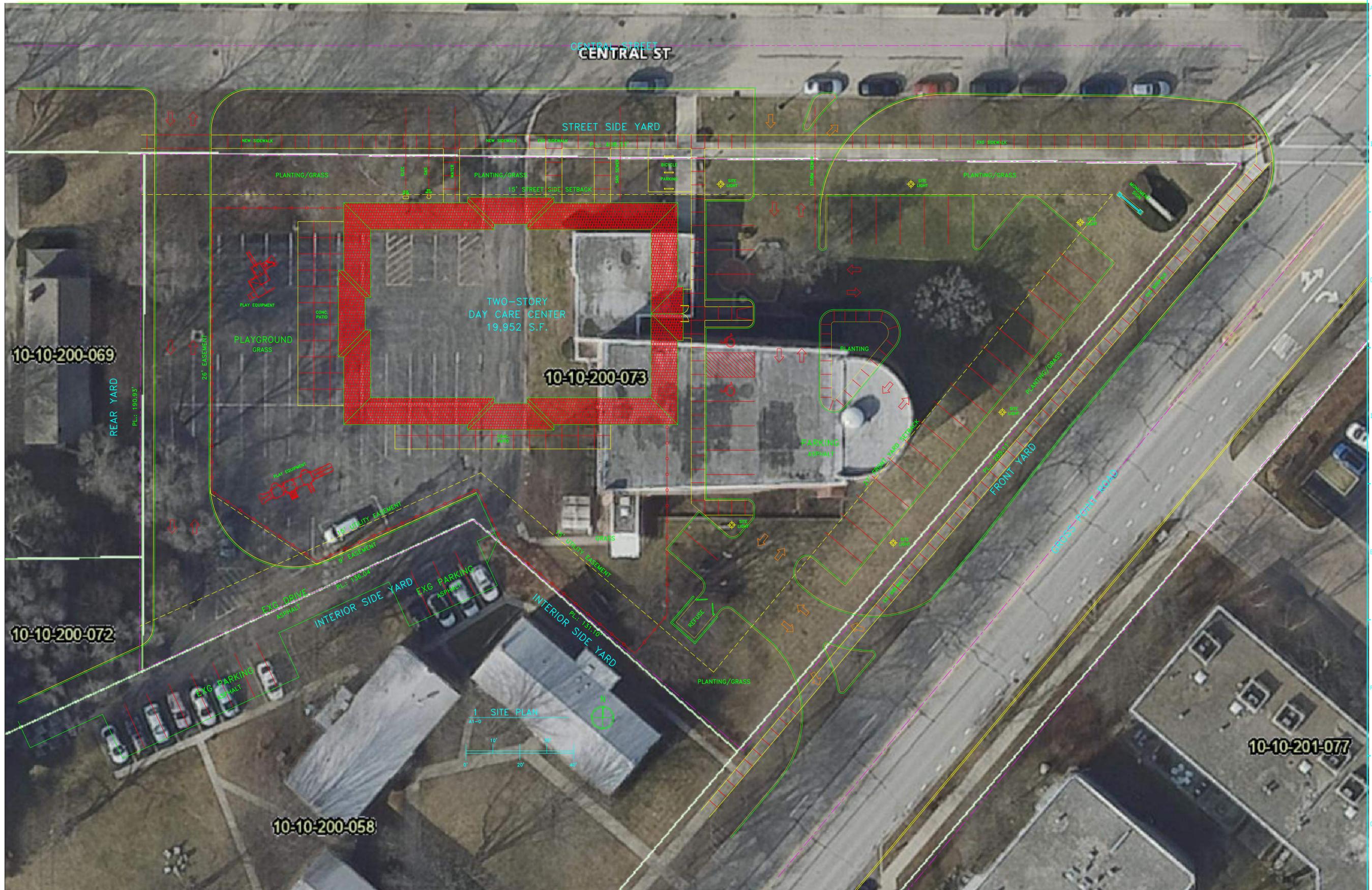
JOB NO.: 25-001

DATE: 5-8-25

DRAWN BY: LEL

DRAWING NO.

A1-0



THE LAUDER GROUP
 743 MCCLINTOCK DRIVE
 BURR RIDGE, IL 60527
 T 312-907-7977

KENSINGTON SCHOOL OF EVANSTON
 3434 CENTRAL STREET
 EVANSTON, IL 60201

DRAWING ISSUE	DATE	DESCRIPTION

DRAWING TITLE	
SITE OVERLAY	
JOB NO.:	21-001
DATE:	10-1-21
DRAWN BY:	LEL

DRAWING NO.
0V-1



charles vincent george
ARCHITECTS

1245 E. Diehl Rd. Suite 101

Naperville, Illinois 60563

P | 630.357.2023 F | 630.357.2662

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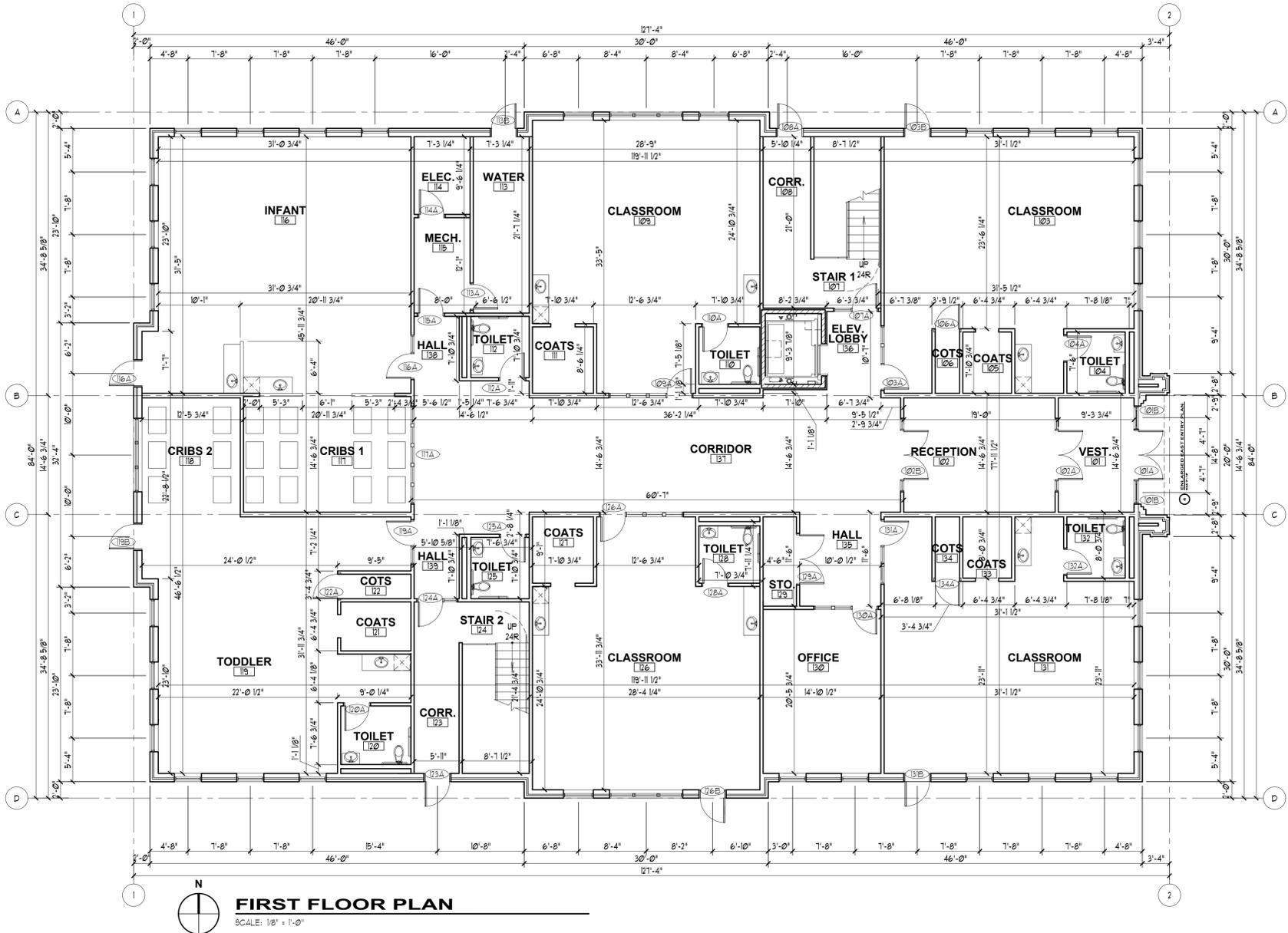
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ISSUE AND/OR REVISIONS:

NO.	DATE	DESCRIPTION
	5-8-25	PD SUBMISSION

PROJECT:



FIRST FLOOR PLAN
SCALE: 1/8" = 1'-0"

NEW CONSTRUCTION FOR
KENSINGTON - EVANSTON
3434 CENTRAL STREET
EVANSTON, IL. 60201

TITLE:
FIRST FLOOR PLAN

DATE: PROJECT #
2022-025

PRINCIPAL: BFG SHEET:
MN EOP A2.1

PROJECT MGR: MN DRAWN BY: EOP
DRAWING FILE PATH: M:\2022\DRAWINGS\A2.1 FIRST FLOOR PLAN_2022



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NO.	DATE	DESCRIPTION
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PROJECT:

NEW CONSTRUCTION FOR

KENSINGTON - EVANSTON

3434 CENTRAL STREET

EVANSTON, IL. 60201

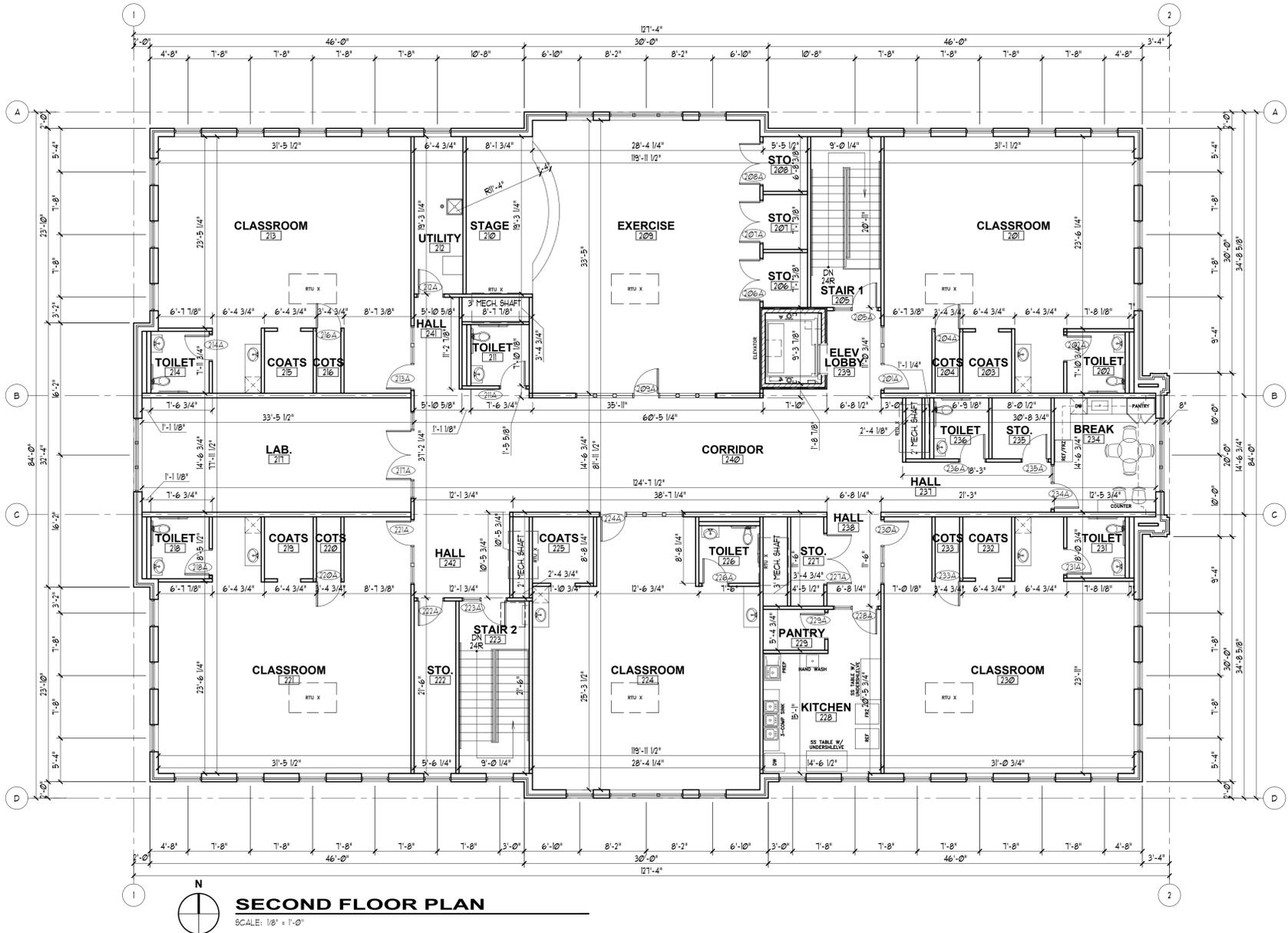
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DATE: PROJECT # 2022-025

PRINCIPAL: BFG SHEET: A2.2

PROJECT MGR: MN DRAWN BY: EOP

DATE: 11/2/2022 DRAWING: A2.2 SECOND FLOOR PLAN_2205





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	5-8-25	PD SUBMISSION

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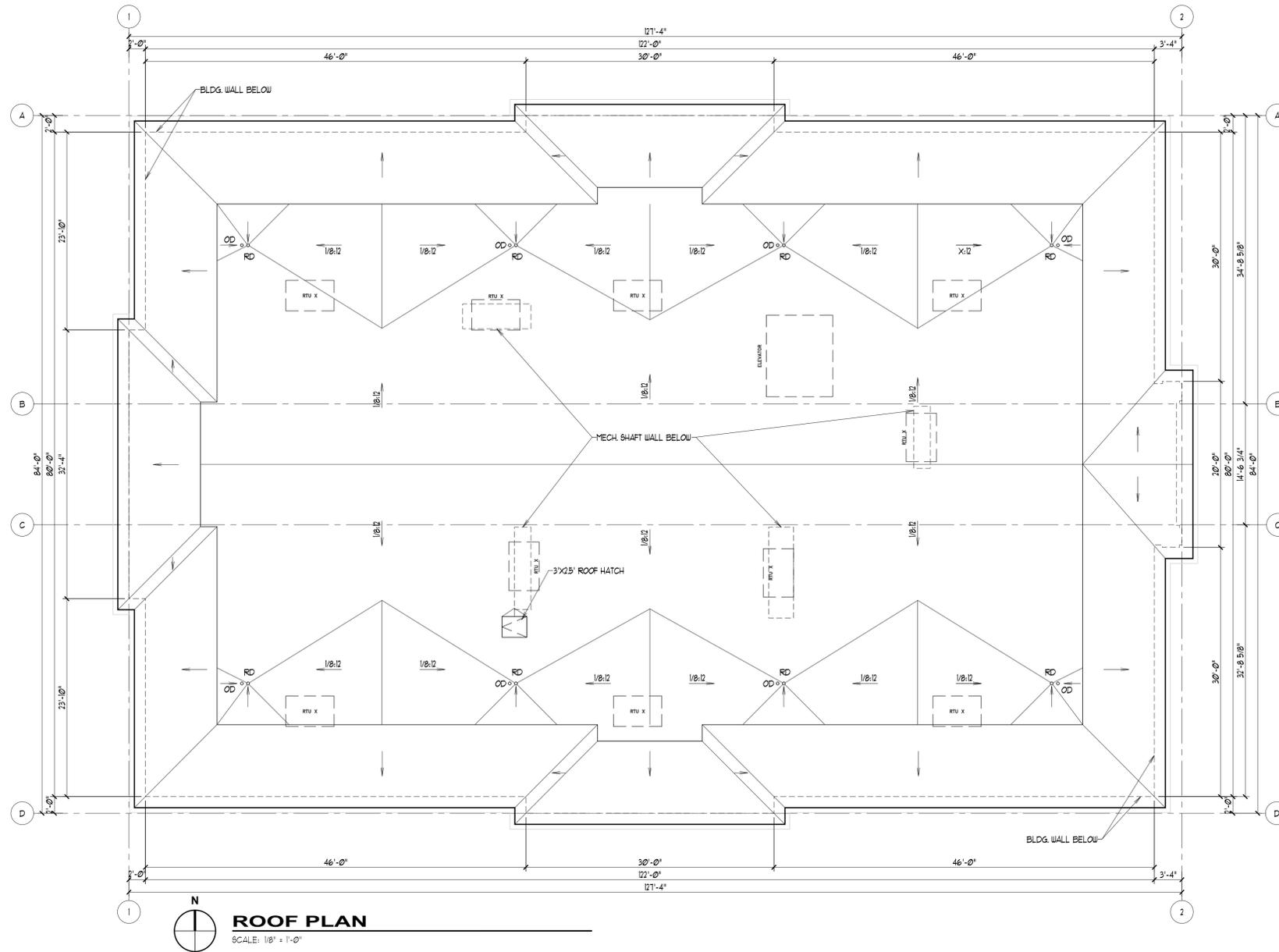
NEW CONSTRUCTION FOR
KENSINGTON - EVANSTON
3434 CENTRAL STREET
EVANSTON, IL. 60201

TITLE:
ROOF PLAN

DATE: PROJECT #
2022-025

PRINCIPAL: BFG SHEET:
A2.3

PROJECT MGR: MN DRAWN BY: EOP
DRAWING FILE PATH:
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NO.	DATE	DESCRIPTION
5-8-25		PD SUBMISSION
10-17-25		PER CITY COMMENTS

PROJECT:

NEW CONSTRUCTION FOR
KENSINGTON - EVANSTON
3434 CENTRAL STREET
EVANSTON, IL. 60201

TITLE:
EXTERIOR ELEVATIONS

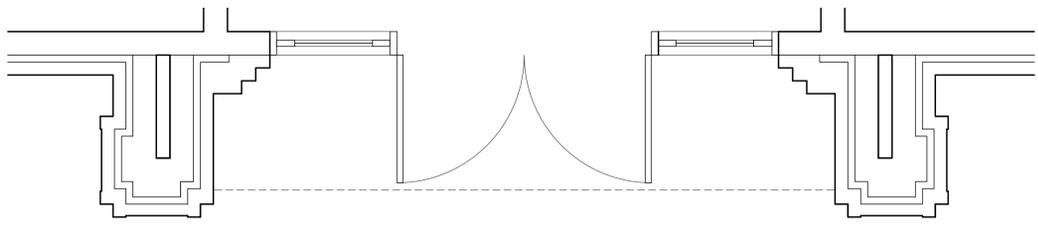
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2022-025

PRINCIPAL: BFG SHEET:
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PROJECT MGR: MN DRAWN BY: EOP
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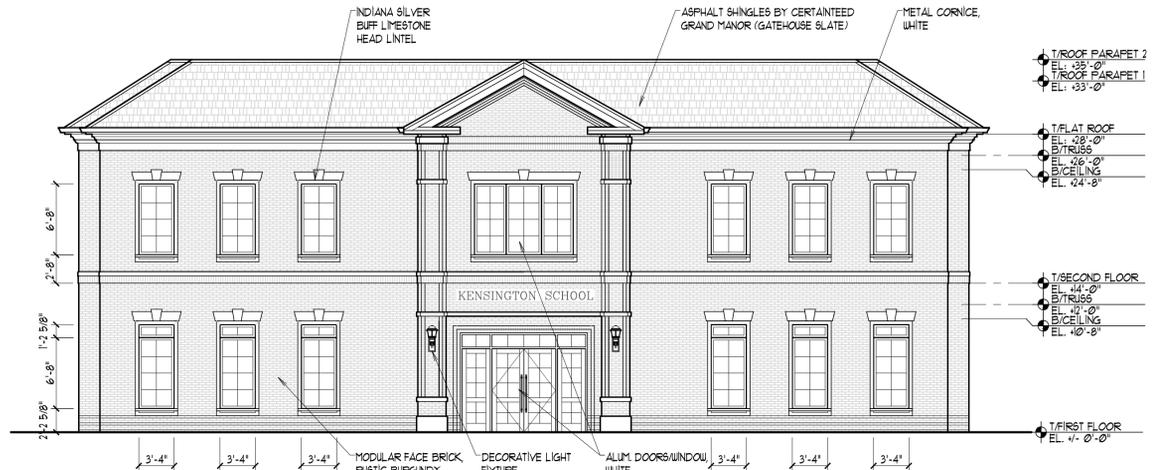
3 ENLARGED EAST ELEVATION
SCALE: 1/2" = 1'-0"



4 ENLARGED EAST ENTRY PLAN
SCALE: 1/2" = 1'-0"



2 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



1 EAST ELEVATION
SCALE: 1/8" = 1'-0"

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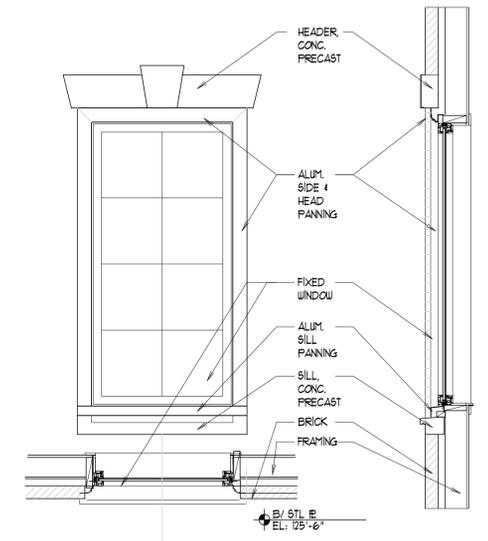
NO.	DATE	DESCRIPTION
	5-8-25	PD SUBMISSION

PROJECT:

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KENSINGTON - EVANSTON
3434 CENTRAL STREET
EVANSTON, IL. 60201

TITLE:
EXTERIOR ELEVATIONS

DATE:	PROJECT #
	2022-025
PRINCIPAL:	SHEET:
BFG	A3.2
PROJECT MGR: MN	DRAWN BY: EOP
DRAWING FILE PATH: M/2022/DRAWINGS/ A3.2 EXTERIOR ELEVATIONS	



3 WINDOW ELEV., PLAN & SECT
SCALE: 1/2" = 1'-0"



2 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"



1 WEST ELEVATION
SCALE: 1/8" = 1'-0"

2022-05-08 10:00 AM C:\PROJECTS\KENSINGTON - EVANSTON\DRAWINGS\EXTERIOR ELEVATIONS\A3.2 EXTERIOR ELEVATIONS.DWG



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NO.	DATE	DESCRIPTION
	5-8-25	PD SUBMISSION

PROJECT:

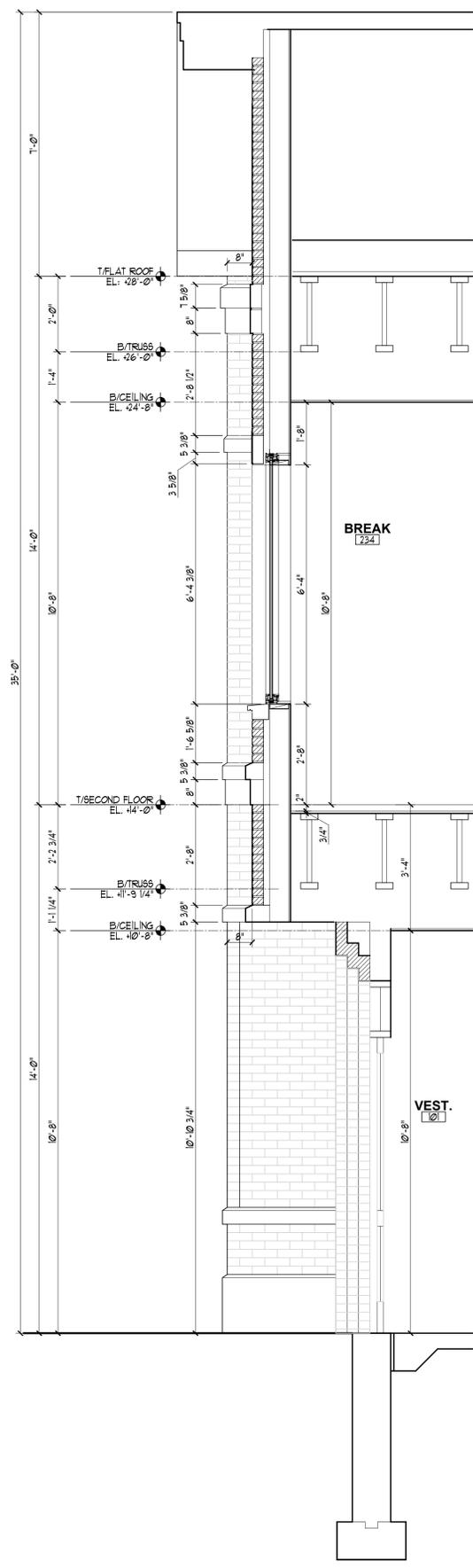
NEW CONSTRUCTION FOR
KENSINGTON - EVANSTON
3434 CENTRAL STREET
EVANSTON, IL 60201

TITLE:
BUILDING SECTIONS

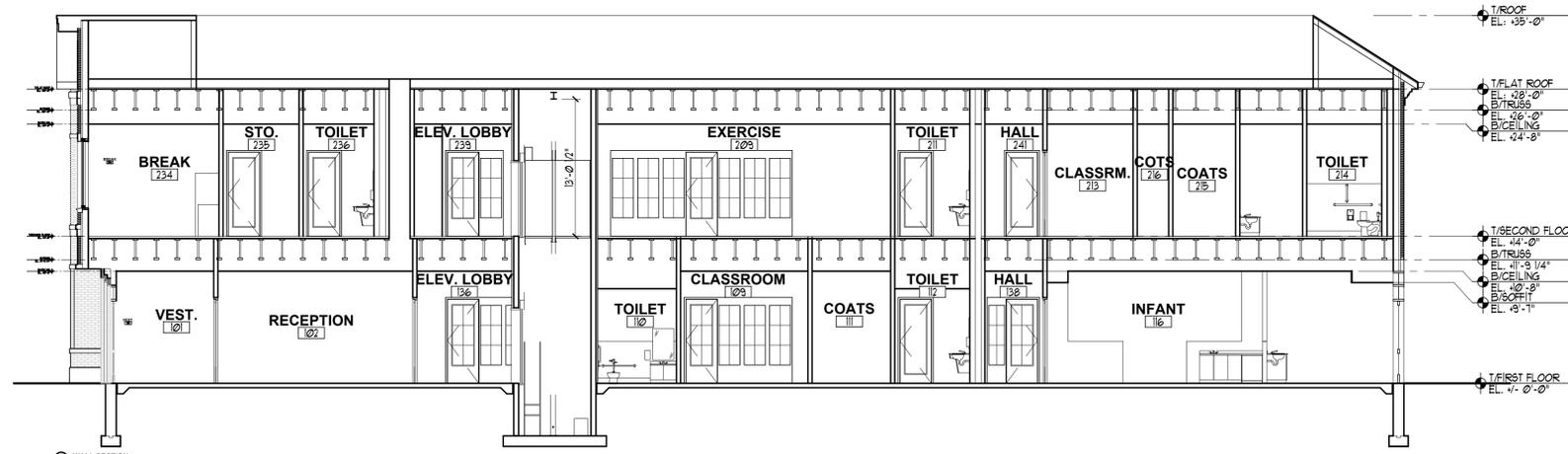
DATE: PROJECT #
2022-025

PRINCIPAL: BFG SHEET:
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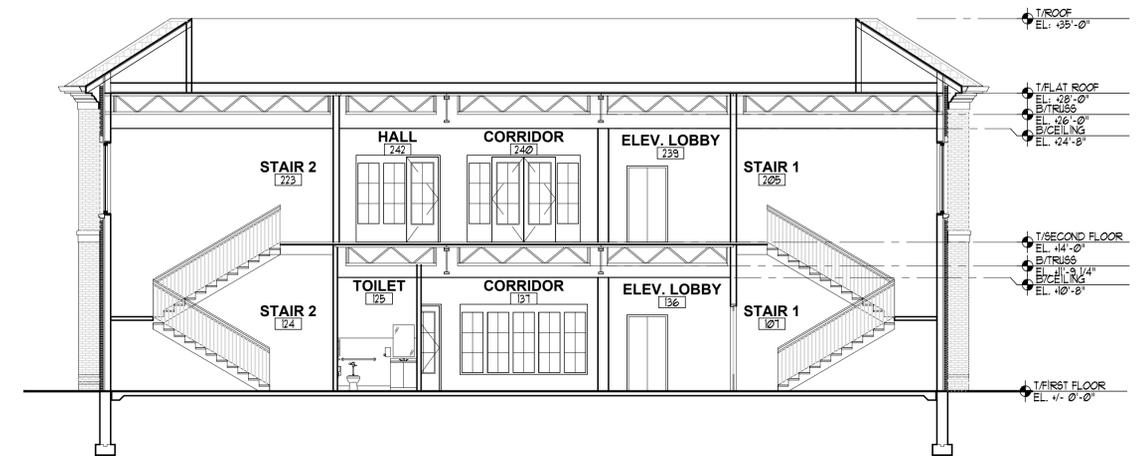
PROJECT MGR: MN DRAWN BY: EOP
DRAWING FILE PATH: M:\2022\DRAWINGS\A4.1 BUILDING SECTIONS



3 WALL SECTION
SCALE: 1/2" = 1'-0"



2 BUILDING SECTION
SCALE: 1/8" = 1'-0"



1 BUILDING SECTION
SCALE: 1/8" = 1'-0"

2022-05-08 10:00 AM C:\PROJECTS\KENSINGTON - EVANSTON\DRAWINGS\SECTION\SECTION 020101.dwg



Kensington School-Evanston View Looking West at Main entry



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Kensington School-Evanston Aerial Looking Southwest



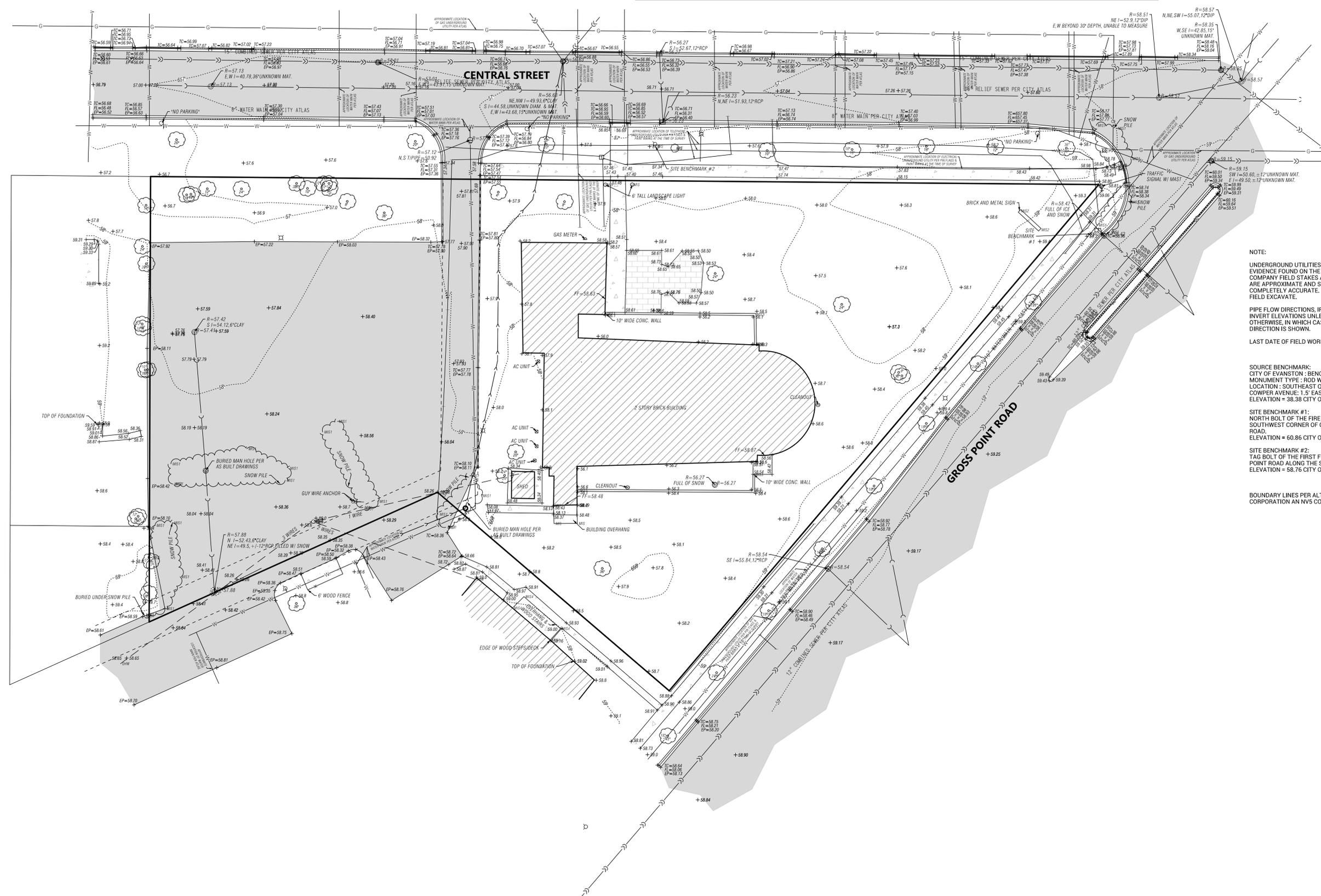
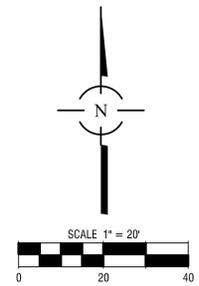
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KENNESAW COLLEGE

15. Engineering Plans & Preliminary Stormwater Report

LEGEND					
	STORM SEWER		AIR CONDITIONING UNIT		MANHOLE
	SANITARY SEWER		BOLLARD		SEWER FLAG POLE
	COMBINED SEWER		HAND HOLE		SATELLITE DISH
	WATER MAIN		TRAFFIC SIGNAL BOX		UNIDENTIFIED PEDESTAL
	GAS MAIN		ELECTRIC METER		DOWNSPOUT
	UNDERGROUND TELEPHONE LINE		LIGHT POLE		STORM MANHOLE
	UNDERGROUND ELECTRIC LINE		GUY WIRE ANCHOR		CATCH BASIN
	OVERHEAD ELECTRIC LINE		LANDSCAPE LIGHT/FLOOD LIGHT		FLARED END SECTION
	OVERHEAD GAS LINE		UTILITY POLE		MEET
	CATV		ELECTRIC PEDESTAL		AUXILIARY VALVE
	FIBER OPTIC LINE		TELEPHONE PEDESTAL		BACKFLOW CHECK
	RAILROAD		TELEPHONE MANHOLE		SAMESIZE CONNECTION/AUTO SPRINKLER
	FENCE		CABLE PEDESTAL		WATER VALVE
	GUARDRAIL		FIRE HYDRANT		WATER METER WELL
	EDGE OF WATER		GAS METER		GAS VALVE
	WETLAND LIMITS		VALVE AND VAULT		TEST PIT
			TREE WITH SIZE		ASPHALT
			TREE WITHOUT SIZE		CONCRETE
			BUSH		GRAVEL
			WINDOW WELL		BRICK
			SPOT ELEVATION		CONCRETE MONUMENT
			SPOT ELEVATION		DISC
			SPOT ELEVATION		BORING
			SPOT ELEVATION		SPOT ELEVATION



NOTE:
 UNDERGROUND UTILITIES ARE SHOWN BY USING PHYSICAL EVIDENCE FOUND ON THE SURFACE AND/OR FROM UTILITY COMPANY FIELD STAKES AND, THEREFORE, THEIR LOCATIONS ARE APPROXIMATE AND SUSPECTED AND MAY NOT BE COMPLETELY ACCURATE. FOR MORE ACCURATE LOCATION, FIELD EXCAVATE.
 PIPE FLOW DIRECTIONS, IF SHOWN, ARE BASED ON FIELD INVERT ELEVATIONS UNLESS EXISTING PLANS INDICATE OTHERWISE. IN WHICH CASE THE EXISTING PLAN FLOW DIRECTION IS SHOWN.
 LAST DATE OF FIELD WORK: FEBRUARY 12, 2022

SOURCE BENCHMARK:
 CITY OF EVANSTON: BENCHMARK #35
 MONUMENT TYPE: ROD WITH CAP
 LOCATION: SOUTHEAST QUADRANT; CENTRAL STREET & COPPER AVENUE; 1.5' EAST & 1' SOUTH OF SIDE ELEVATION = 58.38 CITY OF EVANSTON DATUM

SITE BENCHMARK #1:
 NORTH BOLT OF THE FIRE HYDRANT LOCATED AT THE SOUTHWEST CORNER OF CENTRAL STREET AND GROSS POINT ROAD.
 ELEVATION = 60.86 CITY OF EVANSTON DATUM

SITE BENCHMARK #2:
 TAG BOLT OF THE FIRST FIRE HYDRANT WEST OF THE GROSS POINT ROAD ALONG THE SOUTH SIDE OF CENTRAL STREET.
 ELEVATION = 58.75 CITY OF EVANSTON DATUM

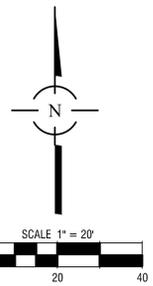
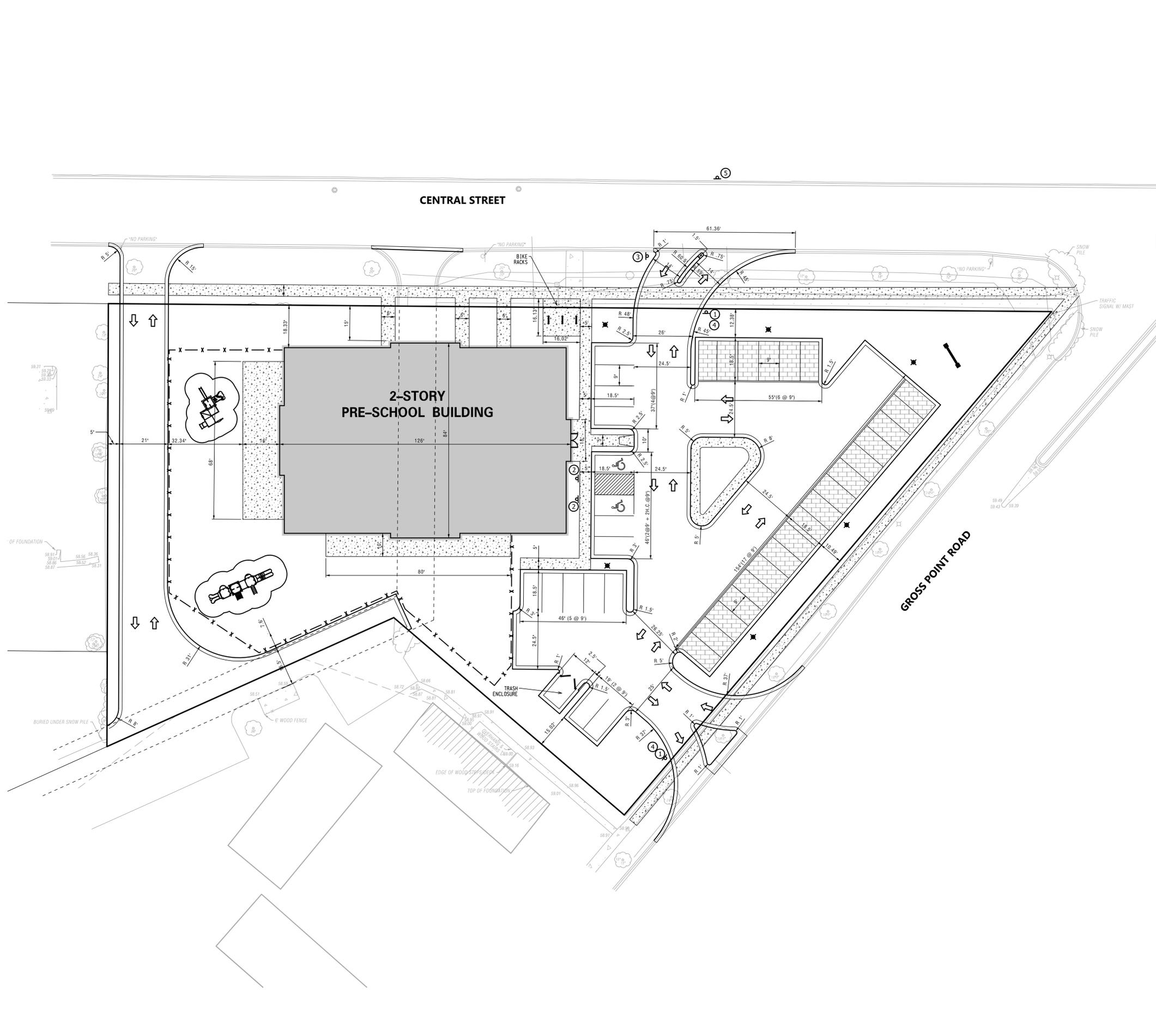
BOUNDARY LINES PER ALTA SURVEY BY BOCK & CLARK CORPORATION AN NVS COMPANY, DATED OCTOBER 11, 2021.

PRELIMINARY EXISTING CONDITIONS
KESINGTON SCHOOL
 3434 CENTRAL STREET
 EVANSTON, ILLINOIS

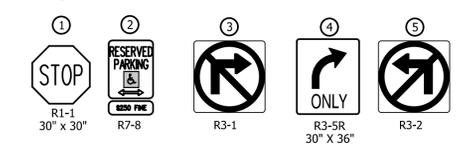
Spaceco
 Civil Engineering & Surveying
 Rosemont, IL - Morris, IL - Indianapolis, IN
 spacecoinc.com

FILENAME:	11641P-EC
DATE:	04/16/25
JOB NO.	11641
SHEET	P-EC
	2 OF 5

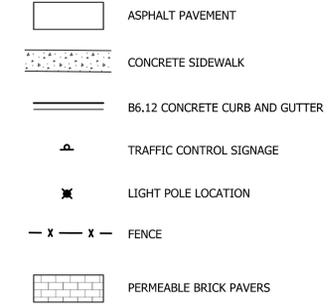
NO.	DATE	REMARKS
1	05/08/25	EVANSTON PD SUBMITTAL



SIGN LEGEND:



LEGEND



PARKING SUMMARY	
STANDARD:	36
ACCESSIBLE:	2
TOTAL:	38

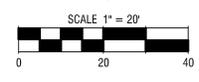
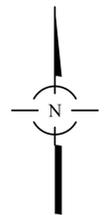
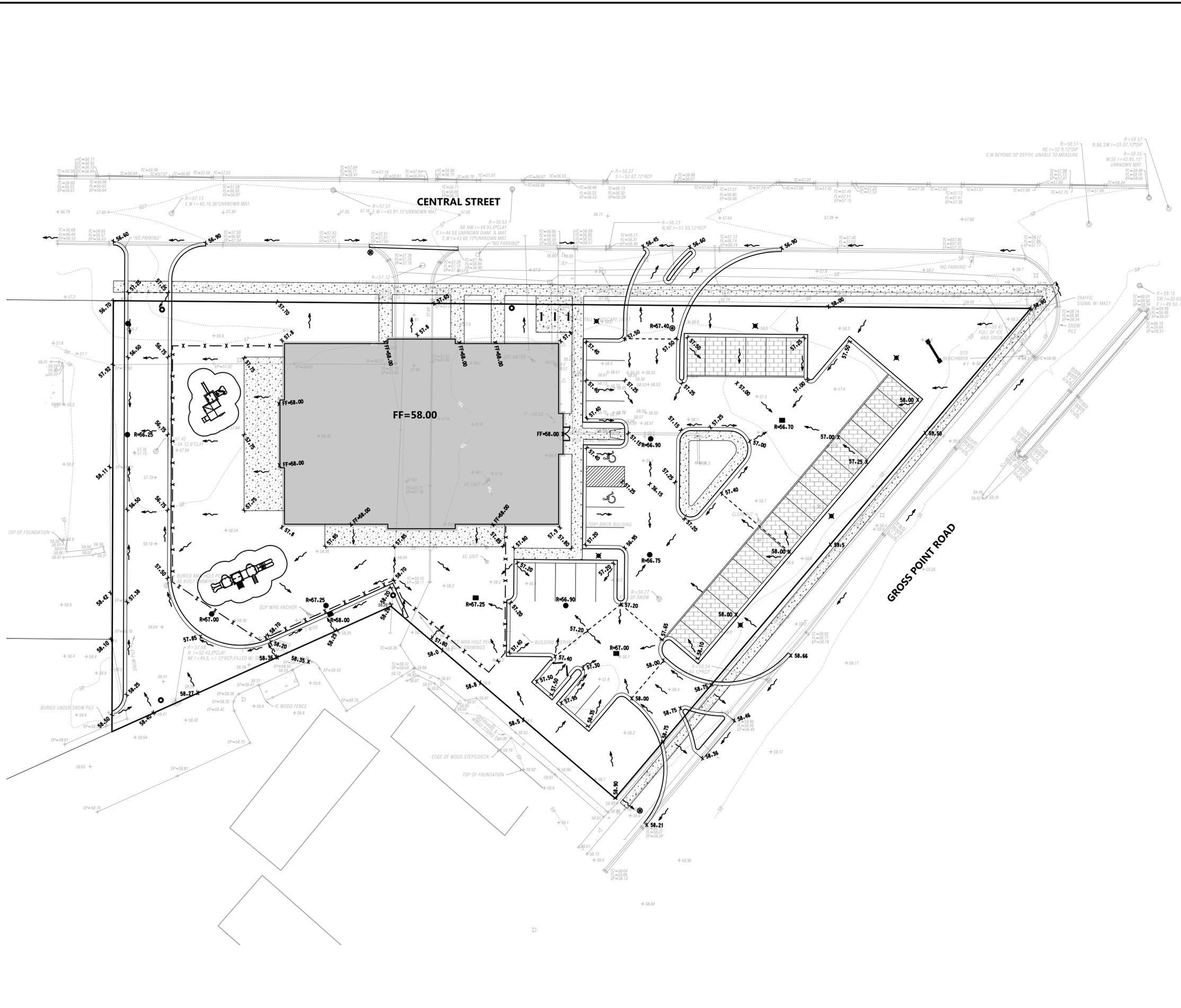
NOTES:
 1. ALL DIMENSIONS ARE TO THE BACK OF CURB UNLESS OTHERWISE NOTED.

NO.	DATE	REMARKS
3	12/08/25	PER EVANSTON COMMENTS
2	10/17/25	PER EVANSTON COMMENTS
1	05/08/25	EVANSTON PD SUBMITTAL

PRELIMINARY GEOMETRIC PLAN
KESINGTON SCHOOL
 3434 CENTRAL STREET
 EVANSTON, ILLINOIS



FILENAME:	11641P-GM
DATE:	04/16/25
JOB NO.:	11641
SHEET	P-GM
	3 OF 5



- LEGEND**
- F/F FINISHED FLOOR
 - LOCAL DRAINAGE
 - OVERLAND FLOW ROUTE
 - LOCAL DRAINAGE DIVIDE

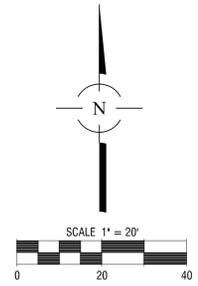
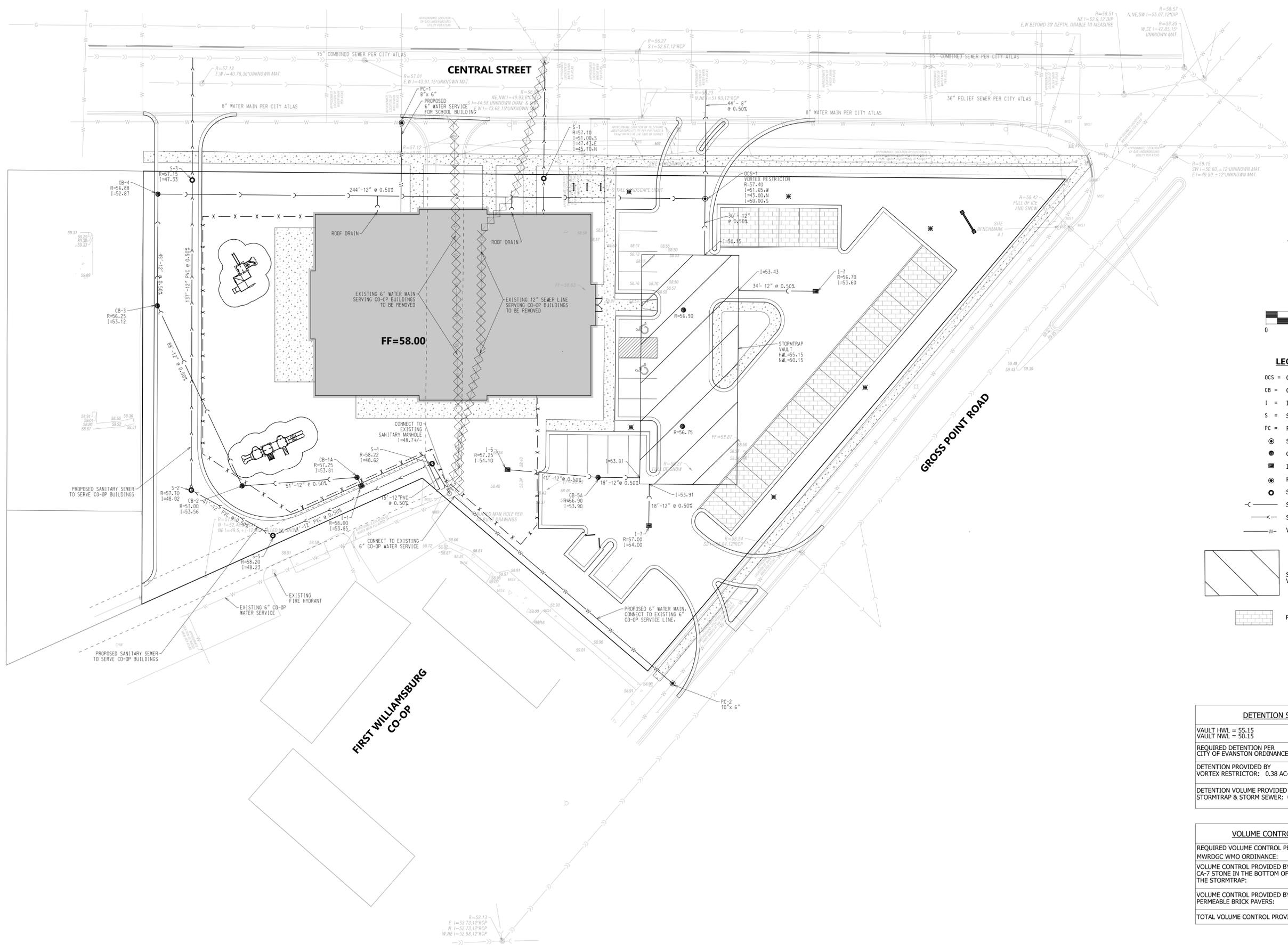
- NOTES:**
1. ALL CURB AND GUTTER IS REVERSE PITCH UNLESS NOTED OTHERWISE.
 2. ALL SPOT GRADES ALONG CURB LINE ARE EDGE OF PAVEMENT UNLESS NOTED OTHERWISE.

NO.	DATE	REMARKS
3	12/08/25	PER EVANSTON COMMENTS
2	10/17/25	PER EVANSTON COMMENTS
1	05/08/25	EVANSTON PD SUBMITTAL

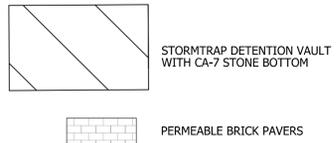
PRELIMINARY GRADING PLAN
KESINGTON SCHOOL
 3434 CENTRAL STREET
 EVANSTON, ILLINOIS



FILENAME:	11641P-GR
DATE:	04/16/25
JOB NO.:	11641
SHEET	P-GR
	4 OF 5



- LEGEND**
- OCSS = OUTLET CONTROL STRUCTURE
 - CB = CATCH BASIN
 - I = INLET
 - S = SANITARY MANHOLE
 - PC = PRESSURE CONNECTION
 - = STORM MANHOLE
 - = CATCH BASIN/DRY WELL
 - = INLET
 - = PRESSURE CONNECTION
 - = SANITARY MANHOLE
 - = STORM SEWER
 - = SANITARY SEWER
 - = WATER MAIN



DETENTION SUMMARY	
VAULT HWL =	55.15
VAULT NWL =	50.15
REQUIRED DETENTION PER CITY OF EVANSTON ORDINANCE:	0.38 AC-FT
DETENTION PROVIDED BY VORTEX RESTRICTOR:	0.38 AC-FT
DETENTION VOLUME PROVIDED BY STORMTRAP & STORM SEWER:	0.40 AC-FT

VOLUME CONTROL SUMMARY	
REQUIRED VOLUME CONTROL PER MWRDGC WMO ORDINANCE:	0.07 AC-FT
VOLUME CONTROL PROVIDED BY CA-7 STONE IN THE BOTTOM OF THE STORMTRAP:	0.03 AC-FT
VOLUME CONTROL PROVIDED BY PERMEABLE BRICK PAVERS:	0.04 AC-FT
TOTAL VOLUME CONTROL PROVIDED:	0.07 AC-FT

NO.	DATE	REMARKS
4	01/02/26	PER EVANSTON COMMENTS
3	12/08/25	PER EVANSTON COMMENTS
2	10/17/25	PER EVANSTON COMMENTS
1	05/08/25	EVANSTON PD SUBMITTAL

PRELIMINARY UTILITY PLAN
KESINGTON SCHOOL
 3434 CENTRAL STREET
 EVANSTON, ILLINOIS



FILENAME:	11641 P-UT
DATE:	04/16/25
JOB NO.	11641
SHEET	P-UT
	5 OF 5



CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

9575 W. Higgins Road, Suite 700, Rosemont, Illinois 60018
Phone: (847) 696-4060 Fax: (847) 696-4065



Preliminary Stormwater Management Report
Kensington School of Evanston
3434 Central Street
Evanston, Illinois

Project No. 11641

INDEX

Tab	Description
	Title Sheet
	Index
	Project Overview
1	C- Value Calculations
2	Detention Calculations

Project Overview

This report summarizes the results of the preliminary storm water analysis performed by SPACECO, Inc. for the proposed pre-school development. The project site is approximately 1.33 acres and is located at 3434 Central Street in the City of Evanston, Cook County, Illinois. The development will include a 2-story pre-school building, at grade parking stalls, and utilities. Starting the effective date of the WMO, any new development on a parcel more than 0.50 acres, runoff control and volume control will apply per the MWRDGC. Since the site is less than 3 acres, detention is not required per MWRD. Per the City of Evanston's Development Code, detention is required for the new development.

Existing Conditions

The project site currently consists of an existing building with at-grade parking.

There is no regulatory floodplain and no special flood hazard areas located on the site according to Flood Insurance Rate Map (FIRM) 17031C0255J with an effective date of August 19, 2008.

Proposed Conditions

Stormwater Detention

The required detention volume was calculated using the 100-year storm event release rate of 0.20 cfs (1.33 acre x 0.15 cfs/acre) as specified in the City of Evanston's Development Code.

Assuming a runoff coefficient of 0.90 for pavements and roofs and 0.30 for grass, the composite C-value for the site was 0.64. To calculate the required detention required, Bulletin 75 rainfall data was used for this hydrologic analysis. The model calculated that approximately 0.36 ac-ft of storage is required for the site. The detention is provided underground by a Stormtrap system and the site storm sewer.

The outlet control structure will be sized so that the 100-year event is detained and that the runoff leaving the site is not increased. The supporting calculations can be found in Tab 2.

Table 1. Detention Summary

Tributary Area (acres)	Impervious Area (acres)	C-Value	Allowable Release Rate (cfs)	100 Year Req'd Detention Storage (AC-FT)
1.33	0.755	0.64	0.20	0.36

Site Volume Control

The MWO requires that 1 inch of stormwater runoff from all impervious areas be treated using volume control practices. Following this specification, the required volume control for this project is 0.06 ac-ft or 2,614 C.F. The volume control for this project will be provided in the open/stone bottom below the main outfall elevation of the Stormtrap detention vault.

Table 2. Volume Control Summary

Impervious Area (acres)	Volume Control Required (AC-FT)	Volume Control Provided (Vault Stone Bottom) (AC-FT)	Total Volume Control Provided (AC-FT)
0.755	0.06	0.07	0.07



1 C-Value Calculations



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CONSULTING ENGINEERS
SITE DEVELOPMENT ENGINEERS
LAND SURVEYORS

PROJECT: **KENSINGTON SCHOOL OF EVANSTON**
LOCATION: **3434 CENTRAL STREET, EVANSTON, IL 60201**

PROJECT #: 11641
DATE: 2/4/2022
LAST REVISED:

CALCULATION TITLE: **SITE AREA AND C-FACTOR CALCULATIONS**
DESCRIPTION: TOTAL SITE AREA
SITE CONDITION: PROPOSED

	AREA (ACRES)	C-FACTOR
<u>AREA OF SITE</u>	1.330	0.64
IMPERVIOUS AREA	0.755	0.90
WATER SURFACE AREA	0.000	1.00
PERVIOUS AREA	0.575	0.3



2 Detention Calculations



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CONSULTING ENGINEERS
 SITE DEVELOPMENT ENGINEERS
 LAND SURVEYORS

PROJECT: KENSINGTON SCHOOL OF EVANSTON
 LOCATION: 3434 CENTRAL STREET, EVANSTON, IL

PROJECT #: 11641
 DATE: 2/4/2022
 LAST REVISED:

CALCULATION TITLE: DETENTION VOLUME REQUIRED
 DESCRIPTION: BULLETIN 75--CITY OF EVANSTON ORDINANCE ORDINANCE
 SITE CONDITION: PROPOSED

SITE AREA 1.33 AC
 COMPOSITE RUNOFF COEFFICIENT 0.64
 ACTUAL/ALLOWABLE RELEASE RATE 0.20 CFS (0.15cfs/acre)

COMPUTED DETENTION STORAGE				=	0.36	acre-ft
STORM	DURATION	100 YEAR	INFLOW RATE	STORED	STORAGE	
MIN.	HOURS	INTENSITY	$Q_i = C * I_{100} * A$	RATE	REQUIRED	
		(in/hr)	CFS	CFS	AC-FT	
5	0.08	12.36	10.53	10.33	0.07	
10	0.17	11.34	9.66	9.46	0.13	
15	0.25	9.28	7.91	7.71	0.16	
20	0.33	6.90	5.88	5.68	0.16	
30	0.50	6.34	5.40	5.20	0.22	
60	1	4.03	3.43	3.23	0.27	
90	1.5	3.00	2.56	2.36	0.29	
120	2	2.49	2.12	1.92	0.32	
180	3	1.83	1.56	1.36	0.34	
240	4	1.45	1.24	1.04	0.35	
300	5	1.22	1.04	0.84	0.35	
360	6	1.07	0.91	0.71	0.36	
420	7	0.94	0.80	0.60	0.35	
480	8	0.85	0.72	0.52	0.35	
540	9	0.77	0.66	0.46	0.34	
600	10	0.71	0.60	0.40	0.34	
660	11	0.66	0.56	0.36	0.33	
720	12	0.62	0.53	0.33	0.33	
780	13	0.58	0.49	0.29	0.32	
840	14	0.55	0.47	0.27	0.31	
900	15	0.52	0.44	0.24	0.30	
960	16	0.49	0.42	0.22	0.29	
1020	17	0.47	0.40	0.20	0.28	
1080	18	0.45	0.38	0.18	0.28	
1140	19	0.43	0.37	0.17	0.26	
1200	20	0.41	0.35	0.15	0.25	
1260	21	0.4	0.34	0.14	0.25	
1320	22	0.38	0.32	0.12	0.23	
1380	23	0.37	0.32	0.12	0.22	
1440	24	0.36	0.31	0.11	0.21	

16. Landscape & Tree Preservation Plans

SITE PREPARATION AND REMOVALS NOTES

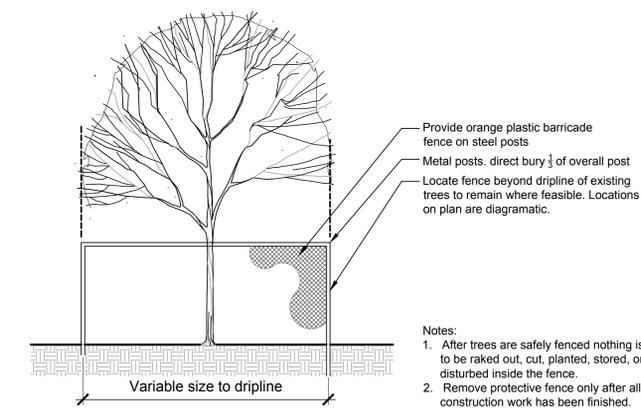
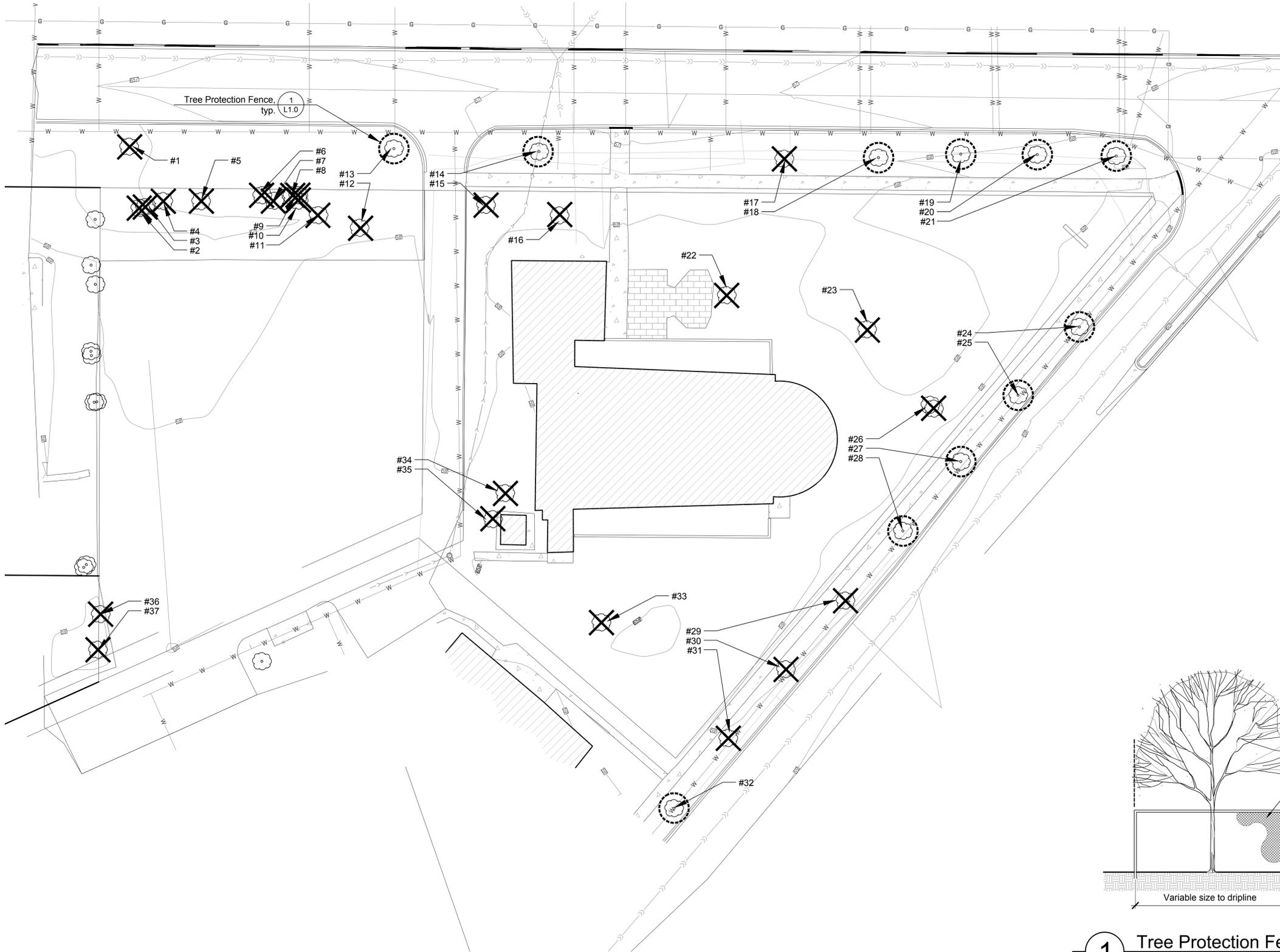
- Contractor shall install tree protection fencing as shown on the plans prior to beginning work. Maintain and adjust tree protection fencing as needed during progress of construction. Storage of materials, vehicular access, and all other construction activities are strictly prohibited within the limits of the tree protection fencing.
- Plans indicate general location and limits of removals. Contractor shall perform removals only as necessary for construction of proposed improvements.
- Tree stumps shall be ground to a minimum depth of 18" below existing grade or as required to properly perform the work.
- Contractor to protect all existing utilities and all other site features not designated for removal. Contractor is responsible for replacing/repairing any existing utilities or other site features damaged during construction to the original condition at no cost to the Owner.
- Contractor responsible for maintaining existing utility services (electrical, sanitary, storm) during construction unless otherwise indicated in the Contract Documents.
- Contractor shall coordinate all work so public sidewalk remains open throughout construction.

SITE PREPARATION AND REMOVALS LEGEND

-  Tree Protection Fence
-  Item to be removed

TREE INVENTORY

Number	Species	Size	Action	Property
1	Norway Maple	20"	remove	public
2	Siberian Elm	18"	remove	private
3	Siberian Elm	10"	remove	private
4	Siberian Elm	12"	remove	private
5	Siberian Elm	24"	remove	private
6	Mulberry	20"	remove	private
7	Siberian Elm	10"	remove	private
8	Siberian Elm	10"	remove	private
9	Siberian Elm	12"	remove	private
10	Siberian Elm	8"	remove	private
11	Siberian Elm	8"	remove	private
12	Siberian Elm	36"	remove	private
13	Norway Maple	20"	preserve	public
14	Hybrid Elm	20"	preserve	public
15	Purpleleaf Sand Cherry	4"	remove	private
16	Red Maple	8"	remove	private
17	Hybrid Elm	10"	remove	public
18	Hybrid Elm	18"	preserve	public
19	Hybrid Elm	18"	preserve	public
20	Hybrid Elm	12"	preserve	public
21	Hybrid Elm	36"	preserve	public
22	Saucer Magnolia	24"	remove	private
23	Saucer Magnolia	24"	remove	private
24	Honeylocust	12"	preserve	public
25	Honeylocust	25"	preserve	public
26	Crabapple	10"	remove	private
27	Honeylocust	18"	preserve	public
28	Honeylocust	18"	preserve	public
29	Honeylocust	18"	remove	public
30	Honeylocust	18"	remove	public
31	Honeylocust	24"	remove	public
32	Honeylocust	18"	preserve	public
33	Norway Maple	36"	remove	private
34	Winter King Hawthorn	20"	remove	private
35	Katsura Tree	24"	remove	private
36	Siberian Elm	15"	remove	private
37	Siberian Elm	8"	remove	private



- Notes:
- After trees are safely fenced nothing is to be raked out, cut, planted, stored, or disturbed inside the fence.
 - Remove protective fence only after all construction work has been finished.

1 Tree Protection Fence
Scale: 1/2" = 1'-0"

ISSUED
May 8, 2025

REVISIONS

No	Date	Issue
10-17-25		Per Evanston Comments
12-8-25		Per Evanston Comments

CHECKED BY _____ DRAWN BY _____

SHEET TITLE
**Tree Preservation
Plan**

SCALE IN FEET
1" = 20'



NORTH SHEET NUMBER

L1.0
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T 630.961.1787

hitchcockdesigngroup.com

PREPARED FOR
Kensington School

3434 Central Street
Evanston, IL 60201

PROJECT

Kensington School of Evanston

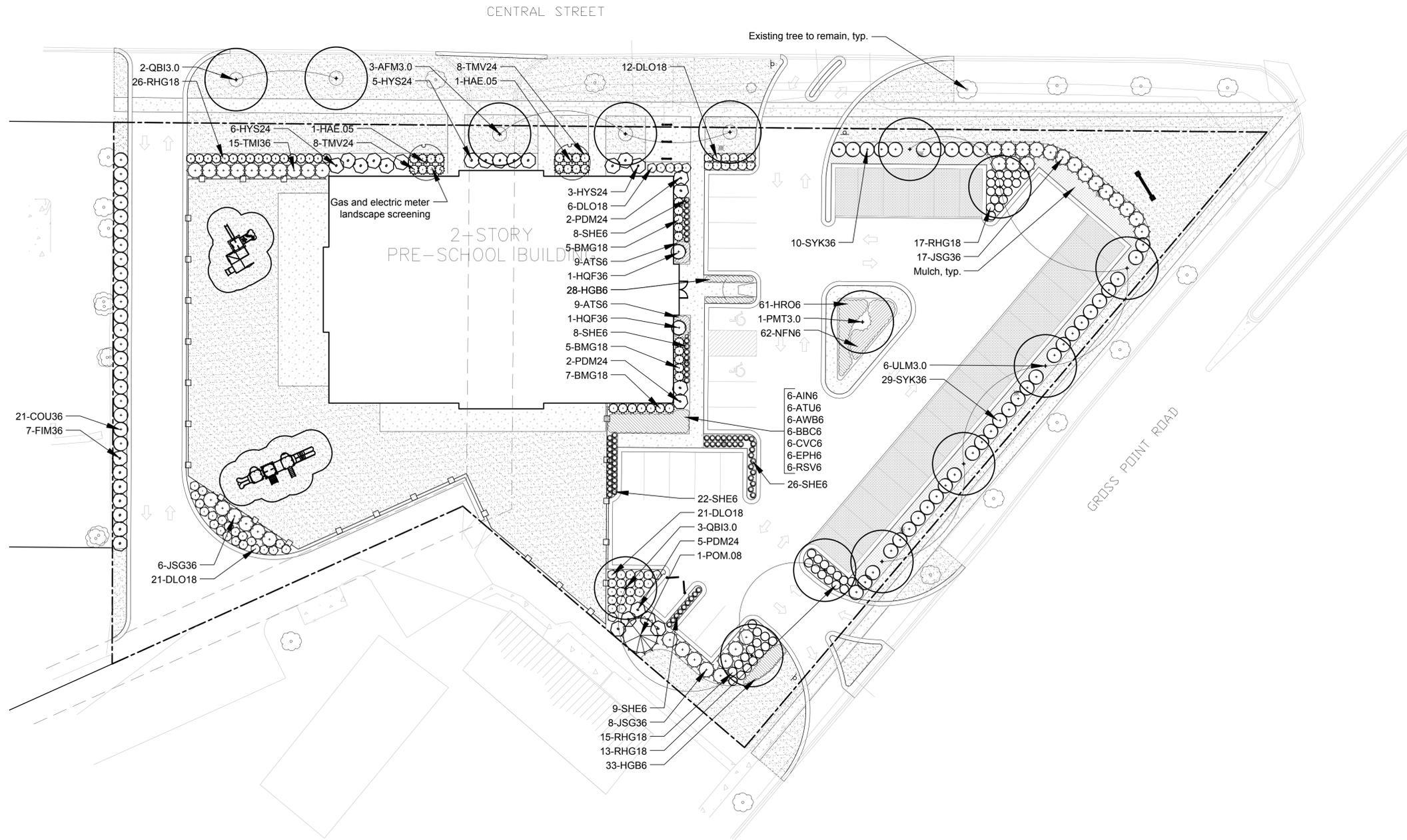
3434 Central Street
Evanston, IL 60201

PLANTING NOTES

- Sod limit line is approximate. Sod to limits of grading and disturbance. Contractor responsible for restoration of any unauthorized disruption outside of designated construction area.
- Contractor responsible for erosion control in all sodded areas.
- Tree mulch rings in turf areas are 5 foot diameter, typical. Contractor shall provide a mulch ring around all existing trees within the limit of work.
- Bedlines are to be spade cut to a minimum depth of 3 inches unless otherwise shown on the plans. Curved bedlines are to be smooth and not segmented.
- Do not locate plants within 10' of utility structures, or within 5' horizontally of underground utility lines unless otherwise shown on the plans.
- Plants and other materials are quantified and summarized for the convenience of the Owner and jurisdictional agencies only. Confirm and install sufficient quantities to complete the work as drawn and specified.
- Refer to specifications for additional conditions, standards and notes.

PLANTING LEGEND

-  Shade Tree, typical. See detail for installation.
-  Ornamental Tree, typical. See detail for installation.
-  Shrub, typical. See detail for installation.
-  Perennial, Groundcover and Annual, typical. See detail for installation.
-  Sod



ISSUED
May 8, 2025
REVISIONS

No	Date	Issue
10-17-25		Per Evanston Comments
12-8-25		Per Evanston Comments

CHECKED BY _____ DRAWN BY _____

SHEET TITLE
Planting Plan

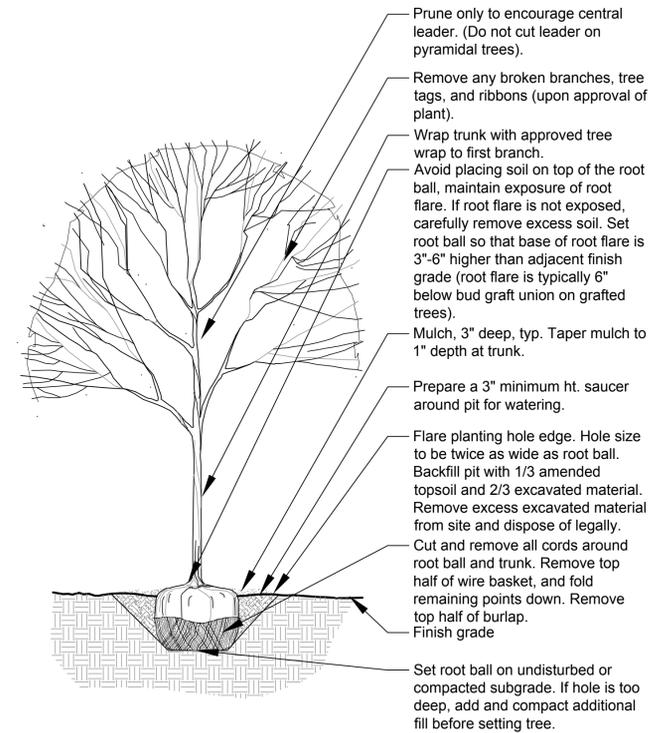
SCALE IN FEET
1" = 20'
0' 10' 20' 60'

NORTH SHEET NUMBER
L2.0

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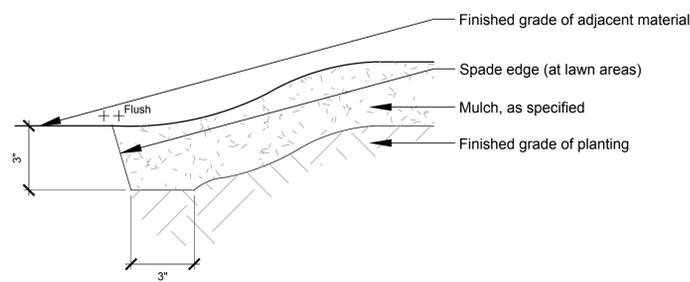
Plant Material List

Code	Botanical Name	Common Name	Size	Qty
Shade Trees				
AFM3.0	Acer x freemanii 'Marmo'	Marmo Freeman Maple	3" C	3
PMT3.0	Platanus x acerifolia 'Morton Circle'	Exclamation! London Planetree	3" C	1
QBI3.0	Quercus bicolor	Swamp White Oak	3" C	5
ULM3.0	Ulmus japonica x wilsoniana 'Morton'	Accolade™ Elm	3" C	6
Intermediate Trees				
HAE.05	Hamamelis vernalis 'Autumn Embers'	Autumn Embers Witchhazel	5' HT	2
Evergreen Trees				
POM.08	Picea omorika	Serbian Spruce	8' HT	1
Deciduous Shrubs				
COU36	Cotoneaster lucidus	Hedge Cotoneaster	36" HT	21
DLO18	Diervilla lonicera	Dwarf Bush-honeysuckle	18" HT	59
FIM36	Forsythia x intermedia 'Mindor'	Show Off Border Forsythia	36" HT	7
HQF36	Hydrangea paniculata 'Quick fire'	Quick Fire Hydrangea	36" HT	2
HY524	Hydrangea paniculata 'Vanilla Strawberry'	Vanilla Strawberry Panicked Hydrangea	24" HT	14
PDM24	Physocarpus opulifolius 'Donna May'	Little Devil Eastern Ninebark	24" HT	9
RHG18	Rhus aromatica 'Gro-Low'	Gro-Low Fragrant Sumac	18" HT	62
SYK36	Syringa patula 'Miss Kim'	Miss Kim Manchurian Lilac	36" HT	39
Evergreen Shrubs				
JSG36	Juniperus chinensis 'Sea Green'	Sea Green Chinese Juniper	36" HT	31
TMV24	Taxus x media 'Everlow'	Everlow Anglojap Yew	24" HT	16
TMI36	Taxus x media 'Hicksii'	Hicks Anglojap Yew	36" HT	10
Broadleaf Evergreens				
BMG18	Buxus x 'Glencoe'	Chicagoland Green Boxwood	18" HT	17
Perennials				
ATS6	Allium tanguticum 'Summer Beauty'	Summer Beauty Ornamental Chive	1 GAL	18
AIN6	Asclepias incarnata	Swamp Milkweed	1 GAL	6
ATU6	Asclepias tuberosa	Butterfly Milkweed	1 GAL	6
AWB6	Aster 'Woods Blue'	Woods Blue Aster	1 GAL	6
BBC6	Buddleia 'Blue Chip'	Lo & Behold Dwarf Butterfly Bush	1 GAL	6
CVC6	Coreopsis verticillata 'Crème Brulee'	Crème Brulee Threadleaf Coreopsis	1 GAL	6
EPH6	Echinacea purpurea 'Kim's Knee High'	Kim's Knee High Purple Coneflower	1 GAL	6
HGB6	Hemerocallis 'Going Bananas'	Going Bananas Daylily	1 GAL	61
HRO6	Hemerocallis 'Rosy Returns'	Rosy Returns Daylily	1 GAL	61
NFN6	Nepeta x faassenii 'Novanepjun'	Junior Walker Catmint	1 GAL	62
RSV6	Rudbeckia fulgida var. speciosa 'Viette's Little Suzy'	Viette's Little Suzy Black-eyed Susan	1 GAL	6
SHE6	Sporobolus heterolepis	Prairie Dropseed	1 GAL	73



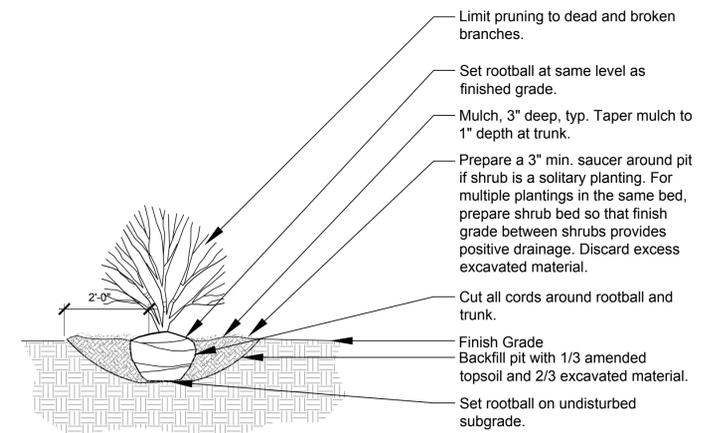
1 Deciduous Tree Planting

Scale: 1/2" = 1'-0"



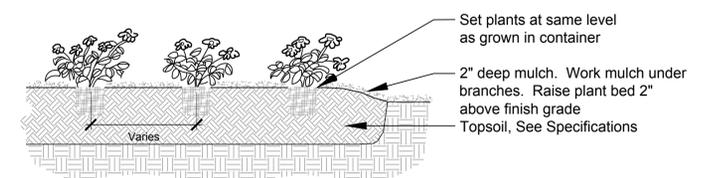
4 Planting Bed Edge

Scale: 3" = 1'-0"



2 Shrub Planting

Scale: 1/2" = 1'-0"



3 Perennial and Groundcover Planting

Scale: 1/2" = 1'-0"



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PREPARED FOR
Kensington School

3434 Central Street
Evanston, IL 60201

PROJECT

Kensington School of Evanston

3434 Central Street
Evanston, IL 60201

ISSUED
May 8, 2025
REVISIONS

No	Date	Issue
	10-17-25	Per Evanston Comments
	12-8-25	Per Evanston Comments

CHECKED BY _____ DRAWN BY _____

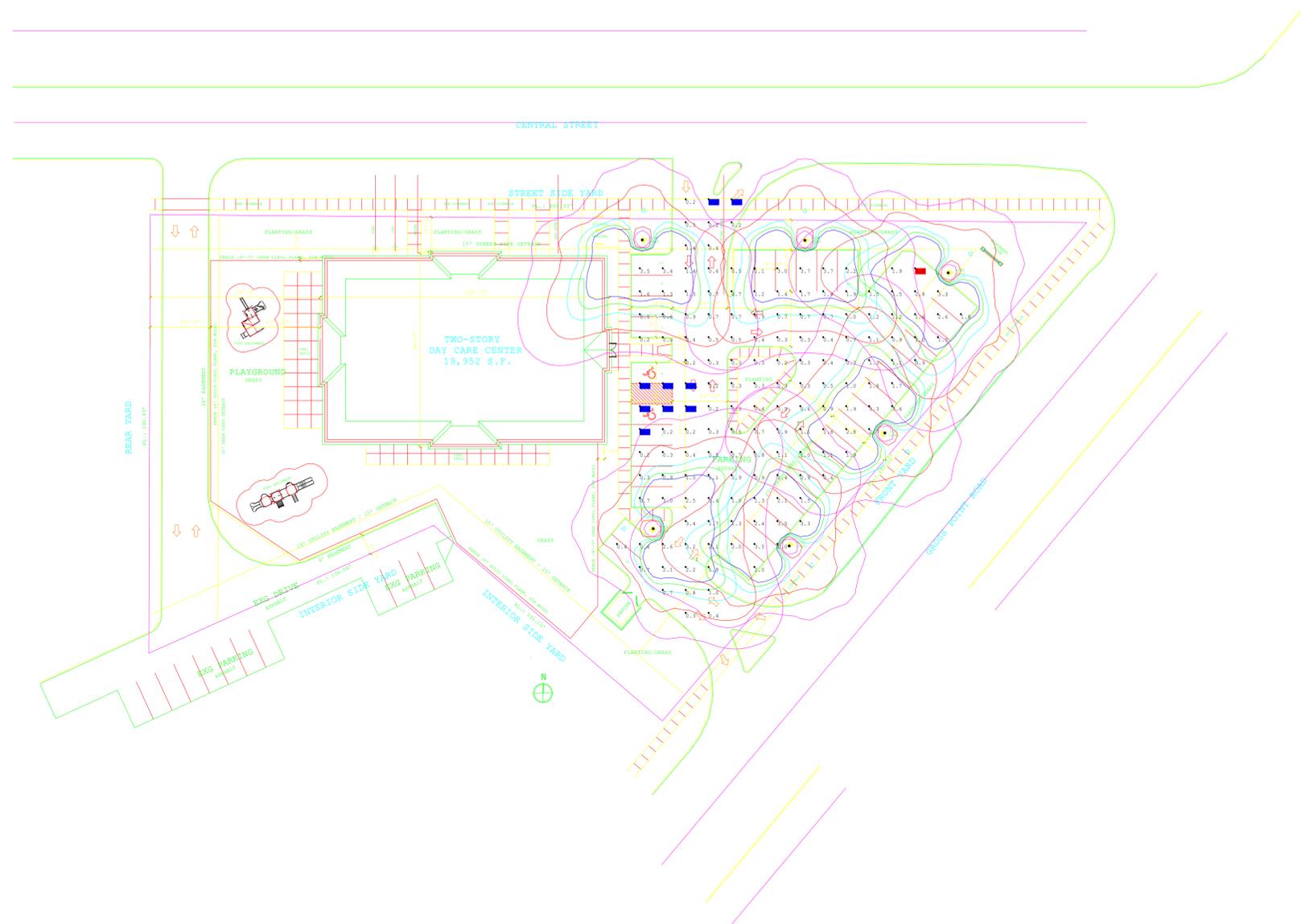
SHEET TITLE Planting Details

SCALE IN FEET
as noted



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17. Photometric Plans & Details



ISSUE: 5-8-25 EVANSTON PD SUBMISSION

Luminaire Schedule						
Symbol	Qty	Label	Arrangement	Total Lamp Lumens	LLF	Description
●	6	0595XP4NL4X06040XXD	SINGLE	N.A.	0.810	R595-P4FL-IV-60 (SSL)-7030-15.5' LC

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
PARKING LOT	Illuminance	Fc	1.17	3.8	0.1	11.70	38.00



King Luminaire StressCrete
Est. 1953



THE STRESSCRETE GROUP

Manufacturing Locations:

Burlington, Ontario: 1-800-268-7809
 Northport, Alabama: 1-800-435-6563
 Atchison, Kansas: 1-800-837-1024
 Jefferson, Ohio: 1-800-268-7809

PROJECT/FILE NAME:
EVANSTON, IL - KENSINGTON SCHOOL

DATE: MJ	CALC. BY: 3-29-22	AGI FILE NAME: KENSINGTON SCHOOL.AGI
--------------------	-----------------------------	--

REV: A **REVISION DATE/DESCRIPTION:**
04/05/23- CAD FILE REVISED, FIXTURES WERE TYPE III

REV.	ALTERATION	DATE	BY

LUMINAIRE SPECIFICATIONS

CATALOGUE NO.: K595-P4NL-III-60(SSL)
-7030-120:277-K30-4K

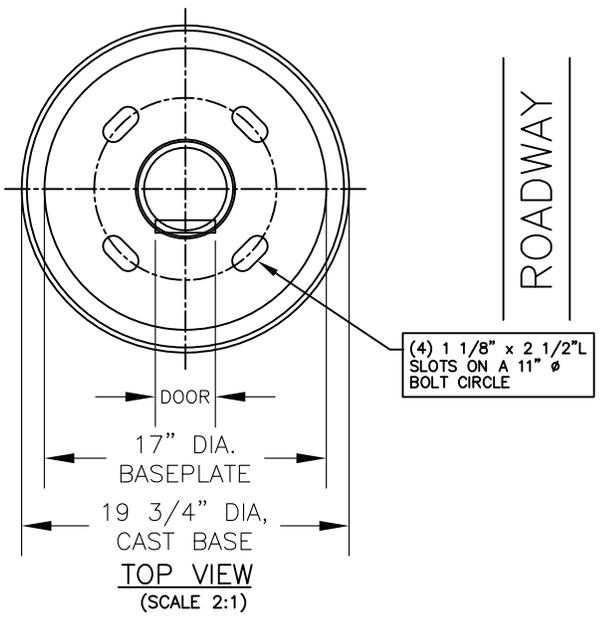
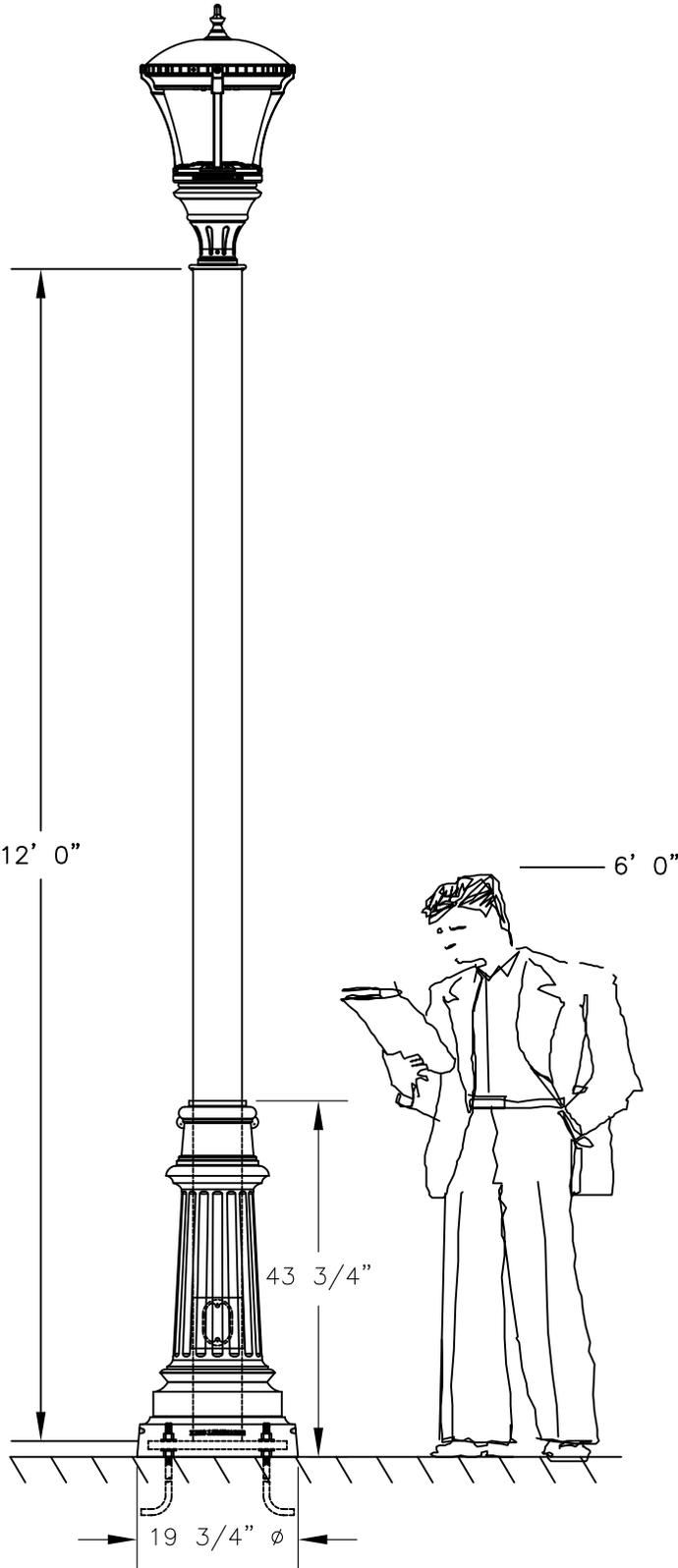
QUANTITY:
OPTICAL SYSTEM: P4 OPTIC NO LENS
IES CLASS.: TYPE III
WATTAGE: 60W (7030 SERIES)
SOLID STATE LIGHTING

LINE VOLTAGE: 120:277V
LIGHT SOURCE: LIGHT EMITTING DIODE
CCT: 4000K
POLE ADAPTOR: K30
PAINT: TEXTURED BLACK
OPTIONS: C/W #1 FINIAL

POLE SPECIFICATIONS

CATALOGUE NO.: KSB52-RE-A-12-0.12

QUANTITY:
SHAFT MATERIAL: ALUM. ROUND EXTRUDED
BASE MATERIAL: CAST ALUM. 2 PC BASE
POLE TIP SIZE: 6" DIA.
POLE HEIGHT: 14' 0"
BOLT CIRCLE: ** 11" ϕ **
ANCHOR BOLTS: (4) 1" x 36" LONG
BOLT PROJECTION: 4 1/8" HIGH
PAINT: TEXTURED BLACK
OPTIONS: -



CUSTOMER APPROVAL & DATE:



King Luminaire • StressCrete • Est. 1953

STRESSCRETE GROUP

Manufacturing Locations:

Burlington, Ontario 1-800-268-7809

Northport, Alabama 1-800-435-6563

Atchison, Kansas 1-800-837-1024

Jefferson, Ohio 1-800-268-7809

PROJECT/CUSTOMER: KENSINGTON SCHOOL

DRAWN BY: GR	AT: SC1	CHECKED BY:	DATE: 09/23/21	REVISION:
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DRAWING TYPE: CONCEPT DWG. DRAWING NUMBER: Q-2109072-1

Notes:
Although rare, oscillating loads severe enough to cause damage can occasionally occur in structures of all types. Because they are influenced by many factors not provided to StressCrete, these loads are unpredictable. The users maintenance program should include observation for excessive vibration & examination for any structural damage. The StressCrete warranty specifically excludes fatigue failure or similar phenomena resulting from induced vibration, oscillation or resonance associated with the eccentric rotation of attached equipment.

DARK SKY FRIENDLY

IDA Certified Luminaires



These dark sky friendly luminaires offer minimal glare while reducing light trespass and skyglow, and are IDA Certified for specifications that are a maximum of 3000K CCT and without glass lenses.



Featuring our high performance P4 LED engine which provides enhanced light quality and control, improved spacing and high efficacy while achieving energy savings and reduced maintenance costs.

Contemporary and Octagonal Styles

These classic luminaires offer exceptional lighting solutions in various designs that are suitable for modern, traditional and historical architectural spaces. Popular with municipalities for city streets, parks and schools plus commercial areas and waterfronts.



K137 Yarmouth
(No Globe)



K595 Aristocrat



K596 Clarion



K54 Cleveland Jr.
(No glass lenses)



K56 Cleveland/Tudor
(Post Top or Pendant,
no glass lenses)



K427 Revolve

K580 SKYLINE Series

A completely modular system that will accentuate the outdoor modern space. Classic sleek lines, beautiful modern spinnings, quality castings, a high performance LED optical system and energy efficiency have come together to deliver a contemporary lighting solution. Each style is available with any spinning option, and 2, 3, or 4 strut designs.



K581 Cobra Strut



K582 Pillar Strut



K583 Curve Strut



K584 Treble Strut



K585 Radius Strut

2-19-2020

18. Traffic Study

Traffic Impact Study

Proposed Kensington School

Evanston, Illinois



Prepared For:



Kenig, Lindgren, O'Hara, Aboona, Inc.

April 22, 2025

1. Introduction

This report summarizes the methodologies, results, and findings of a traffic impact study conducted by Kenig, Lindgren, O’Hara, Aboona, Inc. (KLOA, Inc.) for a proposed Kensington School to be located in Evanston, Illinois. The site, which currently contains the Unity Church on the North Shore, is located in the southwest quadrant of the intersection of Gross Point Road with Central Street. As proposed, the site will be redeveloped with a two-story, approximate 20,000 square-foot Kensington School and 38 parking spaces. The Kensington School will provide a full-day preschool program with a maximum of 165 students and approximately 23 staff members and will operate between 6:30 A.M. and 6:30 P.M. Access to the development is proposed to be provided via a restricted right-turn in and right-turn out access drive located on Gross Point Road and a restricted inbound left-turn and outbound right-turn only access drive on Central Street. In addition, an access road will be located on the south side of Central Street at the west end of the site that will serve the three multi-family buildings located directly southeast of the site.

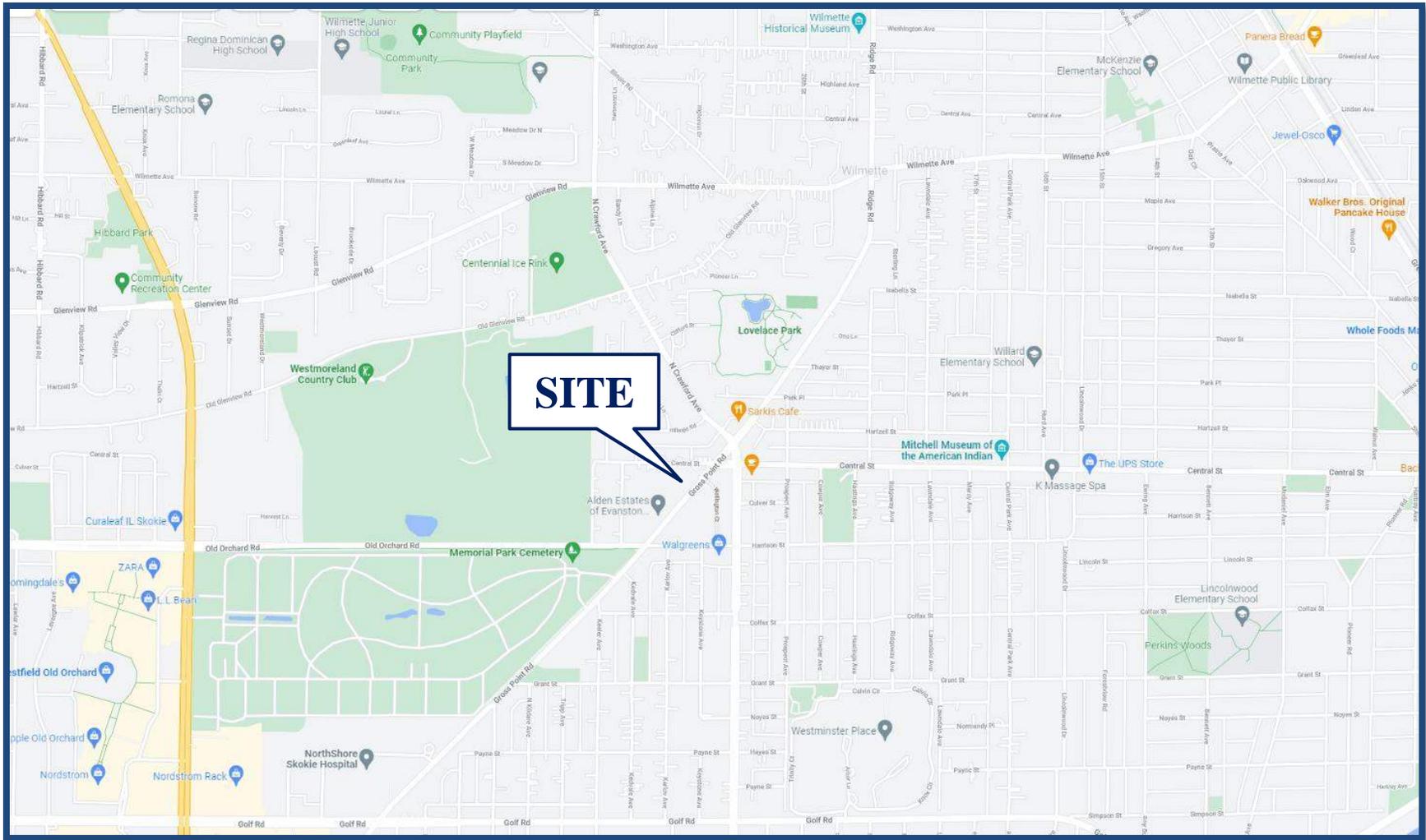
The purpose of this study was to examine background traffic conditions, assess the impact that the proposed school will have on traffic conditions in the area, and determine if any roadway or access improvements are necessary to accommodate the traffic generated by the proposed school. **Figure 1** shows the location of the site and **Figure 2** shows an aerial view of the site.

The sections of this report present the following:

- Existing roadway conditions
- A description of the proposed school
- Directional distribution of the school traffic
- Vehicle trip generation for the school
- Future traffic conditions including access to the school
- Traffic analyses for the weekday morning and weekday evening peak hours
- Recommendations with respect to adequacy of the site access and adjacent roadway system

Traffic capacity analyses were conducted for the weekday morning and weekday evening peak hours for the following conditions:

1. Year 2022 Base Conditions – Analyzes the capacity of the existing roadway system using existing peak hour traffic volumes in the surrounding area adjusted to account for the ongoing Covid 19 pandemic.
2. Year 2031 No-Build Conditions – Analyzes the capacity of the existing roadway system assuming Year 2022 base traffic volumes increased by an ambient area growth factor not attributable to any particular development.
3. Year 2031 Projected Conditions – Analyzes the capacity of the future roadway system using the projected traffic volumes that include the Year 2022 base traffic volumes, background development traffic growth, and the traffic estimated to be generated by the proposed school.



Site Location

Figure 1



Aerial View of Site

Figure 2

2. Existing Conditions

Existing traffic and roadway conditions were documented based on field visits and traffic counts conducted by KLOA, Inc. The following provides a detailed description of the physical characteristics of the roadways including geometry and traffic control, adjacent land uses, and peak hour traffic flows along area roadways.

Site Location

The site, which currently contains the Unity Church on the North Shore, is bounded by Central Street on the north, Gross Point Road on the east, multi-family buildings on the south, and a City of Evanston water tower and single-family homes on the west. Land uses within the vicinity of the site include single-family homes to the north and west and multi-family buildings to the south and east. Commercial developments are located east of the site along Central Street and at the Gross Point Road, Crawford Avenue, and Central Street intersections.

Existing Roadway System Characteristics

The characteristics of the existing roadways that surround the site are illustrated in **Figure 3** and described below.

Gross Point Road is a northeast-to-southwest, minor arterial road that has two lanes in each direction southwest of Crawford Avenue and one lane and one bike lane in each direction northeast of Crawford Avenue. Parking is generally prohibited on both sides of the road. At its signalized intersection with Central Street, Gross Point Road has a shared left-turn/through lane and a separate right-turn lane on the northeast-bound approach and a through lane and a shared through/right-turn lane on the southwest-bound approach. At its signalized intersection with Crawford Avenue, Gross Point Road has a shared left-turn/through lane and a shared through/right-turn lane on the northeast-bound approach and a separate left-turn lane and a shared through/right-turn lane on the southwest-bound approach. Gross Point Road is under the jurisdiction of the Illinois Department of Transportation (IDOT) southwest of Crawford Avenue and the City of Evanston northeast of Crawford Avenue, has an Annual Average Daily Traffic (AADT) volume of 6,850 vehicles (IDOT 2022), and has a posted speed limit of 30 mph.

Central Street is an east-west road. East of Gross Point Road, Central Street is classified as a minor arterial road that generally has one lane and one bike lane in each direction. West of Gross Point Road, Central Street is classified as a local road that has one lane in each direction. Parking is generally permitted on both sides of the road east of Crawford Avenue and permitted on the south side of the road west of Gross Point Road. At its signalized intersection with Crawford Avenue, Central Street has a separate left-turn lane, a through lane, and a shared through/right-turn lane on the westbound approach and a through lane and a shared through/right-turn lane on the eastbound approach. At its signalized intersection with Gross Point Road, Central Street has a separate left-turn lane and a shared left-turn/through lane on the westbound approach and a shared left-turn/through/right-turn lane on the eastbound approach. Central Street is under the jurisdiction of the City of Evanston, has an AADT volume of 10,300 vehicles (IDOT 2022) east of Crawford Avenue, and has a posted speed limit of 25 mph.

Crawford Avenue is generally a north-south road. South of Gross Point Road, Crawford Avenue is classified as a minor arterial road that generally has two lanes in each direction divided by a median. Northwest of Gross Point Road, Crawford Avenue is classified as a major collector road and has one lane in each direction. Parking is generally permitted on both sides of the road northwest of Gross Point Road and prohibited on both sides of the road south of Gross Point Road. At its signalized intersection with Gross Point Road, Crawford Avenue has two through lanes and a separate right-turn lane on the northwest-bound approach and a shared left-turn/through lane and a shared through/right-turn lane on the southeast-bound approach. At its signalized intersection with Central Street, Crawford Avenue has a separate left-turn lane, a through lane, and a shared through/right-turn lane on both approaches. South of Gross Point Road, Crawford Avenue is under the jurisdiction of IDOT, has an AADT volume of 7,100 vehicles (IDOT 2022), and has a posted speed limit of 25 mph. Northwest of Gross Point Road, Crawford Avenue is under the jurisdiction of the City of Evanston, has an AADT volume of 4,400 vehicles (IDOT 2023), and has a posted speed limit of 30 mph.

Wellington Court is generally a north-south, local road that has one lane in each direction. Parking is prohibited on both sides of the road. At its intersection with Gross Point Road, Wellington Court has a shared left-turn/right-turn lane that is under stop sign control.

Public Transportation

The following summarizes the bus routes serving the area:

- *Pace Bus Route 421*, which runs along Wilmette Avenue, has a local bus stop approximately 0.6 miles from the site.
- *Pace Bus Route 208*, which runs along Golf Road, has a local bus stop approximately 0.7 miles from the site.

Bike Accommodations

Bike lanes are provided on both sides of (1) Central Street east of Crawford Avenue and (2) Gross Point Road northeast of Crawford Avenue. Further, Crawford Avenue northwest of Central Street is a designated bike route. In addition, a Divvy bike station that can accommodate 13 bikes is located at the intersection of Central Street with Lincolnwood Drive.

Pedestrian Accommodations

Sidewalks are generally provided on both sides of the roads in the vicinity of the site except on the north side of Central Street west of Gross Point Road. Standard style crosswalks and pedestrian traffic signals are located at all three signalized intersections in the Gross Point Road/Crawford Avenue/Central Street triangle.

Existing Traffic Volumes

In order to determine current traffic conditions in the vicinity of the site, KLOA, Inc. conducted peak period vehicle, pedestrian, and bicycle counts at the following intersections:

- Gross Point Road with Crawford Avenue
- Gross Point Road with Central Street
- Gross Point Road with Wellington Court
- Central Street with Crawford Avenue
- Central Street with the existing church parking lot

The traffic counts were conducted during the weekday morning (7:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods on Tuesday, January 25, 2022. The results of the traffic counts showed that the weekday morning peak hour of traffic occurs from 7:45 A.M. to 8:45 A.M. and the evening peak hour of traffic occurs from 4:45 P.M. to 5:45 P.M.

To determine if the ongoing Covid 19 pandemic had any impact on the area traffic volumes, the 2022 traffic counts were compared to previous 2018 traffic counts conducted by IDOT. Based on the results of the comparison, the weekday morning and evening traffic volumes were increased by 20 percent. **Figure 4** illustrates the Year 2022 base peak hour vehicle traffic volumes and **Figure 5** illustrates the existing pedestrian and bicycle traffic volumes. Copies of the traffic counts are located in the Appendix.

Crash Data Analysis

KLOA, Inc. obtained crash data¹ for the past five years (2019 to 2023) for the following intersections:

- Gross Point Road with Crawford Avenue
- Gross Point Road with Central Street
- Central Street with Crawford Avenue
- Gross Point Road with Wellington Court

Tables 1 through **3** summarize the results of the crash data for the intersections of Gross Point Road with Crawford Avenue, Gross Point Road with Central Street, and Central Street with Crawford Avenue. Given the proximity of the three intersections in the Gross Point Road, Crawford Avenue, and Central Street triangle, the crash data was only provided within 100 feet of each intersection. A review of the crash data indicated that a total of four crashes occurred at the intersection of Gross Point Road with Wellington Court during the five-year period. The review also indicated that no fatal crashes occurred at any of the intersections in the five-year period.

¹ IDOT DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.

Table 1
GROSS POINT ROAD WITH CRAWFORD AVENUE – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2019	0	0	1	0	0	0	0	1
2020	2	0	1	0	0	0	0	3
2021	1	0	0	0	0	1	1	3
2022	1	0	0	0	0	4	0	5
2023	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>
Total	4	0	2	0	0	5	1	12
Average	<1.0	<1.0	<1.0	<1.0	<1.0	1.0	<1.0	2.4

Table 2
GROSS POINT ROAD WITH CENTRAL STREET – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2019	0	0	0	0	0	0	0	0
2020	0	0	0	0	0	1	0	1
2021	0	0	0	2	0	0	0	2
2022	0	0	0	0	0	1	0	1
2023	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>0</u>
Total	0	0	0	2	0	2	0	4
Average	<1.0	<1.0	<1.0	<1.0	<1.0	<1.0	<1.0	<1.0

Table 3
CENTRAL STREET WITH CRAWFORD AVENUE – CRASH SUMMARY

Year	Type of Crash Frequency							Total
	Angle	Head On	Object	Rear End	Sideswipe	Turning	Other	
2019	1	0	0	0	0	0	0	1
2020	2	0	0	0	0	0	0	2
2021	3	0	0	0	0	2	0	5
2022	7	0	0	1	0	0	0	8
2023	<u>5</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>1</u>	<u>2</u>	<u>1</u>	<u>9</u>
Total	18	0	0	1	1	4	1	25
Average	<3.6	0.0	<1.0	<1.0	<1.0	<1.0	<1.0	8.2

3. Traffic Characteristics of the Proposed School

In order to properly evaluate future traffic conditions in the surrounding area, it was necessary to determine the traffic characteristics of the proposed school, including the directional distribution and volumes of traffic that it will generate.

Proposed School Plan

As proposed, the site is to be redeveloped with a two-story, approximate 20,000 square-foot Kensington School. As proposed, the school will provide a full-day preschool program in 10 classrooms with a maximum enrollment of approximately 165 students and approximately 23 staff members. The school will operate between 6:30 A.M. and 6:30 P.M. According to the operator, most students will be dropped off between 6:30 and 8:30 A.M. and picked up between 4:00 and 6:00 P.M. All caregivers will be required to park and walk their students to and from the school. The school will have a total of 38 parking spaces.

Site Access

Access to the school is to be provided via the following two access drives:

- A restricted right-turn in and right-turn out access drive located on the west side of Gross Point Road approximately 385 feet southwest of Central Street. As proposed, the access drive will provide one inbound lane and one outbound lane channelized and signed to prohibit left-turn movements. The outbound lane will be under stop sign control.
- A restricted left-turn in and right-turn out only access drive located on the south side of Central Street approximately 250 feet west of Gross Point Road. As proposed, this access drive will provide one inbound lane and one outbound lane that will be channelized and signed to permit inbound left-turn and outbound right-turn movements only. The outbound lane will be under stop sign control.

In addition, a full-movement access road is proposed to be located on the south side of Central Street approximately 480 feet west of Gross Point Road that will serve the three multi-family buildings and their 13 parking spaces located directly southeast of the site. This access drive will provide one inbound lane and one outbound lane with the outbound lane under stop sign control.

A copy of the preliminary site plan depicting the proposed school is included in the Appendix.

Directional Distribution

The directions from which parents and employees of the proposed school will approach and depart the site were estimated based on existing travel patterns and the operation of the roadway system. **Figure 6** illustrates the directional distribution of the school-generated traffic.

Estimated Site Traffic Generation

The traffic to be generated by the proposed school was estimated based on surveys of an existing Kensington School located in Elmhurst, Illinois. According to the operator, the Elmhurst school has nine classrooms and a similar operation to that of the proposed Evanston school. The surveys were performed on Wednesday, December 9, 2015 during the morning (6:00 A.M. to 9:00 A.M.) and evening (4:00 P.M. to 6:00 P.M.) peak periods. Given that the proposed school will provide 10 classrooms, the surveyed volumes were increased by 10 percent. **Table 4** tabulates the vehicle trips anticipated to be generated by the proposed school during the weekday morning and evening peak hours.

Table 4

ESTIMATED SCHOOL-GENERATED TRAFFIC VOLUMES

Type/Size	Weekday Morning Peak Hour			Weekday Evening Peak Hour		
	In	Out	Total	In	Out	Total
Proposed Kensington School	59	59	118	58	65	123

Further, the school is typically closed after 6:30 P.M. on weeknights and on weekends. As such, other than during the morning and evening peak periods, the school generates a very limited volume of traffic during weekdays and little, if any, traffic on weekday evenings or weekends.

4. Projected Traffic Conditions

The total projected traffic volumes include the base traffic volumes, increase in background traffic due to ambient growth, and the traffic estimated to be generated by the proposed school.

School Traffic Assignment

The estimated weekday morning and evening peak hour traffic volumes that will be generated by the proposed school were assigned to the roadway system in accordance with the previously described directional distribution (Figure 6). The traffic assignment for the school is illustrated in **Figure 7**.

Background Traffic Conditions

The Year 2022 base traffic volumes (Figure 4) were increased by a regional growth factor to account for the increase in existing traffic related to regional growth in the area (i.e., not attributable to any particular planned development). Based on 2050 ADT projections provided by the Chicago Metropolitan Agency for Planning (CMAP), the existing traffic volumes in the area are projected to increase by an average compound annual growth rate of 0.46 percent per year. As such, all the traffic volumes were increased by a total of 4.2 percent (from 2022 to 2031) to represent Year 2031 conditions (buildout of the facility plus five years). A copy of the CMAP projections letter is included in the Appendix. **Figure 8** illustrates the 2031 no-build traffic volumes.

Total Projected Traffic Volumes

The school-generated traffic was added to the Year 2022 base traffic volumes accounting for background growth to determine the Year 2031 projected traffic volumes. Access to the 13 parking spaces serving the three multi-family buildings directly southwest of the site is provided via the existing church parking lot. With the redevelopment of the site, access to the parking spaces is proposed to be provided via a dedicated access road located at the west end of the site. As such, the existing traffic using these parking spaces was redistributed to the proposed access road. **Figure 9** shows the Year 2031 total traffic volumes.

5. Traffic Analysis and Recommendations

The following provides an evaluation conducted for the weekday morning and weekday evening peak hours. The analysis includes conducting capacity analyses to determine how well the roadway system and access drives are projected to operate and whether any roadway improvements or modifications are required.

Traffic Analyses

Roadway and adjacent or nearby intersection analyses were performed for the weekday morning and weekday evening peak hours for the Year 2022 base, Year 2031 no-build, and Year 2031 total projected conditions.

The traffic analyses were performed using the methodologies outlined in the Transportation Research Board's *Highway Capacity Manual (HCM)*, 7th Edition and analyzed using Synchro/SimTraffic 11 software.

The analyses for the unsignalized intersections determine the average control delay to vehicles at an intersection. Control delay is the elapsed time from a vehicle joining the queue at a stop sign (includes the time required to decelerate to a stop) until its departure from the stop sign and resumption of free flow speed. The methodology analyzes each intersection approach controlled by a stop sign and considers traffic volumes on all approaches and lane characteristics.

The ability of an intersection to accommodate traffic flow is expressed in terms of level of service, which is assigned a letter from A to F based on the average control delay experienced by vehicles passing through the intersection. The *Highway Capacity Manual* definitions for levels of service and the corresponding control delay for signalized intersections and unsignalized intersections are included in the Appendix of this report.

Summaries of the traffic analysis results showing the level of service and overall intersection delay (measured in seconds) for the Year 2022 base, Year 2031 no-build, and Year 2031 total projected conditions are presented in **Tables 5** through **10**. A discussion of each intersection follows. Summary sheets for the capacity analyses are included in the Appendix.

Table 5

CAPACITY ANALYSIS RESULTS – GROSS POINT ROAD WITH CENTRAL STREET – SIGNALIZED

	Peak Hour	Eastbound	Westbound		Northeast-bound		Southwest-bound	Overall
		L/T/R	L	L/T	L/T	R	T/R	
Year 2022 Base Conditions	Weekday Morning	D 45.4	A 8.5	A 8.4	D 48.7	A 8.0	B 15.3	B 18.6
			A – 8.4		C – 24.4			
	Weekday Evening	D 48.1	B 15.2	B 15.2	D 51.1	A 7.6	B 19.2	C 22.0
			B – 15.2		C – 29.0			
Year 2031 No-Build Conditions	Weekday Morning	D 45.5	A 9.4	A 9.4	D 48.0	A 7.9	B 15.6	B 18.7
			A – 9.4		C – 24.1			
	Weekday Evening	D 48.1	B 16.9	B 16.9	D 49.8	A 7.2	B 18.6	C 22.1
			B – 16.9		C – 28.3			
Year 2031 Projected Conditions	Weekday Morning	D 53.5	B 11.8	B 11.8	D 47.9	A 8.4	B 16.6	C 21.2
			B – 11.8		C – 24.9			
	Weekday Evening	D 52.9	C 22.3	C 22.2	D 49.8	A 7.4	C 20.0	C 25.6
			C – 22.3		C – 29.1			
Letter denotes Level of Service L – Left Turn R – Right Turn Delay is measured in seconds. T – Through								

Table 6

CAPACITY ANALYSIS RESULTS – CENTRAL STREET WITH CRAWFORD AVENUE – SIGNALIZED

	Peak Hour	Eastbound		Westbound		Northbound		Southbound		Overall
		T/R	L	T/R	L	T/R	L	T/R		
Year 2022 Base Conditions	Weekday Morning	D 53.7	C 28.8	C 31.9	A 9.2	B 15.1	B 10.0	A 9.9	C 26.5	
			C – 31.6		B – 14.9		A – 10.0			
	Weekday Evening	D 53.6	C 34.0	D 36.9	A 8.0	B 13.8	A 7.8	A 10.0	C 27.1	
			D – 36.4		B – 13.6		A – 9.2			
Year 2031 No-Build Conditions	Weekday Morning	D 53.1	C 28.4	C 31.8	A 9.6	B 16.1	B 10.8	B 10.2	C 26.7	
			C – 31.4		B – 15.8		B – 10.5			
	Weekday Evening	D 53.1	C 34.1	D 37.2	A 8.2	B 14.3	A 8.0	B 10.1	C 27.3	
			D – 36.7		B – 14.0		A – 9.4			
Year 2031 Projected Conditions	Weekday Morning	D 53.8	C 27.9	C 31.7	A 9.9	B 16.6	B 11.1	B 11.2	C 27.4	
			C – 31.3		B – 16.2		B – 11.2			
	Weekday Evening	D 53.3	C 33.6	D 36.9	A 8.5	B 14.8	A 8.4	B 11.2	C 28.0	
			D – 36.4		B – 14.4		B – 10.2			
Letter denotes Level of Service L – Left Turn R – Right Turn Delay is measured in seconds. T – Through										

Table 7

CAPACITY ANALYSIS RESULTS – GROSS POINT ROAD WITH CRAWFORD AVENUE – SIGNALIZED

	Peak Hour	Southeast-bound		Northwest-bound		Northeast-bound		Southwest-bound		Overall
		L/T/R		T	R	L/T/R		L	T/R	
Year 2022 Base Conditions	Weekday Morning	A 9.3		A 4.5	A 1.1	C 31.6		C 32.5	D 35.3	B 17.7
				A – 3.3				C – 34.3		
	Weekday Evening	A 10.0		B 10.2	A 3.1	C 24.8		D 36.2	D 37.2	C 20.3
				A – 7.9				D – 36.8		
Year 2031 No-Build Conditions	Weekday Morning	A 9.7		A 4.7	A 1.1	C 31.3		C 32.5	D 35.0	B 17.7
				A – 3.4				C – 34.1		
	Weekday Evening	B 10.4		B 10.5	A 3.1	C 24.6		D 36.5	D 37.3	C 20.4
				A – 8.1				D – 37.0		
Year 2031 Projected Conditions	Weekday Morning	A 10.1		A 4.9	A 1.2	E 55.2		C 31.7	C 34.7	C 22.7
				A – 3.6				C – 33.6		
	Weekday Evening	B 10.9		B 11.0	A 3.1	D 51.4		D 35.5	D 36.4	C 25.4
				A – 8.5				D – 36.0		
Letter denotes Level of Service L – Left Turn R – Right Turn Delay is measured in seconds. T – Through										

Table 8
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED
 YEAR 2022 BASE CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Gross Point Road with Wellington Court				
• Westbound Approach	B	13.7	B	11.7
• Southwest-Bound Left Turn	A	8.8	A	8.5
LOS = Level of Service Delay is measured in seconds.				

Table 9
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED
 YEAR 2031 NO-BUILD CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Gross Point Road with Wellington Court				
• Westbound Approach	B	13.9	B	11.9
• Southwest-Bound Left Turn	A	8.9	A	8.6
LOS = Level of Service Delay is measured in seconds.				

Table 11
 CAPACITY ANALYSIS RESULTS – UNSIGNALIZED
 YEAR 2031 TOTAL CONDITIONS

Intersection	Weekday Morning Peak Hour		Weekday Evening Peak Hour	
	LOS	Delay	LOS	Delay
Gross Point Road with Wellington Court				
• Westbound Approach	B	14.3	B	12.1
• Southwest-Bound Left Turn	A	9.0	A	8.7
Gross Point Road with Right-Turn in/Right-Turn Out Access Drive				
• Eastbound Approach	B	10.4	B	11.3
Central Street with Left-Turn In/Right-Turn Out Access Drive				
• Westbound Left-Turn	A	7.3	A	7.3
• Northbound Right Turn	A	8.6	A	8.5
Central Street with Multi-Family Buildings Access Road				
• Westbound Left-Turn	A	7.3	A	7.2
• Northbound Right Turn	A	8.4	A	8.4
LOS = Level of Service Delay is measured in seconds.				

Discussion and Recommendations

The following is an evaluation of the analyzed intersections based on the projected traffic volumes and the capacity analyses performed.

Gross Point Road with Central Street

The results of the capacity analysis indicate that this intersection currently operates at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour. Further, all the intersection movements are operating at LOS D or better. Assuming the Year 2031 no-build conditions, the intersection is projected to continue to operate at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. Assuming the Year 2031 total conditions, the intersection is projected to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Central Street with Crawford Avenue

The results of the capacity analysis indicate that this intersection currently operates at an overall LOS C during the weekday morning and evening peak hours. Assuming the Year 2031 no-build conditions, the intersection is projected to continue to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. Assuming the Year 2031 total conditions, the intersection is projected to continue to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Gross Point Road with Crawford Avenue

The results of the capacity analysis indicate that this intersection currently operates at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour. Further, all the intersection movements are operating at LOS D or better. Assuming the Year 2031 no-build conditions, the intersection is projected to continue to operate at an overall LOS B during the weekday morning peak hour and LOS C during the weekday evening peak hour and all the intersection movements are projected to continue to operate at LOS D or better during the peak hours. Assuming the Year 2031 total conditions, the intersection is projected to operate at an overall LOS C during the weekday morning and evening peak hours. Further, all the intersection movements are projected to continue to operate on the threshold of a LOS D/E or better during the peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Gross Point Road with Wellington Court

The results of the capacity analysis indicate that the critical movements at this intersection currently operate at LOS B or better during the weekday morning and evening peak hours. Assuming the Year 2031 no-build conditions, the critical movements at this intersection are projected to continue to operate at LOS B or better during the weekday morning and evening peak hours. Assuming the Year 2031 total conditions, the critical movements at this intersection are projected to continue to operate at LOS B or better during the weekday morning and evening peak hours. As such, this intersection has sufficient reserve capacity to accommodate the traffic projected to be generated by the proposed school and no roadway improvements and/or traffic control modifications are required.

Gross Pointe Road with Right-Turn In/Right-Turn Out Access Drive

Access to and from the development is proposed via a restricted right-turn in/right-turn out access drive to be located on the west side of Gross Point Road approximately 345 feet southwest of Central Street. As proposed, the access drive will provide one inbound lane and one outbound lane that will be channelized and signed to restrict left-turn movements. The outbound lane will be under stop sign control. Assuming the Year 2031 total conditions, the outbound right-turn movement is projected to operate at LOS B during the weekday morning and evening peak hours. As such, the access drive will provide efficient and orderly access to and from the development with limited impact on Gross Point Road.

Central Street with Left-Turn In/Right-Turn Out Access Drive

Access from the development is proposed via a restricted left-turn in and right-turn out only access drive located on the south side of Central Street approximately 250 feet west of Gross Point Road. As proposed, this access drive will provide one inbound lane and one outbound lane that will be channelized and signed to permit inbound left-turn and outbound right-turn movements only. The outbound lane will be under stop sign control. Assuming the Year 2031 total conditions, the critical movements at this intersection are projected to operate at LOS A. As such, the access drive will provide efficient and orderly access to and from the development with limited impact on Central Street.

Central Street with Access Road to Multi-Family Buildings

Access to the three multi-family buildings and their 13 parking spaces located directly southeast of the site is proposed via a full-movement access road located on the south side of Central Street approximately 480 feet west of Gross Point Road. This access drive will provide one inbound lane and one outbound lane with the outbound lane under stop sign control. Assuming the Year 2031 total conditions, the critical movements at this intersection are projected to operate at LOS A. As such, the access road will provide efficient and orderly access to and from the three multi-family buildings and their 13 parking spaces with limited impact on Central Street.

6. Conclusion

Based on existing conditions and the traffic capacity analyses for the full buildout of the school, the findings and recommendations of this study are outlined below:

- The proposed Kensington School primarily generates traffic from 6:30 A.M. to 8:30 A.M. and from 4:00 P.M. to 6:00 P.M. Further, the school is typically closed after 6:30 P.M. on weeknights and on weekends. As such, other than during the morning and evening peak periods, the Kensington School generates a limited volume of traffic during weekdays and little, if any, traffic on weekday evenings or weekends.
- Access to the school is to be provided via the following two access drives:
 - A restricted right-turn in and right-turn out access drive located on the west side of Gross Point Road approximately 345 feet southwest of Central Street. As proposed, the access drive will provide one inbound lane and one outbound lane channelized and signed to prohibit left-turn movements. The outbound lane will be under stop sign control.
 - A restricted left-turn in and right-turn out only access drive located on the south side of Central Street approximately 250 feet west of Gross Point Road. As proposed, this access drive will provide one inbound lane and one outbound lane that will be channelized and signed to permit inbound left-turn and outbound right-turn movements only. The outbound lane will be under stop sign control.
- In addition, a full-movement access road is to be located on the south side of Central Street approximately 480 feet west of Gross Point Road that will serve the three multi-family buildings and their 13 parking spaces located directly southeast of the site. This access drive will provide one inbound lane and one outbound lane with the outbound lane under stop sign control.
- The access drives will provide efficient and orderly access with limited impact on the existing roadway system
- The results of the capacity analyses show that the roadway system has sufficient reserve capacity to accommodate the traffic to be generated by the proposed school and no geometric improvements and/or traffic control modifications are required.

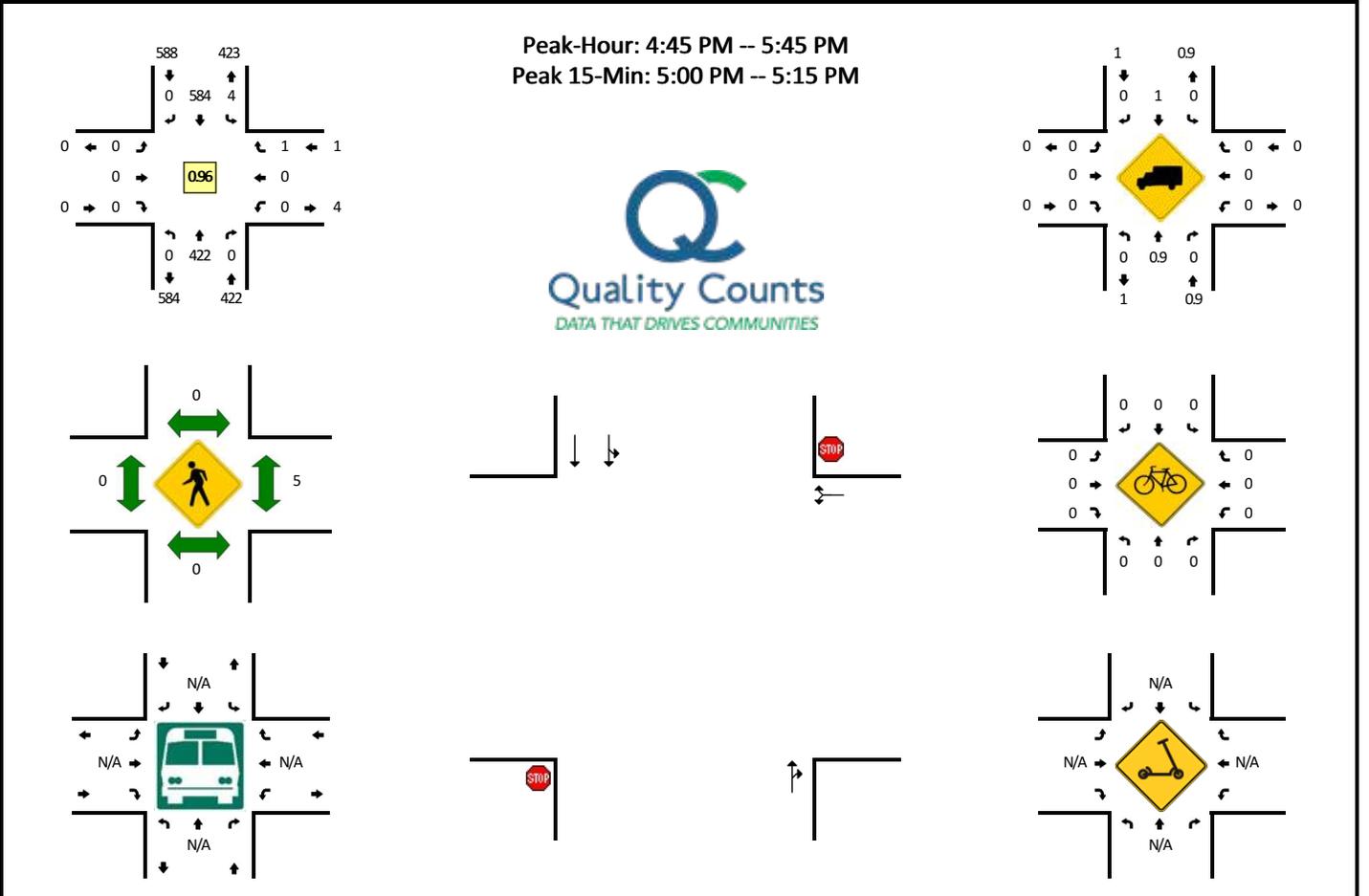
Appendix

Traffic Count Summary Sheets
Site Plan
CMAP Projections Letter
Level of Service Table
Capacity Analysis Summary Sheets

Traffic Count Summary Sheets

LOCATION: Gross Point Rd -- Wellington Ct
CITY/STATE: Evanston, IL

QC JOB #: 15687808
DATE: Tue, Jan 25 2022

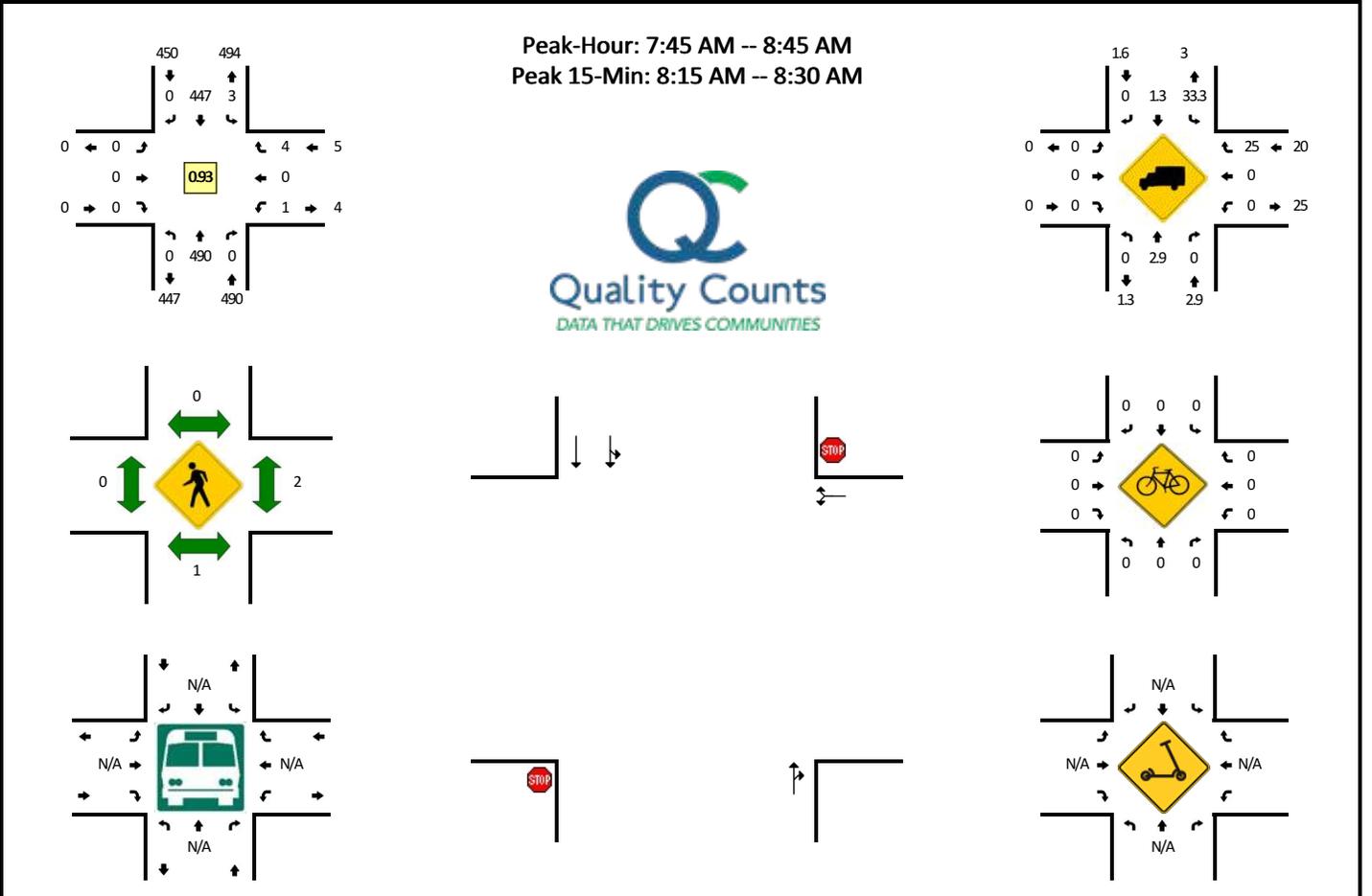


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Wellington Ct (Eastbound)				Wellington Ct (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	0	93	1	0	0	131	0	0	0	0	0	0	0	0	1	0	226	
4:15 PM	0	90	0	0	0	145	0	0	0	0	0	0	0	0	2	0	237	
4:30 PM	0	99	0	0	0	129	0	0	0	0	0	0	0	0	2	0	230	
4:45 PM	0	109	0	0	1	125	0	0	0	0	0	0	0	0	1	0	236	929
5:00 PM	0	104	0	0	1	159	0	0	0	0	0	0	0	0	0	0	264	967
5:15 PM	0	95	0	0	0	164	0	0	0	0	0	0	0	0	0	0	259	989
5:30 PM	0	114	0	0	2	136	0	0	0	0	0	0	0	0	0	0	252	1011
5:45 PM	0	99	0	0	0	122	0	0	0	0	0	0	0	0	1	0	222	997
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	416	0	0	4	636	0	0	0	0	0	0	0	0	0	0	1056	
Heavy Trucks	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	4	
Buses																		
Pedestrians		0				0					0			4			4	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																		

Comments:

LOCATION: Gross Point Rd -- Wellington Ct
CITY/STATE: Evanston, IL

QC JOB #: 15687807
DATE: Tue, Jan 25 2022

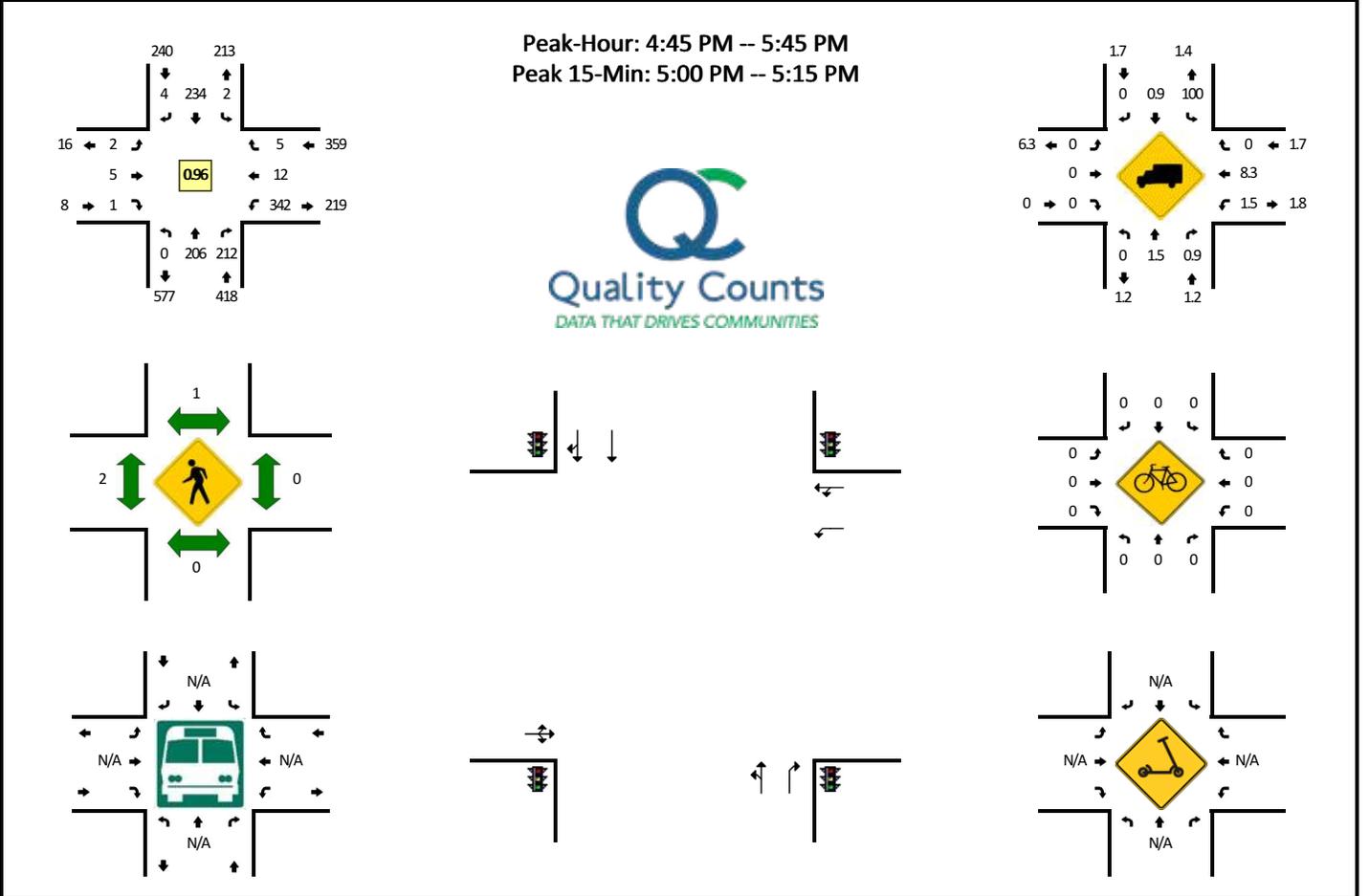


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Wellington Ct (Eastbound)				Wellington Ct (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	0	85	0	0	0	67	0	0	0	0	0	0	0	0	0	0	152	
7:15 AM	0	115	0	0	0	65	0	0	0	0	0	0	0	0	0	0	180	
7:30 AM	0	99	0	0	1	94	0	0	0	0	0	0	0	0	0	0	194	
7:45 AM	0	106	0	0	0	132	0	0	0	0	0	0	0	0	0	0	238	764
8:00 AM	0	122	0	0	0	92	0	0	0	0	0	0	0	0	1	0	215	827
8:15 AM	0	138	0	0	2	110	0	0	0	0	0	0	0	0	2	1	253	900
8:30 AM	0	124	0	0	1	113	0	0	0	0	0	0	0	0	1	0	239	945
8:45 AM	0	123	0	0	0	98	0	0	0	0	0	0	1	0	1	0	223	930
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	552	0	0	8	440	0	0	0	0	0	0	0	0	8	4	1012	
Heavy Trucks	0	8	0	0	4	4	0	0	0	0	0	0	0	0	0	0	16	
Buses																		
Pedestrians		0				0				0				8			8	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																		

Comments:

LOCATION: Gross Point Rd -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687806
DATE: Tue, Jan 25 2022



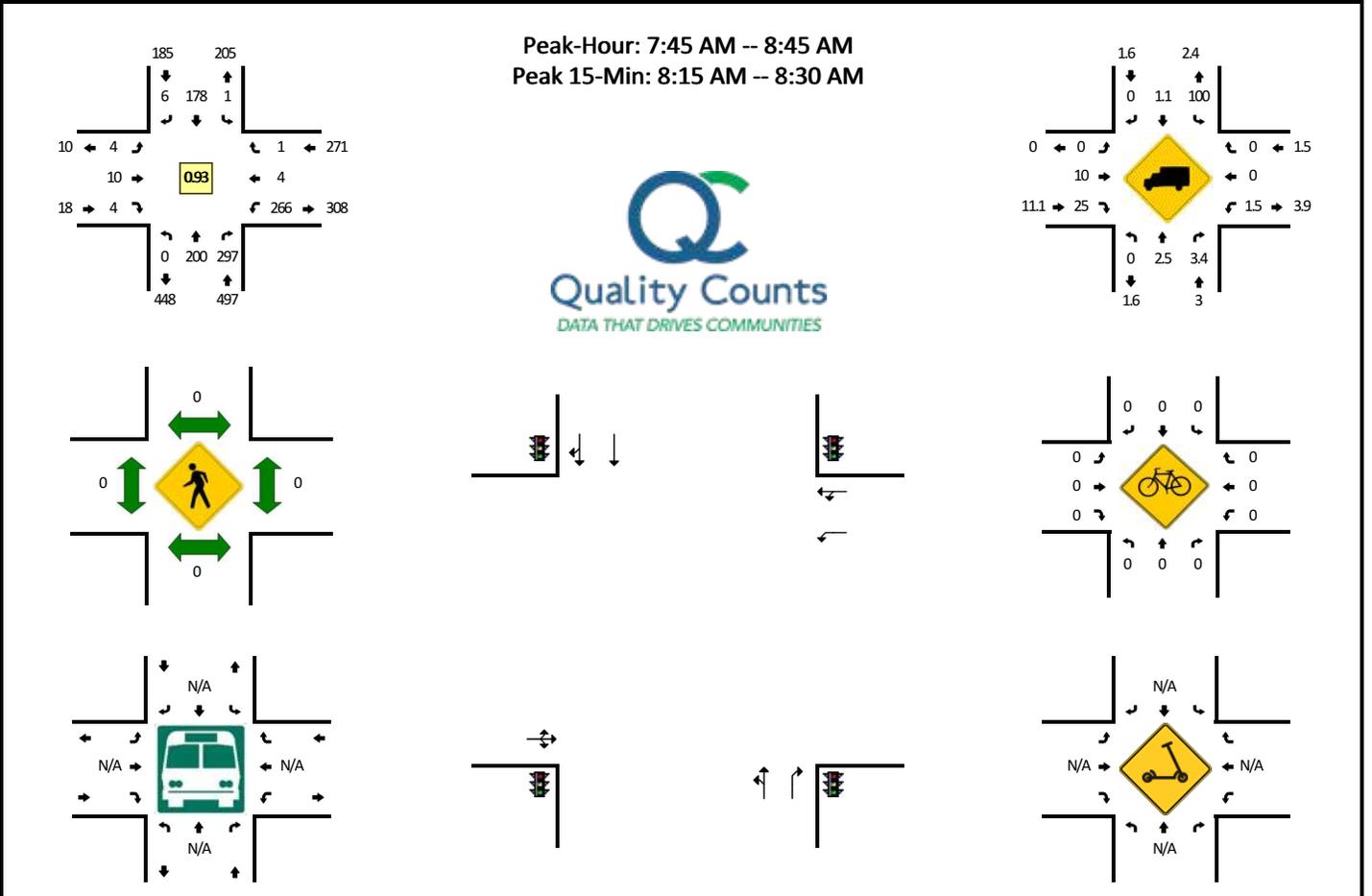
15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	1	44	52	0	1	43	0	0	0	1	1	0	86	1	1	0	231	
4:15 PM	1	29	62	0	0	53	2	0	2	1	0	0	89	2	2	0	243	
4:30 PM	0	43	59	0	0	45	0	0	0	3	1	0	84	2	1	0	238	
4:45 PM	0	54	56	0	1	49	1	0	1	0	0	0	76	7	3	0	248	960
5:00 PM	0	61	44	0	1	63	1	0	0	1	0	0	93	2	0	0	266	995
5:15 PM	0	41	51	0	0	71	0	0	1	1	0	0	88	1	1	0	255	1007
5:30 PM	0	50	61	0	0	51	2	0	0	3	1	0	85	2	1	0	256	1025
5:45 PM	0	56	49	0	0	44	2	0	0	2	0	0	79	0	0	0	232	1009

Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	
All Vehicles	0	244	176	0	4	252	4	0	0	4	0	0	372	8	0	0	1064
Heavy Trucks	0	0	0		4	0	0		0	0	0		4	4	0		12
Buses																	
Pedestrians		0				0				8				0			8
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0
Scoters																	

Comments:

LOCATION: Gross Point Rd -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687805
DATE: Tue, Jan 25 2022

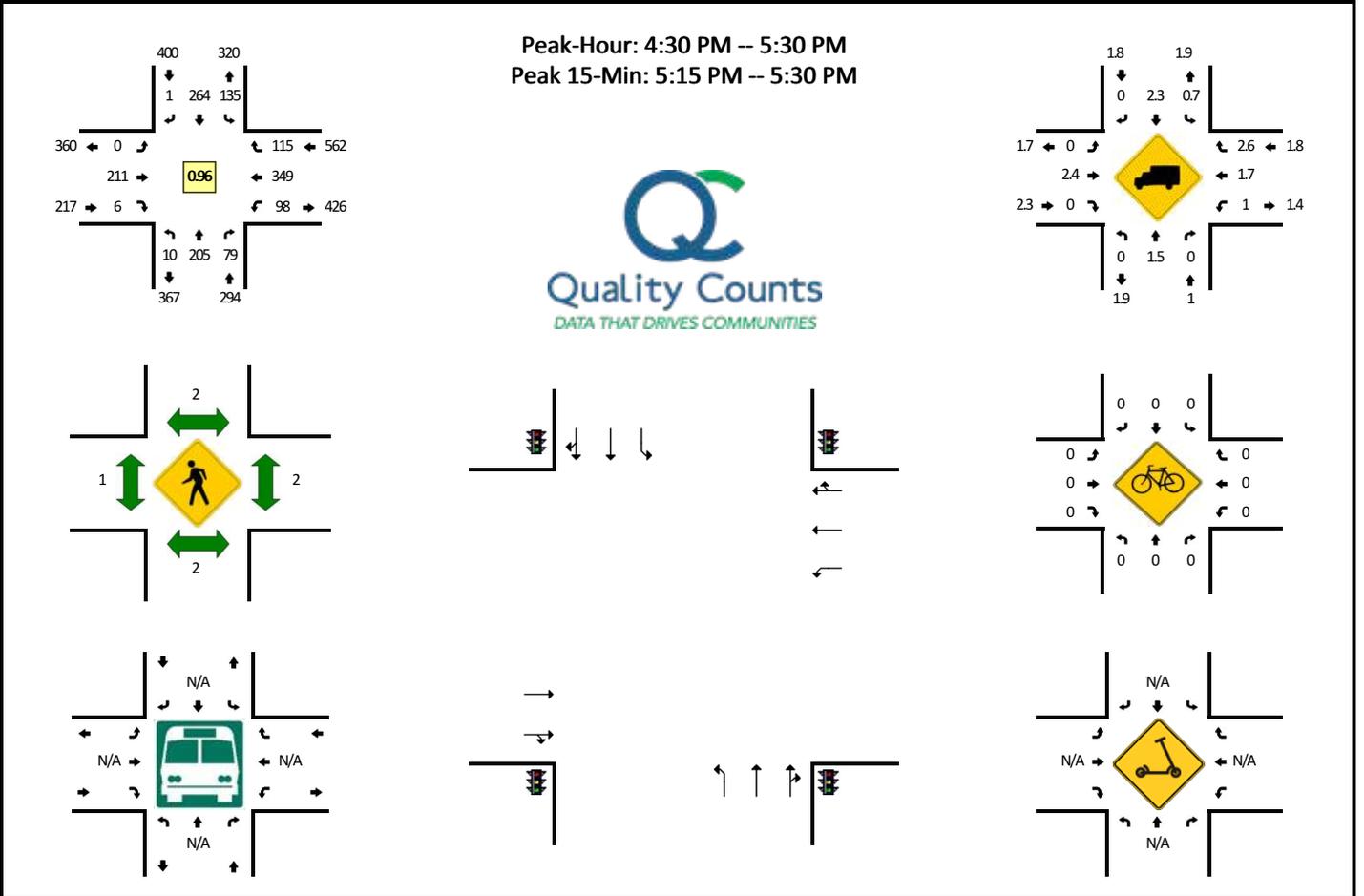


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	2	29	55	0	0	20	1	0	0	1	0	0	46	0	0	0	154	
7:15 AM	0	50	63	0	0	19	0	0	0	0	1	0	45	0	0	0	178	
7:30 AM	0	42	57	0	1	31	0	0	0	3	2	0	61	1	0	0	198	
7:45 AM	0	47	61	0	1	46	2	0	1	3	1	0	84	1	0	0	247	777
8:00 AM	0	46	75	0	0	33	1	0	1	3	0	0	58	1	1	0	219	842
8:15 AM	0	59	83	0	0	46	3	0	1	3	0	0	66	1	0	0	262	926
8:30 AM	0	48	78	0	0	53	0	0	1	1	3	0	58	1	0	0	243	971
8:45 AM	0	59	66	0	0	39	2	0	1	3	1	0	58	3	0	0	232	956
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	0	236	332	0	0	184	12	0	4	12	0	0	264	4	0	0	1048	
Heavy Trucks	0	0	8		0	8	0		0	0	0		0	0	0		16	
Buses																	0	
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																	0	

Comments:

LOCATION: N Crawford Ave -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687804
DATE: Tue, Jan 25 2022

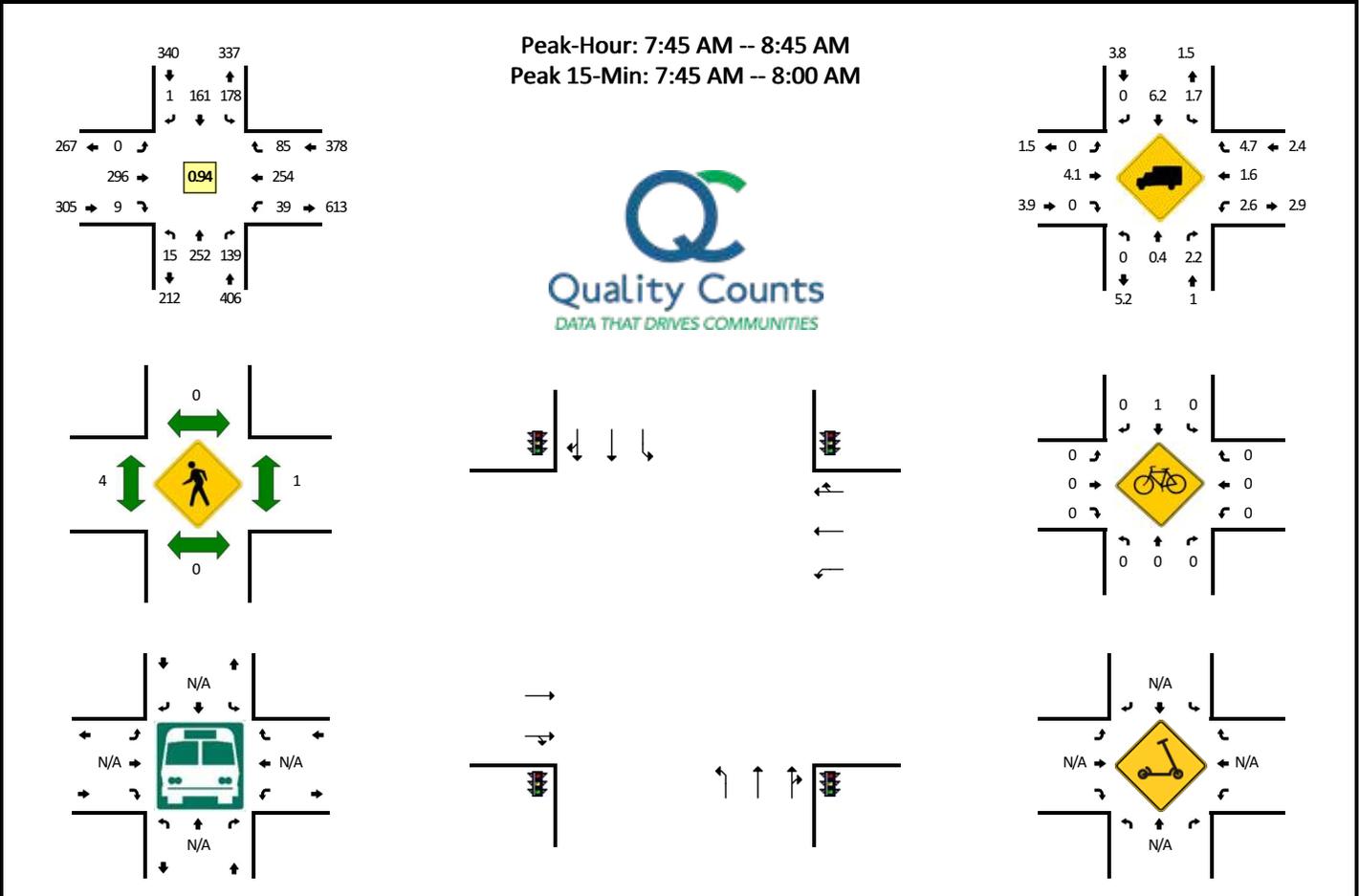


15-Min Count Period Beginning At	N Crawford Ave (Northbound)				N Crawford Ave (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	5	49	27	1	31	75	0	0	0	48	5	0	31	78	34	0	384	
4:15 PM	4	45	23	0	33	62	0	0	0	61	2	0	17	87	18	0	352	
4:30 PM	3	51	30	0	28	62	0	0	0	60	1	0	23	83	33	1	375	
4:45 PM	4	54	16	0	39	65	1	0	0	51	2	0	24	81	19	0	356	1467
5:00 PM	1	46	11	0	21	67	0	0	0	47	3	0	26	99	36	0	357	1440
5:15 PM	2	54	22	0	47	70	0	0	0	53	0	0	24	86	27	0	385	1473
5:30 PM	4	54	19	1	30	51	0	0	0	63	0	0	18	78	31	0	349	1447
5:45 PM	1	67	23	0	28	60	0	0	0	52	2	0	19	78	23	0	353	1444
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	8	216	88	0	188	280	0	0	0	212	0	0	96	344	108	0	1540	
Heavy Trucks	0	4	0		0	4	0		0	0	0		4	4	0		16	
Buses																	0	
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																	0	

Comments:

LOCATION: N Crawford Ave -- Central St
CITY/STATE: Evanston, IL

QC JOB #: 15687803
DATE: Tue, Jan 25 2022

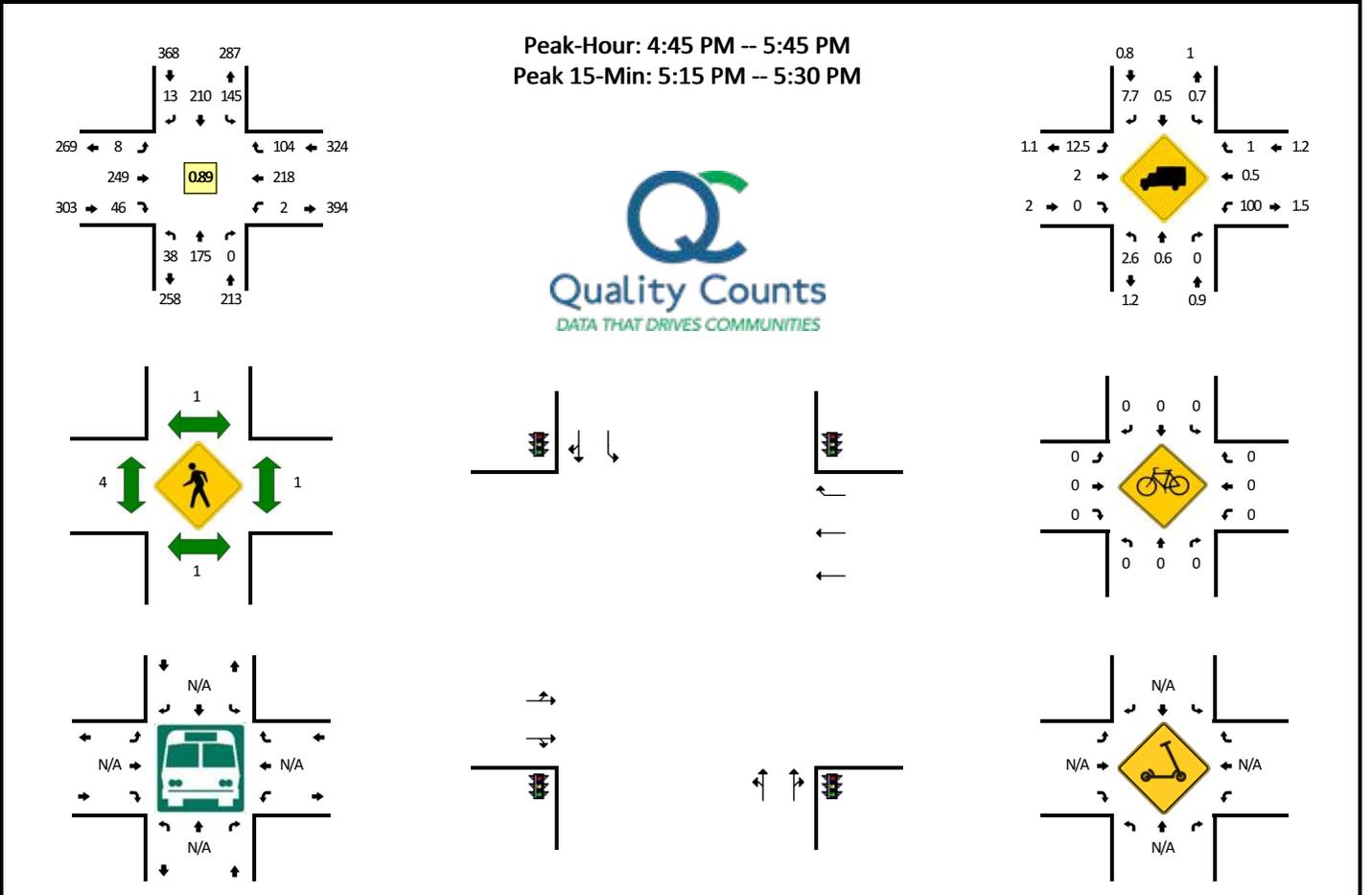


15-Min Count Period Beginning At	N Crawford Ave (Northbound)				N Crawford Ave (Southbound)				Central St (Eastbound)				Central St (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
7:00 AM	0	37	10	0	14	12	0	0	0	52	1	0	11	46	17	0	200	
7:15 AM	0	64	16	0	33	29	0	0	0	64	0	0	6	47	25	0	284	
7:30 AM	3	77	27	0	33	40	0	0	0	62	1	0	12	61	27	0	343	
7:45 AM	3	84	29	1	48	38	0	0	0	65	0	0	10	78	24	0	380	1207
8:00 AM	3	55	34	0	52	42	0	0	0	72	4	0	10	57	17	0	346	1353
8:15 AM	4	60	37	1	40	31	0	0	0	81	3	0	11	62	22	0	352	1421
8:30 AM	2	53	39	1	38	50	1	0	0	78	2	0	8	57	22	0	351	1429
8:45 AM	4	56	39	0	43	47	0	0	0	71	1	0	8	61	40	0	370	1419
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	12	336	116	4	192	152	0	0	0	260	0	0	40	312	96	0	1520	
Heavy Trucks	0	4	4		4	4	0		0	16	0		4	0	8		44	
Buses																		
Pedestrians		0				0				4				0			4	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scoters																		

Comments:

LOCATION: Gross Point Rd -- N Crawford Ave
CITY/STATE: Evanston, IL

QC JOB #: 15687802
DATE: Tue, Jan 25 2022

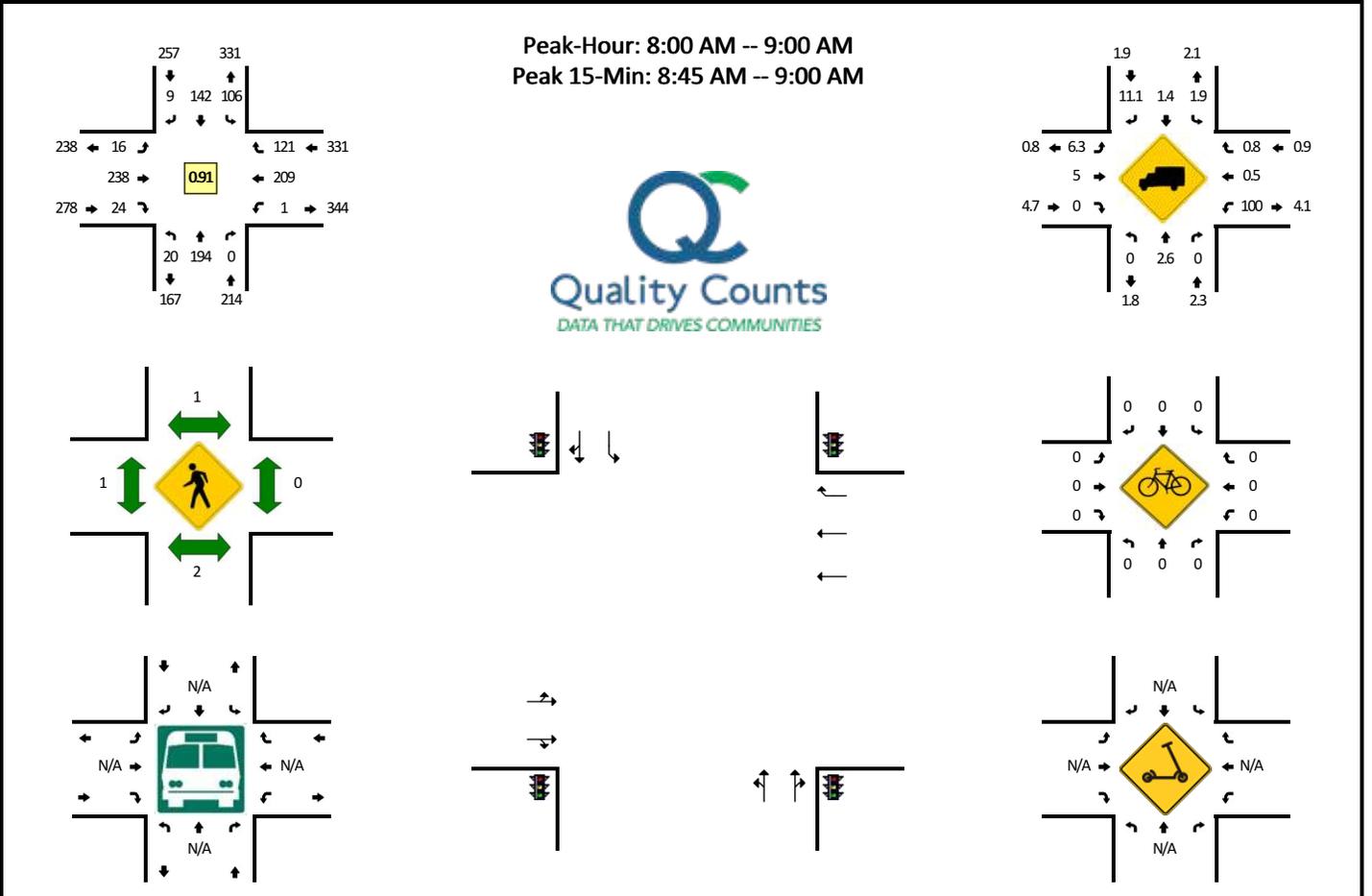


15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				N Crawford Ave (Eastbound)				N Crawford Ave (Westbound)				Total	Hourly Totals
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
4:00 PM	10	39	0	0	37	42	1	0	2	68	5	0	1	67	19	0	291	
4:15 PM	8	24	0	0	32	45	4	0	1	60	10	0	0	43	18	0	245	
4:30 PM	3	42	0	0	30	39	2	0	3	59	4	0	0	56	34	0	272	
4:45 PM	11	42	0	0	39	35	4	0	2	64	16	0	2	49	23	0	287	1095
5:00 PM	8	52	0	0	32	58	4	0	2	57	9	0	0	56	24	0	302	1106
5:15 PM	10	39	0	0	51	67	1	0	2	72	13	0	0	62	24	0	341	1202
5:30 PM	9	42	0	0	23	50	4	0	2	56	8	0	0	51	33	0	278	1208
5:45 PM	10	47	0	0	25	30	1	0	1	58	4	0	0	57	27	0	260	1181
Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	40	156	0	0	204	268	4	0	8	288	52	0	0	248	96	0	1364	
Heavy Trucks	0	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	4	
Buses																		
Pedestrians		0				0				0				0			0	
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0		0	
Scooters																		

Comments:

LOCATION: Gross Point Rd -- N Crawford Ave
CITY/STATE: Evanston, IL

QC JOB #: 15687801
DATE: Tue, Jan 25 2022



15-Min Count Period Beginning At	Gross Point Rd (Northbound)				Gross Point Rd (Southbound)				N Crawford Ave (Eastbound)				N Crawford Ave (Westbound)				Total	Hourly Totals	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U			
7:00 AM	3	24	0	0	8	16	1	0	0	18	5	0	0	33	19	0	0	127	
7:15 AM	0	52	0	0	14	15	2	0	1	51	5	0	0	61	26	0	0	227	
7:30 AM	2	39	1	0	16	32	1	0	1	56	3	0	1	67	37	0	0	256	
7:45 AM	3	44	0	0	17	44	3	0	0	72	9	0	1	65	36	0	0	294	904
8:00 AM	8	40	0	0	25	24	1	0	2	65	3	0	0	52	22	0	0	242	1019
8:15 AM	5	49	0	0	19	40	3	0	3	54	5	0	1	58	28	0	0	265	1057
8:30 AM	4	47	0	0	32	43	4	0	3	57	10	0	0	45	30	0	0	275	1076
8:45 AM	3	58	0	0	30	35	1	0	8	62	6	0	0	54	41	0	0	298	1080

Peak 15-Min Flowrates	Northbound				Southbound				Eastbound				Westbound				Total	
	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U	Left	Thru	Right	U		
All Vehicles	12	232	0	0	120	140	4	0	32	248	24	0	0	216	164	0	0	1192
Heavy Trucks	0	4	0	0	0	4	0	0	4	12	0	0	0	0	4	0	0	28
Buses																		0
Pedestrians		0				0				0				0				0
Bicycles	0	0	0		0	0	0		0	0	0		0	0	0			0
Scooters																		0

Comments:

Evanston, IL Weather: Cold and Dry
 Central St and Unity Church Access
 Thursday January 27, 2022

01/28/22
 10:17:18

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Counts: All Vehicles - by Mvmt

Intersection # 3 central/unitychurch

Begin Time	N-Approach			E-Approach			S-Approach			W-Approach			Int Total
	RT	TH	LT										
700	0	0	0	0	0	0	0	0	0	0	4	0	4
715	0	0	0	0	2	0	0	0	0	0	1	0	3
730	0	0	0	0	0	0	0	0	0	0	7	0	7
745	0	0	0	0	5	1	0	0	0	0	4	0	10
800	0	0	0	0	4	0	0	0	0	0	4	0	8
815	0	0	0	0	2	0	1	0	0	1	1	0	5
830	0	0	0	0	6	0	0	0	0	0	8	0	14
845	0	0	0	0	4	0	0	0	0	0	4	0	8
1600	0	0	0	0	3	2	0	0	0	0	2	0	7
1615	0	0	0	0	2	0	1	0	1	0	4	0	8
1630	0	0	0	0	4	0	0	0	0	0	4	0	8
1645	0	0	0	0	2	0	1	0	0	0	3	0	6
1700	0	0	0	0	2	0	0	0	0	0	3	0	5
1715	0	0	0	0	4	1	0	0	0	0	2	0	7
1730	0	0	0	0	5	0	0	0	0	0	1	0	6
1745	0	0	0	0	4	0	0	0	0	0	2	0	6
Total	0	0	0	0	49	4	3	0	1	1	54	0	112

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Counts: All Vehicles - Totals

Intersection # 3 central/unitychurch

Begin Time	Approach Totals				Exit Totals				Int Total
	N	E	S	W	N	E	S	W	
700	0	0	0	4	0	4	0	0	4
715	0	2	0	1	0	1	0	2	3
730	0	0	0	7	0	7	0	0	7
745	0	6	0	4	0	4	1	5	10
800	0	4	0	4	0	4	0	4	8
815	0	2	1	2	0	2	1	2	5
830	0	6	0	8	0	8	0	6	14
845	0	4	0	4	0	4	0	4	8
1600	0	5	0	2	0	2	2	3	7
1615	0	2	2	4	0	5	0	3	8
1630	0	4	0	4	0	4	0	4	8
1645	0	2	1	3	0	4	0	2	6
1700	0	2	0	3	0	3	0	2	5
1715	0	5	0	2	0	2	1	4	7
1730	0	5	0	1	0	1	0	5	6
1745	0	4	0	2	0	2	0	4	6
Total	0	53	4	55	0	57	5	50	112

Evanston, IL Weather: Cold and Dry
 Central St and Unity Church Access
 Thursday January 27, 2022

01/28/22
 10:17:18

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Flow Rates: by Movement

Intersection # 3 central/unitychurch

Begin Time	N-Approach			E-Approach			S-Approach			W-Approach			Int Total
	RT	TH	LT										
700	0	0	0	0	0	0	0	0	0	0	16	0	16
715	0	0	0	0	8	0	0	0	0	0	4	0	12
730	0	0	0	0	0	0	0	0	0	0	28	0	28
745	0	0	0	0	20	4	0	0	0	0	16	0	40
800	0	0	0	0	16	0	0	0	0	0	16	0	32
815	0	0	0	0	8	0	4	0	0	4	4	0	20
830	0	0	0	0	24	0	0	0	0	0	32	0	56
845	0	0	0	0	16	0	0	0	0	0	16	0	32
1600	0	0	0	0	12	8	0	0	0	0	8	0	28
1615	0	0	0	0	8	0	4	0	4	0	16	0	32
1630	0	0	0	0	16	0	0	0	0	0	16	0	32
1645	0	0	0	0	8	0	4	0	0	0	12	0	24
1700	0	0	0	0	8	0	0	0	0	0	12	0	20
1715	0	0	0	0	16	4	0	0	0	0	8	0	28
1730	0	0	0	0	20	0	0	0	0	0	4	0	24
1745	0	0	0	0	16	0	0	0	0	0	8	0	24

URNS/TEAPAC[Ver 3.61.12] - 15-Minute Flow Rates: Appr/Exit Totals

Intersection # 3 central/unitychurch

Begin Time	Approach Totals				Exit Totals				Int Total
	N	E	S	W	N	E	S	W	
700	0	0	0	16	0	16	0	0	16
715	0	8	0	4	0	4	0	8	12
730	0	0	0	28	0	28	0	0	28
745	0	24	0	16	0	16	4	20	40
800	0	16	0	16	0	16	0	16	32
815	0	8	4	8	0	8	4	8	20
830	0	24	0	32	0	32	0	24	56
845	0	16	0	16	0	16	0	16	32
1600	0	20	0	8	0	8	8	12	28
1615	0	8	8	16	0	20	0	12	32
1630	0	16	0	16	0	16	0	16	32
1645	0	8	4	12	0	16	0	8	24
1700	0	8	0	12	0	12	0	8	20
1715	0	20	0	8	0	8	4	16	28
1730	0	20	0	4	0	4	0	20	24
1745	0	16	0	8	0	8	0	16	24

Evanston, IL Weather: Cold and Dry
 Central St and Unity Church Access
 Thursday January 27, 2022

01/28/22
 10:17:18

URNS/TEAPAC[Ver 3.61.12] - 60-Minute Volumes: by Movement

Intersection # 3 central/unitychurch

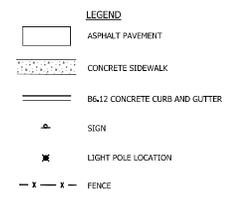
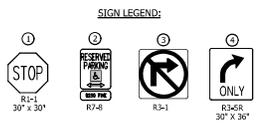
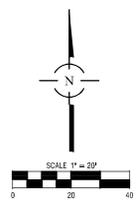
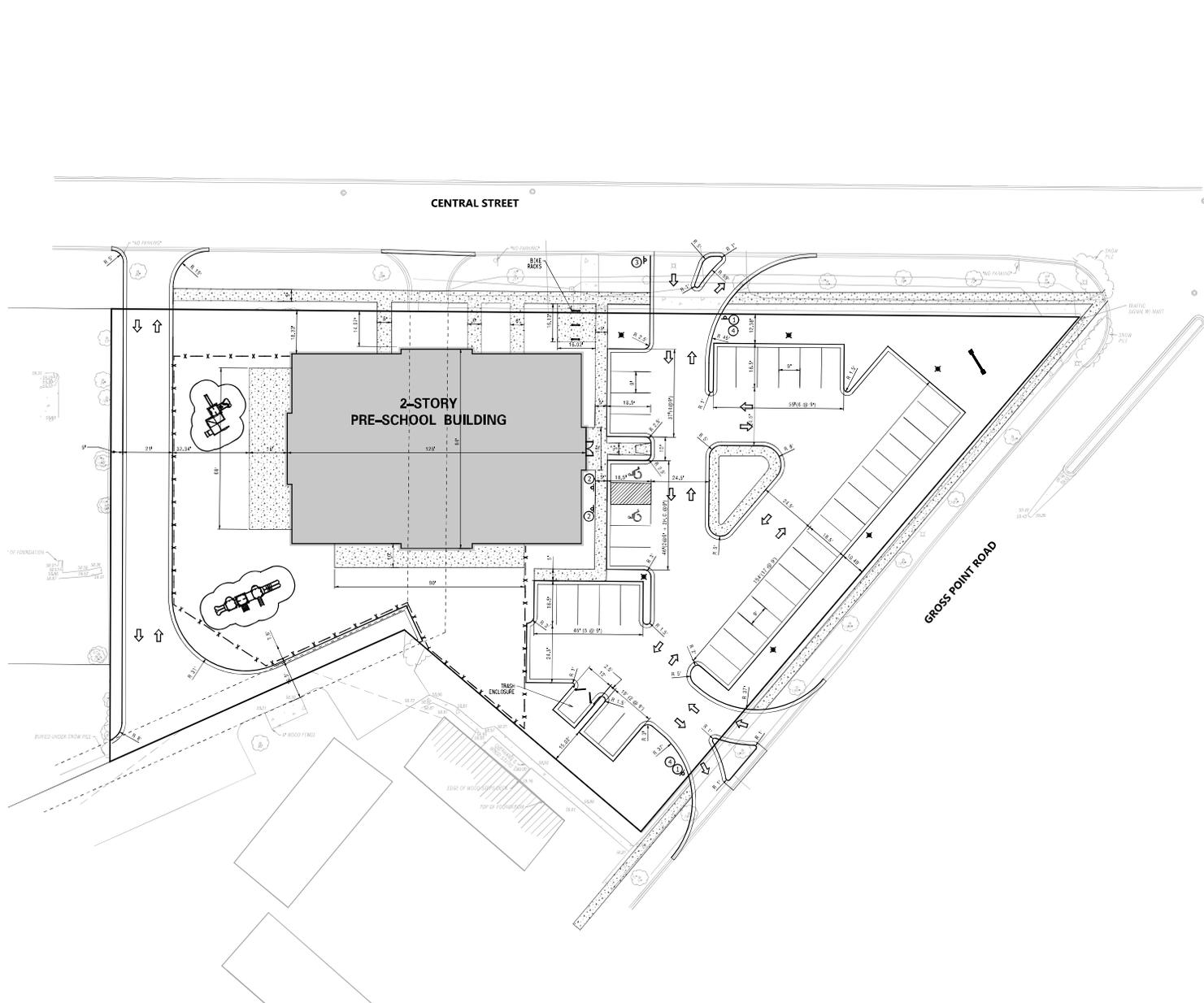
Begin Time	N-Approach			E-Approach			S-Approach			W-Approach			Int Total
	RT	TH	LT										
700	0	0	0	0	7	1	0	0	0	0	16	0	24
715	0	0	0	0	11	1	0	0	0	0	16	0	28
730	0	0	0	0	11	1	1	0	0	1	16	0	30
745	0	0	0	0	17	1	1	0	0	1	17	0	37
800	0	0	0	0	16	0	1	0	0	1	17	0	35
815	0	0	0	0	12	0	1	0	0	1	13	0	27*
830	0	0	0	0	10	0	0	0	0	0	12	0	22*
845	0	0	0	0	4	0	0	0	0	0	4	0	8*
1600	0	0	0	0	11	2	2	0	1	0	13	0	29
1615	0	0	0	0	10	0	2	0	1	0	14	0	27
1630	0	0	0	0	12	1	1	0	0	0	12	0	26
1645	0	0	0	0	13	1	1	0	0	0	9	0	24
1700	0	0	0	0	15	1	0	0	0	0	8	0	24
1715	0	0	0	0	13	1	0	0	0	0	5	0	19*
1730	0	0	0	0	9	0	0	0	0	0	3	0	12*
1745	0	0	0	0	4	0	0	0	0	0	2	0	6*

URNS/TEAPAC[Ver 3.61.12] - 60-Minute Volumes: Appr/Exit Totals

Intersection # 3 central/unitychurch

Begin Time	Approach Totals				Exit Totals				Int Total
	N	E	S	W	N	E	S	W	
700	0	8	0	16	0	16	1	7	24
715	0	12	0	16	0	16	1	11	28
730	0	12	1	17	0	17	2	11	30
745	0	18	1	18	0	18	2	17	37
800	0	16	1	18	0	18	1	16	35
815	0	12	1	14	0	14	1	12	27*
830	0	10	0	12	0	12	0	10	22*
845	0	4	0	4	0	4	0	4	8*
1600	0	13	3	13	0	15	2	12	29
1615	0	10	3	14	0	16	0	11	27
1630	0	13	1	12	0	13	1	12	26
1645	0	14	1	9	0	10	1	13	24
1700	0	16	0	8	0	8	1	15	24
1715	0	14	0	5	0	5	1	13	19*
1730	0	9	0	3	0	3	0	9	12*
1745	0	4	0	2	0	2	0	4	6*

Site Plan



PARKING SUMMARY

STANDARD:	36
ACCESSIBLE:	2
TOTAL:	38

NO.	DATE	REMARKS

PRELIMINARY GEOMETRIC PLAN
KESINGTON SCHOOL
 3494 CENTRAL STREET
 EVANSTON, ILLINOIS



FILENAME:	11641P-GM
DATE:	02/27/23
JOB NO.:	11641
SHEET	P-GM
	3 OF 5

\\spaceco\11641\PROJECTS\11641\GIS\11641\11641P-GM.dwg Plot: 11641P-GM.dwg

CMAP Projections Letter



Chicago Metropolitan Agency for Planning

433 West Van Buren Street
Suite 450
Chicago, IL 60607

312-454-0400
cmap.illinois.gov

January 13, 2022

Kelly Pachowicz
Consultant
Kenig, Lindgren, O'Hara and Aboona, Inc.
9575 West Higgins Road
Suite 400
Rosemont, IL 60018

**Subject: Gross Point Road - Central Street - Crawford Avenue
IDOT**

Dear Ms. Pachowicz:

In response to a request made on your behalf and dated January 12, 2022, we have developed year 2050 average daily traffic (ADT) projections for the subject location.

ROAD SEGMENT	Current ADT	Year 2050 ADT
Gross Point Rd fr Golf Rd to Lake Ave	11,100	12,400
Central St east of Crawford Ave	12,500	13,900
Crawford Ave south of Gross Point Rd	9,350	11,100
Crawford Ave south of Gross Point Rd	6,000	7,100

Traffic projections are developed using existing ADT data provided in the request letter and the results from the June 2021 CMAP Travel Demand Analysis. The regional travel model uses CMAP 2050 socioeconomic projections and assumes the implementation of the ON TO 2050 Comprehensive Regional Plan for the Northeastern Illinois area. The provision of this data in support of your request does not constitute a CMAP endorsement of the proposed development or any subsequent developments.

If you have any questions, please call me at (312) 386-8806.

Sincerely,

Jose Rodriguez, PTP, AICP
Senior Planner, Research & Analysis

cc: Rios (IDOT)
2022_ForecastTraffic\Evanston\ck-11-22\ck-11-22.docx

Level of Service Table

LEVEL OF SERVICE CRITERIA

Signalized Intersections		
Level of Service	Interpretation	Average Control Delay (seconds per vehicle)
A	Favorable progression. Most vehicles arrive during the green indication and travel through the intersection without stopping.	≤10
B	Good progression, with more vehicles stopping than for Level of Service A.	>10 - 20
C	Individual cycle failures (i.e., one or more queued vehicles are not able to depart as a result of insufficient capacity during the cycle) may begin to appear. Number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping.	>20 - 35
D	The volume-to-capacity ratio is high and either progression is ineffective or the cycle length is too long. Many vehicles stop and individual cycle failures are noticeable.	>35 - 55
E	Progression is unfavorable. The volume-to-capacity ratio is high and the cycle length is long. Individual cycle failures are frequent.	>55 - 80
F	The volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue.	>80.0
Unsignalized Intersections		
Level of Service	Average Total Delay (SEC/VEH)	
A	0 - 10	
B	> 10 - 15	
C	> 15 - 25	
D	> 25 - 35	
E	> 35 - 50	
F	> 50	

Source: *Highway Capacity Manual*, 2010.

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – 2022 Base Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

04/17/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕		↕	↕			↕	↕		↕	↕
Traffic Volume (vph)	5	12	5	319	5	0	0	240	356	0	214	7
Future Volume (vph)	5	12	5	319	5	0	0	240	356	0	214	7
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.971							0.850		0.995	
Flt Protected		0.989		0.950	0.954							
Satd. Flow (prot)	0	1643	0	1681	1689	0	0	1863	1568	0	3558	0
Flt Permitted		0.989		0.950	0.954							
Satd. Flow (perm)	0	1643	0	1681	1689	0	0	1863	1568	0	3558	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5							383			4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	10%	25%	2%	0%	0%	0%	2%	3%	0%	1%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)				49%								
Lane Group Flow (vph)	0	23	0	175	173	0	0	258	383	0	238	0
Turn Type	Split	NA		Split	NA			NA	Perm		NA	
Protected Phases	4	4		8	8			2				6
Permitted Phases							2		2			
Detector Phase	4	4		8	8		2	2	2			6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0			5.0
Minimum Split (s)	20.0	20.0		24.0	24.0		24.0	24.0	24.0			24.0
Total Split (s)	20.0	20.0		36.0	36.0		54.0	54.0	54.0			54.0
Total Split (%)	18.2%	18.2%		32.7%	32.7%		49.1%	49.1%	49.1%			49.1%
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0			4.0
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0			2.0
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0			0.0
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0			6.0
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min		Min	
Act Effct Green (s)		6.8		69.9	69.9			22.5	22.5		22.5	
Actuated g/C Ratio		0.06		0.64	0.64			0.20	0.20		0.20	

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/17/2025

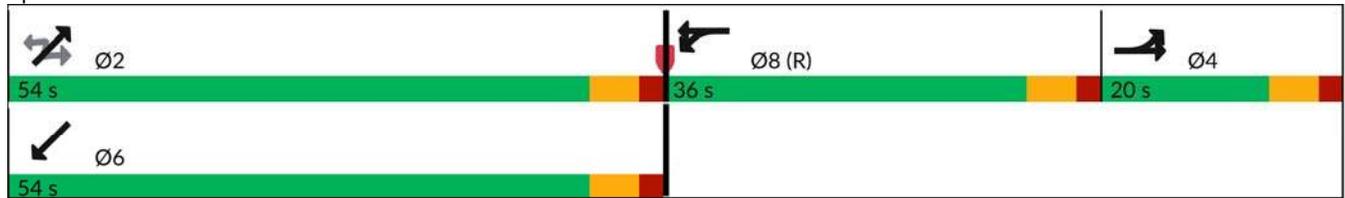


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.22		0.16	0.16			0.68	0.61			0.33
Control Delay (s/veh)		45.4		7.2	7.2			48.7	7.9			15.3
Queue Delay		0.0		1.2	1.2			0.0	0.1			0.0
Total Delay (s/veh)		45.4		8.5	8.4			48.7	8.0			15.3
LOS		D		A	A			D	A			B
Approach Delay (s/veh)		45.4			8.4			24.4				15.3
Approach LOS		D			A			C				B
Queue Length 50th (ft)		12		24	23			169	0			21
Queue Length 95th (ft)		38		56	56			231	71			101
Internal Link Dist (ft)		283			141			230				175
Turn Bay Length (ft)									95			
Base Capacity (vph)		213		1067	1072			812	900			1554
Starvation Cap Reductn		0		699	704			0	0			0
Spillback Cap Reductn		0		0	0			0	83			0
Storage Cap Reductn		0		0	0			0	0			0
Reduced v/c Ratio		0.11		0.48	0.47			0.32	0.47			0.15

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay (s/veh):	18.6
Intersection LOS:	B
Intersection Capacity Utilization:	38.3%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/17/2025

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	10	290	32	0	263	139	24	222	0	112	188	13
Future Volume (vph)	10	290	32	0	263	139	24	222	0	112	188	13
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Ped Bike Factor												
Frt		0.986				0.850						0.990
Flt Protected		0.998						0.995		0.950		
Satd. Flow (prot)	0	3228	0	0	3574	1599	0	3497	0	1770	1851	0
Flt Permitted		0.944						0.894		0.403		
Satd. Flow (perm)	0	3053	0	0	3574	1599	0	3142	0	751	1851	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		13				153						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	6%	5%	0%	2%	1%	1%	0%	3%	0%	2%	1%	11%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)	0	0	0									
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	365	0	0	289	153	0	270	0	123	221	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		69.0			69.0	69.0		14.7		31.5	29.0	
Actuated g/C Ratio		0.63			0.63	0.63		0.13		0.29	0.26	

Lanes, Volumes, Timings
 6: Gross Point Road & Crawford Avenue

04/17/2025

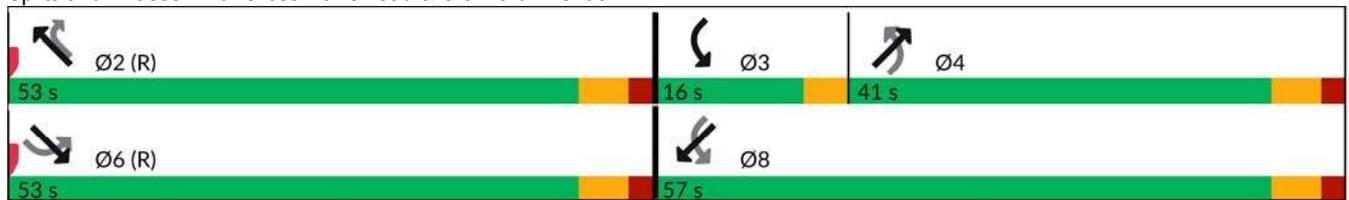


Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.19			0.13	0.14		0.64		0.39	0.45	
Control Delay (s/veh)		9.3			4.1	0.6		31.6		32.5	35.3	
Queue Delay		0.0			0.3	0.6		0.0		0.0	0.0	
Total Delay (s/veh)		9.3			4.5	1.1		31.6		32.5	35.3	
LOS		A			A	A		C		C	D	
Approach Delay (s/veh)		9.3			3.3			31.6			34.3	
Approach LOS		A			A			C			C	
Queue Length 50th (ft)		52			11	0		20		66	127	
Queue Length 95th (ft)		84			22	0		79		107	187	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1918			2240	1059		999		331	860	
Starvation Cap Reductn		0			1441	617		0		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.19			0.36	0.35		0.27		0.37	0.26	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.64
Intersection Signal Delay (s/veh):	17.7
Intersection LOS:	B
Intersection Capacity Utilization:	48.9%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue



Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

04/17/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑		↖	↑↑		↖	↑↑		↖	↑↑	
Traffic Volume (vph)	0	358	11	47	306	102	18	302	167	214	195	1
Future Volume (vph)	0	358	11	47	306	102	18	302	167	214	195	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.995			0.962			0.946				0.999
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3458	0	1752	3380	0	1805	3369	0	1770	3403	0
Flt Permitted				0.290			0.621			0.414		
Satd. Flow (perm)	0	3458	0	535	3380	0	1180	3369	0	771	3403	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		3			50			89				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	4%	0%	3%	2%	5%	0%	1%	2%	2%	6%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)						0						
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	393	0	50	435	0	19	499	0	228	208	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	31.0		29.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	28.2%		26.4%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		17.9		29.8	27.3		64.2	55.7		73.2	66.7	
Actuated g/C Ratio		0.16		0.27	0.25		0.58	0.51		0.67	0.61	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

04/17/2025

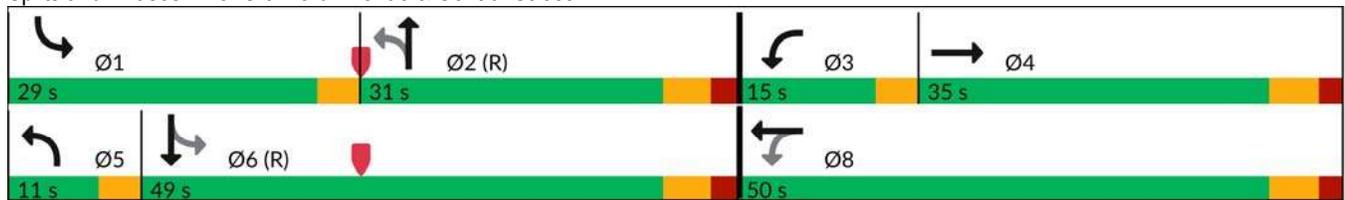


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
v/c Ratio		0.70		0.22	0.50		0.03	0.28		0.37	0.10	
Control Delay (s/veh)		52.9		28.8	31.9		9.2	15.1		9.7	9.5	
Queue Delay		0.7		0.0	0.0		0.0	0.0		0.4	0.4	
Total Delay (s/veh)		53.7		28.8	31.9		9.2	15.1		10.0	9.9	
LOS		D		C	C		A	B		B	A	
Approach Delay (s/veh)		53.7			31.6			14.9			10.0	
Approach LOS		D			C			B			A	
Queue Length 50th (ft)		139		26	120		4	85		66	28	
Queue Length 95th (ft)		183		50	152		16	152		100	53	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		913		272	1382		747	1751		744	2062	
Starvation Cap Reductn		251		0	0		0	0		189	1415	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.59		0.18	0.31		0.03	0.28		0.41	0.32	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay (s/veh):	26.5
Intersection LOS:	C
Intersection Capacity Utilization:	56.6%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 7th TWSC
 10: Gross Point Road & Wellington Court

04/17/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	1	5	591	0	4	534
Future Vol, veh/h	1	5	591	0	4	534
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	5	642	0	4	580

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	941	642	0	0	642	0
Stage 1	642	-	-	-	-	-
Stage 2	299	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	276	473	-	-	940	-
Stage 1	523	-	-	-	-	-
Stage 2	727	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	275	473	-	-	940	-
Mov Cap-2 Maneuver	275	-	-	-	-	-
Stage 1	523	-	-	-	-	-
Stage 2	723	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s/v	13.66	0	0.11
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	422	27	-
HCM Lane V/C Ratio	-	-	0.015	0.005	-
HCM Control Delay (s/veh)	-	-	13.7	8.8	0
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

HCM 7th TWSC
 12: East Site Access & Central Street

04/17/2025

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↔		↗
Traffic Vol, veh/h	21	1	1	11	0	1
Future Vol, veh/h	21	1	1	11	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	23	1	1	12	0	1

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	24	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	4.12	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	2.218	-	-
Pot Cap-1 Maneuver	-	-	1591	-	0
Stage 1	-	-	-	-	0
Stage 2	-	-	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1591	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s/v	0	0.61	8.42
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1053	-	-	150	-
HCM Lane V/C Ratio	0.001	-	-	0.001	-
HCM Control Delay (s/veh)	8.4	-	-	7.3	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 7th TWSC
 14: Gross Point Road & Access Drive

04/17/2025

Intersection						
Int Delay, s/veh	0					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations		↗		↑	↗↘	
Traffic Vol, veh/h	0	0	0	0	0	0
Future Vol, veh/h	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	0	0	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	1	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.93	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.319	-	-	-	-
Pot Cap-1 Maneuver	0	1084	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	1084	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s/v	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NET SELn1	SWT	SWR
Capacity (veh/h)	-	-	-
HCM Lane V/C Ratio	-	-	-
HCM Control Delay (s/veh)	-	0	-
HCM Lane LOS	-	A	-
HCM 95th %tile Q(veh)	-	-	-

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – 2022 Base Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

04/17/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕		↕	↕			↕	↕		↕	
Traffic Volume (vph)	2	6	1	410	14	6	0	247	254	2	302	5
Future Volume (vph)	2	6	1	410	14	6	0	247	254	2	302	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	0.95	0.95	0.95
Ped Bike Factor												
Frt		0.985			0.996				0.850		0.998	
Flt Protected		0.989		0.950	0.957							
Satd. Flow (prot)	0	1851	0	1681	1681	0	0	1863	1599	0	3546	0
Flt Permitted		0.989		0.950	0.957						0.953	
Satd. Flow (perm)	0	1851	0	1681	1681	0	0	1863	1599	0	3379	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		1			1				265			2
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	0%	0%	2%	8%	0%	0%	2%	1%	100%	1%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)				48%								
Lane Group Flow (vph)	0	9	0	222	226	0	0	257	265	0	322	0
Turn Type	Split	NA		Split	NA			NA	Perm	Perm	NA	
Protected Phases	4	4		8	8			2				6
Permitted Phases							2		2		6	
Detector Phase	4	4		8	8		2	2	2		6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	18.0	18.0		24.0	24.0		24.0	24.0	24.0	24.0	24.0	24.0
Total Split (s)	18.0	18.0		38.0	38.0		54.0	54.0	54.0	54.0	54.0	54.0
Total Split (%)	16.4%	16.4%		34.5%	34.5%		49.1%	49.1%	49.1%	49.1%	49.1%	49.1%
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	4.0
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min	Min	Min	Min
Act Effct Green (s)		6.1		73.8	73.8			21.6	21.6		21.6	
Actuated g/C Ratio		0.06		0.67	0.67			0.20	0.20		0.20	

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/17/2025

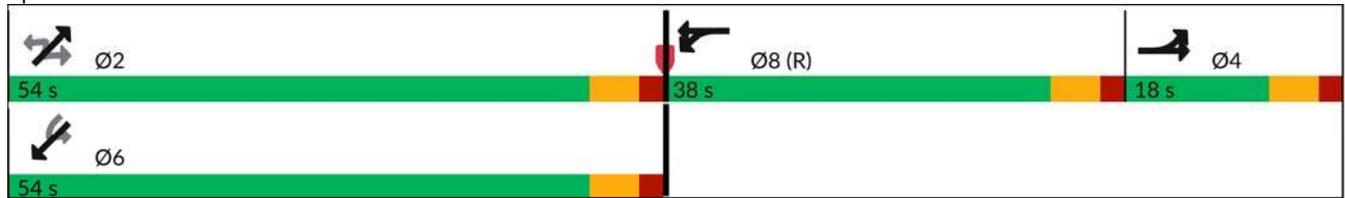


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.09		0.20	0.20			0.70	0.50		0.48	
Control Delay (s/veh)		48.1		13.6	13.6			51.1	7.6		19.2	
Queue Delay		0.0		1.6	1.6			0.0	0.0		0.0	
Total Delay (s/veh)		48.1		15.2	15.2			51.1	7.6		19.2	
LOS		D		B	B			D	A		B	
Approach Delay (s/veh)		48.1			15.2			29.0			19.2	
Approach LOS		D			B			C			B	
Queue Length 50th (ft)		6		54	55			170	0		33	
Queue Length 95th (ft)		22		128	129			236	62		128	
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		202		1127	1127			812	847		1475	
Starvation Cap Reductn		0		730	729			0	0		183	
Spillback Cap Reductn		0		0	0			0	13		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.04		0.56	0.57			0.32	0.32		0.25	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay (s/veh):	22.0
Intersection LOS:	C
Intersection Capacity Utilization:	43.5%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/17/2025

													
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR	
Lane Configurations													
Traffic Volume (vph)	10	295	55	0	262	125	36	219	0	174	252	16	
Future Volume (vph)	10	295	55	0	262	125	36	219	0	174	252	16	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12	
Grade (%)		0%			0%			0%			0%		
Storage Length (ft)	0		0	0		0	0		0	78		0	
Storage Lanes	0		0	0		1	0		0	1		0	
Taper Length (ft)	25			25			25			95			
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00	
Ped Bike Factor													
Frt		0.977				0.850						0.991	
Flt Protected		0.999						0.993		0.950			
Satd. Flow (prot)	0	3283	0	0	3574	1599	0	3539	0	1787	1857	0	
Flt Permitted		0.944						0.846		0.392			
Satd. Flow (perm)	0	3102	0	0	3574	1599	0	3015	0	737	1857	0	
Right Turn on Red			Yes			Yes			Yes			Yes	
Satd. Flow (RTOR)		23				140						4	
Link Speed (mph)		30			30			30				30	
Link Distance (ft)		386			211			255				600	
Travel Time (s)		8.8			4.8			5.8				13.6	
Confl. Peds. (#/hr)													
Confl. Bikes (#/hr)													
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	
Heavy Vehicles (%)	12%	2%	0%	0%	1%	1%	3%	1%	0%	1%	1%	8%	
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0	
Parking (#/hr)	0	0	0										
Mid-Block Traffic (%)		0%			0%			0%				0%	
Shared Lane Traffic (%)													
Lane Group Flow (vph)	0	404	0	0	294	140	0	286	0	196	301	0	
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA		
Protected Phases		6			2			4		3	8		
Permitted Phases	6					2	4			8			
Detector Phase	6	6			2	2	4	4		3	8		
Switch Phase													
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0		
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0		
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0		
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%		
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0		
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0		
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0		
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0		
Lead/Lag							Lag	Lag		Lead			
Lead-Lag Optimize?							Yes	Yes		Yes			
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None		
Act Effct Green (s)		66.9			66.9	66.9		15.6		33.6	31.1		
Actuated g/C Ratio		0.61			0.61	0.61		0.14		0.31	0.28		

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/17/2025

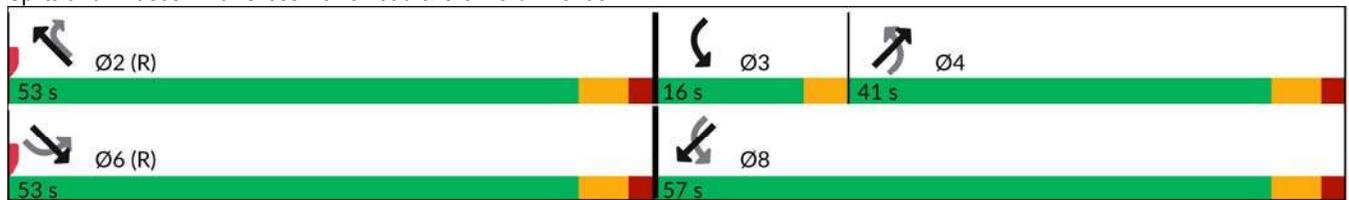


Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.21			0.14	0.14		0.67		0.58	0.57	
Control Delay (s/veh)		10.0			9.7	2.3		24.8		36.2	37.2	
Queue Delay		0.0			0.5	0.7		0.0		0.0	0.0	
Total Delay (s/veh)		10.0			10.2	3.1		24.8		36.2	37.2	
LOS		A			B	A		C		D	D	
Approach Delay (s/veh)		10.0			7.9			24.8			36.8	
Approach LOS		A			A			C			D	
Queue Length 50th (ft)		59			27	2		15		107	178	
Queue Length 95th (ft)		93			43	13		78		159	248	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1894			2172	1026		959		344	863	
Starvation Cap Reductn		0			1452	641		12		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.21			0.41	0.36		0.30		0.57	0.35	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.67
Intersection Signal Delay (s/veh):	20.3
Intersection LOS:	C
Intersection Capacity Utilization:	53.6%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue



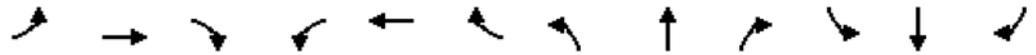
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

04/17/2025

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	256	6	112	416	136	13	253	82	164	304	1
Future Volume (vph)	0	256	6	112	416	136	13	253	82	164	304	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.997			0.963			0.963				
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3530	0	1787	3400	0	1805	3425	0	1787	3539	0
Flt Permitted				0.387			0.559			0.511		
Satd. Flow (perm)	0	3530	0	728	3400	0	1062	3425	0	961	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		2			48			38				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	2%	0%	1%	2%	3%	0%	2%	0%	1%	2%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)						0						
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	273	0	117	575	0	14	349	0	171	318	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	35.0		25.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	31.8%		22.7%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		13.8		30.0	27.5		65.7	57.4		73.0	66.6	
Actuated g/C Ratio		0.13		0.27	0.25		0.60	0.52		0.66	0.61	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

04/17/2025

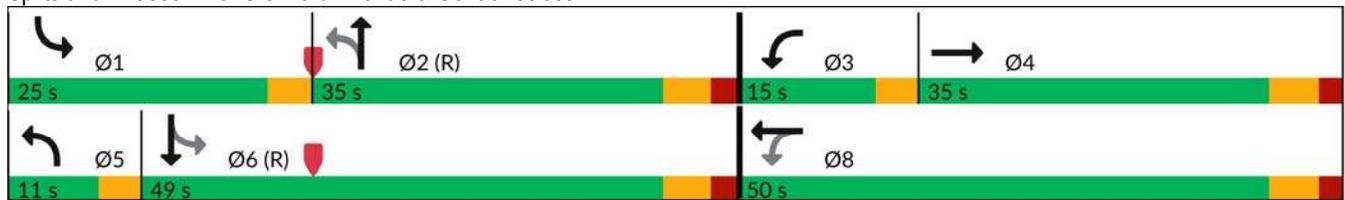


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
v/c Ratio		0.62		0.39	0.65		0.02	0.19		0.24	0.15	
Control Delay (s/veh)		53.5		34.0	36.9		8.0	13.8		7.2	9.3	
Queue Delay		0.1		0.0	0.0		0.0	0.0		0.6	0.6	
Total Delay (s/veh)		53.6		34.0	36.9		8.0	13.8		7.8	10.0	
LOS		D		C	D		A	B		A	A	
Approach Delay (s/veh)		53.6			36.4			13.6			9.2	
Approach LOS		D			D			B			A	
Queue Length 50th (ft)		94		64	173		3	59		54	54	
Queue Length 95th (ft)		138		106	222		11	100		82	82	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		932		309	1388		701	1804		798	2142	
Starvation Cap Reductn		139		0	0		0	0		354	1460	
Spillback Cap Reductn		0		0	0		11	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.34		0.38	0.41		0.02	0.19		0.39	0.47	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.65
Intersection Signal Delay (s/veh):	27.1
Intersection LOS:	C
Intersection Capacity Utilization:	48.8%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 7th TWSC
 10: Gross Point Road & Wellington Court

04/17/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations	TT		T			TT
Traffic Vol, veh/h	0	1	500	0	5	708
Future Vol, veh/h	0	1	500	0	5	708
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	543	0	5	770

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	939	543	0	0	543	0
Stage 1	543	-	-	-	-	-
Stage 2	396	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	277	538	-	-	1023	-
Stage 1	581	-	-	-	-	-
Stage 2	650	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	275	538	-	-	1023	-
Mov Cap-2 Maneuver	275	-	-	-	-	-
Stage 1	581	-	-	-	-	-
Stage 2	646	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s/v	11.7	0	0.12
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	538	25	-
HCM Lane V/C Ratio	-	-	0.002	0.005	-
HCM Control Delay (s/veh)	-	-	11.7	8.5	0.1
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

HCM 7th TWSC
 12: East Site Access & Central Street

04/17/2025

Intersection						
Int Delay, s/veh	0.6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↓		↑
Traffic Vol, veh/h	8	0	1	18	0	1
Future Vol, veh/h	8	0	1	18	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	0	1	20	0	1

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	-	9	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	4.12	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	2.218	-	-
Pot Cap-1 Maneuver	-	0	1611	-	0
Stage 1	-	0	-	-	0
Stage 2	-	0	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1611	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s/v	0	0.38	8.36
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT
Capacity (veh/h)	1073	-	95	-
HCM Lane V/C Ratio	0.001	-	0.001	-
HCM Control Delay (s/veh)	8.4	-	7.2	0
HCM Lane LOS	A	-	A	A
HCM 95th %tile Q(veh)	0	-	0	-

HCM 7th TWSC
 14: Gross Point Road & Access Drive

04/17/2025

Intersection						
Int Delay, s/veh	0					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations		↗		↑	↗↘	
Traffic Vol, veh/h	0	0	0	0	0	0
Future Vol, veh/h	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	0	0	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	1	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.93	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.319	-	-	-	-
Pot Cap-1 Maneuver	0	1084	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	1084	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s/v	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NET SELn1	SWT	SWR
Capacity (veh/h)	-	-	-
HCM Lane V/C Ratio	-	-	-
HCM Control Delay (s/veh)	-	0	-
HCM Lane LOS	-	A	-
HCM 95th %tile Q(veh)	-	-	-

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – 2031 No-Build
Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

04/16/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	5	12	5	335	5	1	0	251	372	1	223	8
Future Volume (vph)	5	12	5	335	5	1	0	251	372	1	223	8
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	0.95	0.95	0.95
Ped Bike Factor												
Frt		0.971			0.999				0.850		0.995	
Flt Protected		0.989		0.950	0.954							
Satd. Flow (prot)	0	1643	0	1681	1688	0	0	1863	1568	0	3558	0
Flt Permitted		0.989		0.950	0.954						0.954	
Satd. Flow (perm)	0	1643	0	1681	1688	0	0	1863	1568	0	3394	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5							400			4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	10%	25%	2%	0%	0%	0%	2%	3%	0%	1%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)				49%								
Lane Group Flow (vph)	0	23	0	184	182	0	0	270	400	0	250	0
Turn Type	Split	NA		Split	NA			NA	Perm	Perm	NA	
Protected Phases	4	4		8	8			2			6	
Permitted Phases							2		2	6		
Detector Phase	4	4		8	8		2	2	2	6	6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0	5.0	
Minimum Split (s)	20.0	20.0		24.0	24.0		24.0	24.0	24.0	24.0	24.0	
Total Split (s)	20.0	20.0		36.0	36.0		54.0	54.0	54.0	54.0	54.0	
Total Split (%)	18.2%	18.2%		32.7%	32.7%		49.1%	49.1%	49.1%	49.1%	49.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min	Min	Min	
Act Effct Green (s)		6.8		69.0	69.0			23.4	23.4		23.4	
Actuated g/C Ratio		0.06		0.63	0.63			0.21	0.21		0.21	

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/16/2025

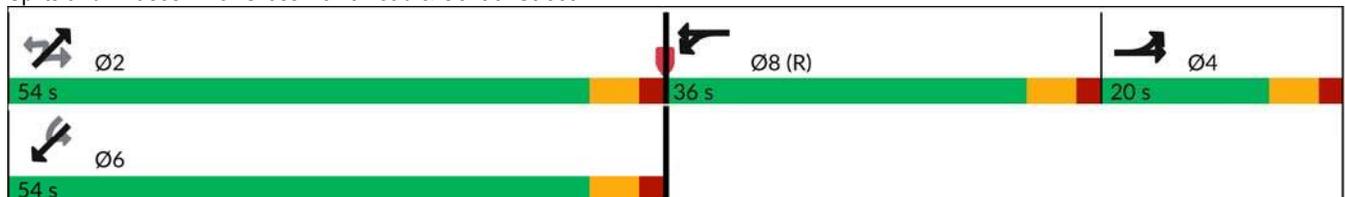


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.22		0.17	0.17			0.68	0.62		0.35	
Control Delay (s/veh)		45.4		8.1	8.1			48.0	7.7		15.6	
Queue Delay		0.0		1.3	1.3			0.0	0.2		0.0	
Total Delay (s/veh)		45.4		9.4	9.4			48.0	7.9		15.6	
LOS		D		A	A			D	A		B	
Approach Delay (s/veh)		45.4			9.4			24.1			15.6	
Approach LOS		D			A			C			B	
Queue Length 50th (ft)		12		26	25			177	0		22	
Queue Length 95th (ft)		38		66	66			240	72		106	
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		213		1054	1058			812	909		1483	
Starvation Cap Reductn		0		681	687			0	0		0	
Spillback Cap Reductn		0		0	0			0	106		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.11		0.49	0.49			0.33	0.50		0.17	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay (s/veh):	18.7
Intersection LOS:	B
Intersection Capacity Utilization:	48.6%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/16/2025

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	11	311	33	2	276	145	26	231	0	117	197	15
Future Volume (vph)	11	311	33	2	276	145	26	231	0	117	197	15
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	0.95	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Ped Bike Factor												
Frt		0.986				0.850					0.990	
Flt Protected		0.998						0.995		0.950		
Satd. Flow (prot)	0	3227	0	0	3574	1599	0	3498	0	1770	1850	0
Flt Permitted		0.942			0.954			0.887		0.391		
Satd. Flow (perm)	0	3046	0	0	3410	1599	0	3118	0	728	1850	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		12				159						5
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	6%	5%	0%	2%	1%	1%	0%	3%	0%	2%	1%	11%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)	0	0	0									
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	390	0	0	305	159	0	283	0	129	232	0
Turn Type	Perm	NA		Perm	NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6			2		2	4			8		
Detector Phase	6	6		2	2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0		24.0	24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0		53.0	53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%		48.2%	48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0		4.0	4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max		C-Max	C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		68.3			68.3	68.3		15.2		32.2	29.7	
Actuated g/C Ratio		0.62			0.62	0.62		0.14		0.29	0.27	

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

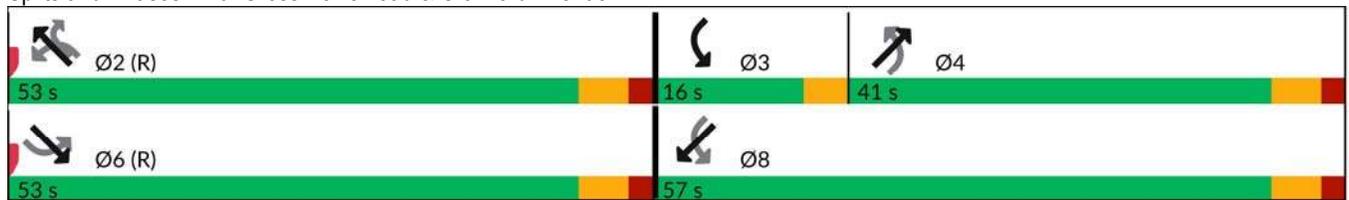
04/16/2025

Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.21			0.14	0.15		0.66		0.41	0.46	
Control Delay (s/veh)		9.7			4.3	0.6		31.3		32.5	35.0	
Queue Delay		0.0			0.4	0.6		0.0		0.0	0.0	
Total Delay (s/veh)		9.7			4.7	1.1		31.3		32.5	35.0	
LOS		A			A	A		C		C	D	
Approach Delay (s/veh)		9.7			3.4			31.3			34.1	
Approach LOS		A			A			C			C	
Queue Length 50th (ft)		57			12	0		25		69	132	
Queue Length 95th (ft)		92			24	0		81		110	194	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1896			2118	1053		992		331	860	
Starvation Cap Reductn		0			1304	600		13		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.21			0.37	0.35		0.29		0.39	0.27	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWTL and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.66
Intersection Signal Delay (s/veh):	17.7
Intersection LOS:	B
Intersection Capacity Utilization:	51.1%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue



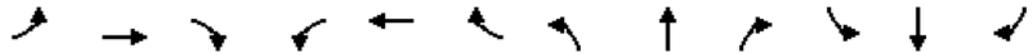
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

04/16/2025

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	374	11	49	320	107	20	316	175	223	201	1
Future Volume (vph)	0	374	11	49	320	107	20	316	175	223	201	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.996			0.962			0.947			0.999	
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3461	0	1752	3380	0	1805	3373	0	1770	3403	0
Flt Permitted				0.280			0.617			0.398		
Satd. Flow (perm)	0	3461	0	517	3380	0	1172	3373	0	741	3403	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		3			50			89				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	4%	0%	3%	2%	5%	0%	1%	2%	2%	6%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)						0						
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	410	0	52	454	0	21	522	0	237	215	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	31.0		29.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	28.2%		26.4%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		18.5		30.6	28.1		63.2	54.6		72.4	65.9	
Actuated g/C Ratio		0.17		0.28	0.26		0.57	0.50		0.66	0.60	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

04/16/2025

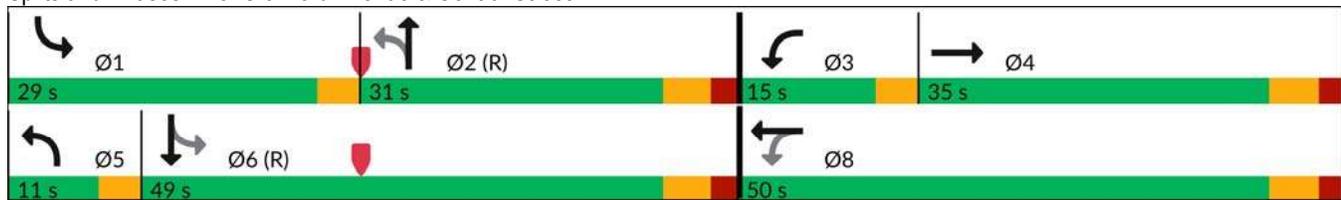


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
v/c Ratio		0.70		0.23	0.51		0.03	0.30		0.40	0.11	
Control Delay (s/veh)		52.2		28.4	31.8		9.6	16.1		10.4	9.8	
Queue Delay		0.9		0.0	0.0		0.0	0.0		0.3	0.4	
Total Delay (s/veh)		53.1		28.4	31.8		9.6	16.1		10.8	10.2	
LOS		D		C	C		A	B		B	B	
Approach Delay (s/veh)		53.1			31.4			15.8			10.5	
Approach LOS		D			C			B			B	
Queue Length 50th (ft)		145		27	125		5	92		68	31	
Queue Length 95th (ft)		190		51	158		17	164		103	54	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		914		272	1382		731	1719		726	2038	
Starvation Cap Reductn		264		0	0		0	0		167	1390	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.63		0.19	0.33		0.03	0.30		0.42	0.33	

Intersection Summary

Area Type: Other
 Cycle Length: 110
 Actuated Cycle Length: 110
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
 Natural Cycle: 70
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.70
 Intersection Signal Delay (s/veh): 26.7
 Intersection Capacity Utilization 58.2%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service B

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 7th TWSC
 10: Gross Point Road & Wellington Court

04/16/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	1	5	618	0	4	559
Future Vol, veh/h	1	5	618	0	4	559
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	5	672	0	4	608

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	984	672	0	0	672	0
Stage 1	672	-	-	-	-	-
Stage 2	313	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	260	455	-	-	917	-
Stage 1	507	-	-	-	-	-
Stage 2	716	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	259	455	-	-	917	-
Mov Cap-2 Maneuver	259	-	-	-	-	-
Stage 1	507	-	-	-	-	-
Stage 2	712	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s/v	14.06	0	0.11
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	404	26	-
HCM Lane V/C Ratio	-	-	0.016	0.005	-
HCM Control Delay (s/veh)	-	-	14.1	8.9	0.1
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

HCM 7th TWSC
 12: East Site Access & Central Street

04/16/2025

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↓		↑
Traffic Vol, veh/h	22	1	1	12	0	1
Future Vol, veh/h	22	1	1	12	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	24	1	1	13	0	1

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	25	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	4.12	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	2.218	-	-
Pot Cap-1 Maneuver	-	-	1589	-	0
Stage 1	-	-	-	-	0
Stage 2	-	-	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1589	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s/v	0	0.56	8.43
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1052	-	-	138	-
HCM Lane V/C Ratio	0.001	-	-	0.001	-
HCM Control Delay (s/veh)	8.4	-	-	7.3	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 7th TWSC
 14: Gross Point Road & Access Drive

04/16/2025

Intersection						
Int Delay, s/veh	0					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations		↗		↑	↗↘	
Traffic Vol, veh/h	0	0	0	0	0	0
Future Vol, veh/h	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	2	2	0
Mvmt Flow	0	0	0	0	0	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	1	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.9	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.3	-	-	-	-
Pot Cap-1 Maneuver	0	1090	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	1090	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s/v	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NET SELn1	SWT	SWR
Capacity (veh/h)	-	-	-
HCM Lane V/C Ratio	-	-	-
HCM Control Delay (s/veh)	-	0	-
HCM Lane LOS	-	A	-
HCM 95th %tile Q(veh)	-	-	-

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – 2031 No-Build
Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

04/16/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕		↕	↕			↕	↕		↕↔	
Traffic Volume (vph)	2	6	1	430	15	6	0	258	265	2	317	5
Future Volume (vph)	2	6	1	430	15	6	0	258	265	2	317	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	0.95	0.95	0.95
Ped Bike Factor												
Frt		0.985			0.996				0.850		0.998	
Flt Protected		0.989		0.950	0.957							
Satd. Flow (prot)	0	1851	0	1681	1681	0	0	1863	1599	0	3547	0
Flt Permitted		0.989		0.950	0.957						0.953	
Satd. Flow (perm)	0	1851	0	1681	1681	0	0	1863	1599	0	3380	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		1			1				276			2
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	0%	0%	2%	8%	0%	0%	2%	1%	100%	1%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)				48%								
Lane Group Flow (vph)	0	9	0	233	237	0	0	269	276	0	337	0
Turn Type	Split	NA		Split	NA			NA	Perm	Perm	NA	
Protected Phases	4	4		8	8			2				6
Permitted Phases							2		2	6		
Detector Phase	4	4		8	8		2	2	2	6		6
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0	5.0		5.0
Minimum Split (s)	18.0	18.0		24.0	24.0		24.0	24.0	24.0	24.0		24.0
Total Split (s)	18.0	18.0		38.0	38.0		54.0	54.0	54.0	54.0		54.0
Total Split (%)	16.4%	16.4%		34.5%	34.5%		49.1%	49.1%	49.1%	49.1%		49.1%
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0	4.0		4.0
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0	2.0		2.0
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0			0.0
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0			6.0
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min	Min		Min
Act Effct Green (s)		6.1		72.7	72.7			22.6	22.6			22.6
Actuated g/C Ratio		0.06		0.66	0.66			0.21	0.21			0.21

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/16/2025

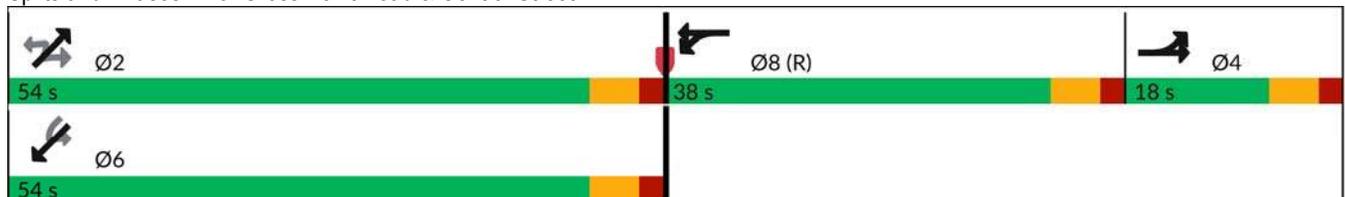


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.09		0.21	0.21			0.70	0.50		0.48	
Control Delay (s/veh)		48.1		15.1	15.0			49.8	7.2		18.6	
Queue Delay		0.0		1.8	1.8			0.0	0.0		0.0	
Total Delay (s/veh)		48.1		16.9	16.9			49.8	7.2		18.6	
LOS		D		B	B			D	A		B	
Approach Delay (s/veh)		48.1			16.9			28.3			18.6	
Approach LOS		D			B			C			B	
Queue Length 50th (ft)		6		63	64			178	0		35	
Queue Length 95th (ft)		22		140	141			242	61		127	
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		202		1111	1112			812	853		1476	
Starvation Cap Reductn		0		715	712			0	0		202	
Spillback Cap Reductn		0		0	0			0	16		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.04		0.59	0.59			0.33	0.33		0.26	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay (s/veh):	22.1
Intersection LOS:	C
Intersection Capacity Utilization:	44.6%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/16/2025

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	11	308	58	0	273	131	38	228	0	182	264	17
Future Volume (vph)	11	308	58	0	273	131	38	228	0	182	264	17
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Ped Bike Factor												
Frt		0.977				0.850						0.991
Flt Protected		0.999						0.993		0.950		
Satd. Flow (prot)	0	3282	0	0	3574	1599	0	3539	0	1787	1857	0
Flt Permitted		0.943						0.840		0.382		
Satd. Flow (perm)	0	3098	0	0	3574	1599	0	2994	0	719	1857	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		24				147						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	12%	2%	0%	0%	1%	1%	3%	1%	0%	1%	1%	8%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)	0	0	0									
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	423	0	0	307	147	0	299	0	204	316	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		66.2			66.2	66.2		16.2		34.3	31.8	
Actuated g/C Ratio		0.60			0.60	0.60		0.15		0.31	0.29	

Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/16/2025

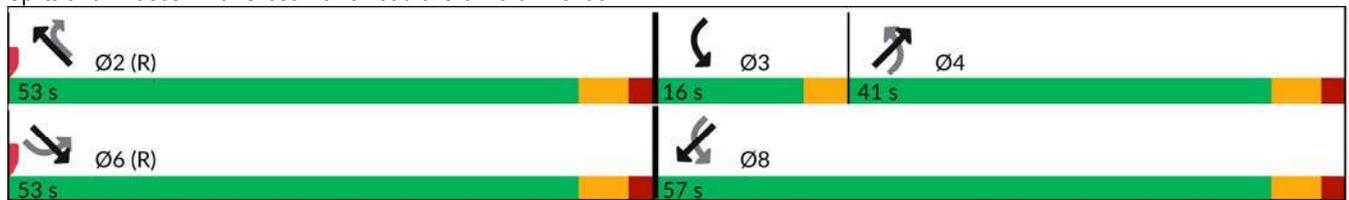


Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.23			0.14	0.14		0.68		0.60	0.59	
Control Delay (s/veh)		10.4			10.0	2.4		24.6		36.5	37.3	
Queue Delay		0.0			0.5	0.7		0.0		0.0	0.0	
Total Delay (s/veh)		10.4			10.5	3.1		24.6		36.5	37.3	
LOS		B			B	A		C		D	D	
Approach Delay (s/veh)		10.4			8.1			24.6			37.0	
Approach LOS		B			A			C			D	
Queue Length 50th (ft)		64			29	3		16		111	187	
Queue Length 95th (ft)		98			45	14		80		163	259	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1874			2151	1021		952		345	863	
Starvation Cap Reductn		0			1420	628		13		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.23			0.42	0.37		0.32		0.59	0.37	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay (s/veh):	20.4
Intersection LOS:	C
Intersection Capacity Utilization:	55.8%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue



Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

04/16/2025

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	267	6	117	435	142	15	264	86	171	313	1
Future Volume (vph)	0	267	6	117	435	142	15	264	86	171	313	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.997			0.963			0.963				
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3530	0	1787	3400	0	1805	3425	0	1787	3539	0
Flt Permitted				0.376			0.554			0.500		
Satd. Flow (perm)	0	3530	0	707	3400	0	1053	3425	0	941	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		2			48			39				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	2%	0%	1%	2%	3%	0%	2%	0%	1%	2%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)						0						
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	284	0	122	601	0	16	365	0	178	327	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	35.0		25.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	31.8%		22.7%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		14.1		30.5	28.0		65.1	56.7		72.5	66.1	
Actuated g/C Ratio		0.13		0.28	0.25		0.59	0.52		0.66	0.60	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

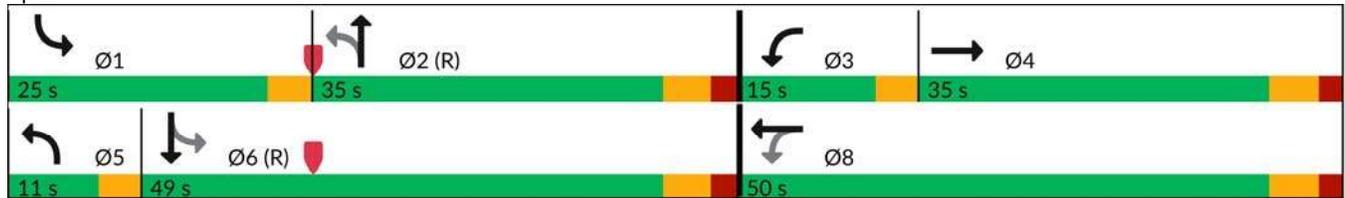
04/16/2025

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
v/c Ratio		0.63		0.41	0.67		0.02	0.20		0.26	0.15	
Control Delay (s/veh)		53.0		34.1	37.2		8.2	14.3		7.4	9.5	
Queue Delay		0.1		0.0	0.0		0.0	0.0		0.6	0.7	
Total Delay (s/veh)		53.1		34.1	37.2		8.2	14.3		8.0	10.1	
LOS		D		C	D		A	B		A	B	
Approach Delay (s/veh)		53.1			36.7			14.0			9.4	
Approach LOS		D			D			B			A	
Queue Length 50th (ft)		98		66	183		4	63		56	56	
Queue Length 95th (ft)		143		109	232		13	106		84	84	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		932		308	1388		689	1784		785	2127	
Starvation Cap Reductn		145		0	0		0	0		335	1438	
Spillback Cap Reductn		0		0	27		56	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.36		0.40	0.44		0.03	0.20		0.40	0.47	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.67
Intersection Signal Delay (s/veh):	27.3
Intersection LOS:	C
Intersection Capacity Utilization:	50.2%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 7th TWSC
 10: Gross Point Road & Wellington Court

04/16/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations	TT		T			TT
Traffic Vol, veh/h	0	1	522	0	5	743
Future Vol, veh/h	0	1	522	0	5	743
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	567	0	5	808

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	982	567	0	0	567	0
Stage 1	567	-	-	-	-	-
Stage 2	415	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	261	522	-	-	1003	-
Stage 1	566	-	-	-	-	-
Stage 2	636	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	259	522	-	-	1003	-
Mov Cap-2 Maneuver	259	-	-	-	-	-
Stage 1	566	-	-	-	-	-
Stage 2	631	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s/v	11.91	0	0.12
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWL	SWL	SWT
Capacity (veh/h)	-	-	522	24	-
HCM Lane V/C Ratio	-	-	0.002	0.005	-
HCM Control Delay (s/veh)	-	-	11.9	8.6	0.1
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

HCM 7th TWSC
 12: East Site Access & Central Street

04/16/2025

Intersection						
Int Delay, s/veh	0.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↔			↔		↔
Traffic Vol, veh/h	8	0	1	19	0	1
Future Vol, veh/h	8	0	1	19	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	0	1	21	0	1

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	9	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	4.12	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	2.218	-	-
Pot Cap-1 Maneuver	-	-	1611	-	0
Stage 1	-	-	-	-	0
Stage 2	-	-	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1611	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s/v	0	0.36	8.36
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1073	-	-	90	-
HCM Lane V/C Ratio	0.001	-	-	0.001	-
HCM Control Delay (s/veh)	8.4	-	-	7.2	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

HCM 7th TWSC
 14: Gross Point Road & Access Drive

04/16/2025

Intersection						
Int Delay, s/veh	0					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations		↗		↑	↗↘	
Traffic Vol, veh/h	0	0	0	0	0	0
Future Vol, veh/h	0	0	0	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	0	0	0	0

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	1	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.93	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.319	-	-	-	-
Pot Cap-1 Maneuver	0	1084	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	1084	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s/v	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NET SELn1	SWT	SWR
Capacity (veh/h)	-	-	-
HCM Lane V/C Ratio	-	-	-
HCM Control Delay (s/veh)	-	0	-
HCM Lane LOS	-	A	-
HCM 95th %tile Q(veh)	-	-	-

Capacity Analysis Summary Sheets
Weekday Morning Peak Hour – 2031 Projected
Conditions

Lanes, Volumes, Timings
3: Gross Point Road & Central Street

04/16/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕		↕	↕			↕	↕		↕	
Traffic Volume (vph)	23	35	8	343	20	1	18	251	372	0	232	17
Future Volume (vph)	23	35	8	343	20	1	18	251	372	0	232	17
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.983			0.999				0.850		0.990	
Flt Protected		0.983		0.950	0.958			0.997				
Satd. Flow (prot)	0	1694	0	1681	1697	0	0	1860	1568	0	3541	0
Flt Permitted		0.983		0.950	0.958			0.963				
Satd. Flow (perm)	0	1694	0	1681	1697	0	0	1796	1568	0	3541	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5							386			8
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	10%	25%	2%	0%	0%	0%	2%	3%	0%	1%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)				47%								
Lane Group Flow (vph)	0	72	0	196	196	0	0	289	400	0	267	0
Turn Type	Split	NA		Split	NA		Perm	NA	Perm		NA	
Protected Phases	4	4		8	8			2			6	
Permitted Phases							2		2			
Detector Phase	4	4		8	8		2	2	2		6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0		5.0	
Minimum Split (s)	20.0	20.0		24.0	24.0		24.0	24.0	24.0		24.0	
Total Split (s)	20.0	20.0		36.0	36.0		54.0	54.0	54.0		54.0	
Total Split (%)	18.2%	18.2%		32.7%	32.7%		49.1%	49.1%	49.1%		49.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0		4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0		2.0	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min		Min	
Act Effct Green (s)		9.7		59.6	59.6			25.0	25.0		25.0	
Actuated g/C Ratio		0.09		0.54	0.54			0.23	0.23		0.23	

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/16/2025

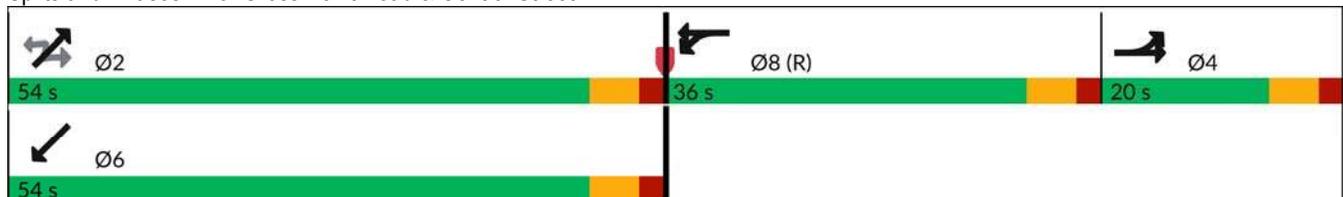


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.47		0.22	0.21			0.71	0.61			0.33
Control Delay (s/veh)		53.5		11.0	11.0			47.9	8.0			16.6
Queue Delay		0.0		0.8	0.8			0.0	0.3			0.0
Total Delay (s/veh)		53.5		11.8	11.8			47.9	8.4			16.6
LOS		D		B	B			D	A			B
Approach Delay (s/veh)		53.5			11.8			24.9				16.6
Approach LOS		D			B			C				B
Queue Length 50th (ft)		46		33	33			189	8			90
Queue Length 95th (ft)		90		80	80			252	80			120
Internal Link Dist (ft)		283			141			230				175
Turn Bay Length (ft)									95			
Base Capacity (vph)		219		911	919			783	901			1549
Starvation Cap Reductn		0		473	480			0	0			0
Spillback Cap Reductn		0		0	0			0	139			0
Storage Cap Reductn		0		0	0			0	0			0
Reduced v/c Ratio		0.33		0.45	0.45			0.37	0.52			0.17

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.71
Intersection Signal Delay (s/veh):	21.2
Intersection LOS:	C
Intersection Capacity Utilization:	52.9%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/16/2025

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	11	311	42	0	276	145	35	240	0	117	206	15
Future Volume (vph)	11	311	42	0	276	145	35	240	0	117	206	15
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Ped Bike Factor												
Frt		0.983				0.850					0.990	
Flt Protected		0.999						0.994		0.950		
Satd. Flow (prot)	0	3224	0	0	3574	1599	0	3497	0	1770	1850	0
Flt Permitted		0.943						0.867		0.377		
Satd. Flow (perm)	0	3043	0	0	3574	1599	0	3050	0	702	1850	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		16				159						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	6%	5%	0%	2%	1%	1%	0%	3%	0%	2%	1%	11%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)	0	0	0									
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	400	0	0	303	159	0	302	0	129	242	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		67.4			67.4	67.4		16.1		33.1	30.6	
Actuated g/C Ratio		0.61			0.61	0.61		0.15		0.30	0.28	

Lanes, Volumes, Timings
 6: Gross Point Road & Crawford Avenue

04/16/2025

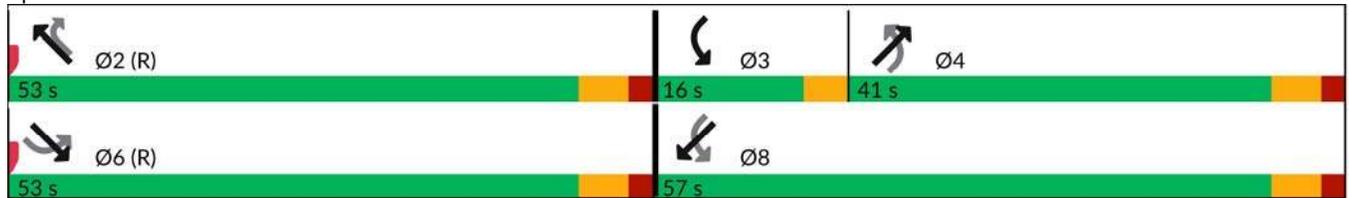


Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.21			0.14	0.15		0.68		0.41	0.47	
Control Delay (s/veh)		10.1			4.5	0.6		55.2		31.7	34.7	
Queue Delay		0.0			0.3	0.6		0.0		0.0	0.0	
Total Delay (s/veh)		10.1			4.9	1.2		55.2		31.7	34.7	
LOS		B			A	A		E		C	C	
Approach Delay (s/veh)		10.1			3.6			55.2			33.6	
Approach LOS		B			A			E			C	
Queue Length 50th (ft)		60			12	0		78		68	138	
Queue Length 95th (ft)		96			25	0		119		109	199	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1871			2191	1041		970		332	859	
Starvation Cap Reductn		0			1372	588		17		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.21			0.37	0.35		0.32		0.39	0.28	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.68
Intersection Signal Delay (s/veh):	22.7
Intersection LOS:	C
Intersection Capacity Utilization:	52.4%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue



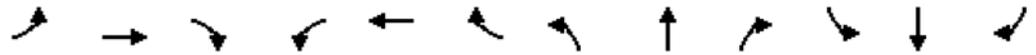
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

04/16/2025

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	389	19	49	335	107	28	316	175	223	201	1
Future Volume (vph)	0	389	19	49	335	107	28	316	175	223	201	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.993			0.964			0.947				0.999
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3453	0	1752	3388	0	1805	3373	0	1770	3403	0
Flt Permitted				0.266			0.617			0.396		
Satd. Flow (perm)	0	3453	0	491	3388	0	1172	3373	0	738	3403	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		4			47			89				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	4%	0%	3%	2%	5%	0%	1%	2%	2%	6%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)						0						
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	434	0	52	470	0	30	522	0	237	215	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	31.0		29.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	28.2%		26.4%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		19.3		31.3	28.8		62.5	53.7		71.7	63.0	
Actuated g/C Ratio		0.18		0.28	0.26		0.57	0.49		0.65	0.57	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

04/16/2025

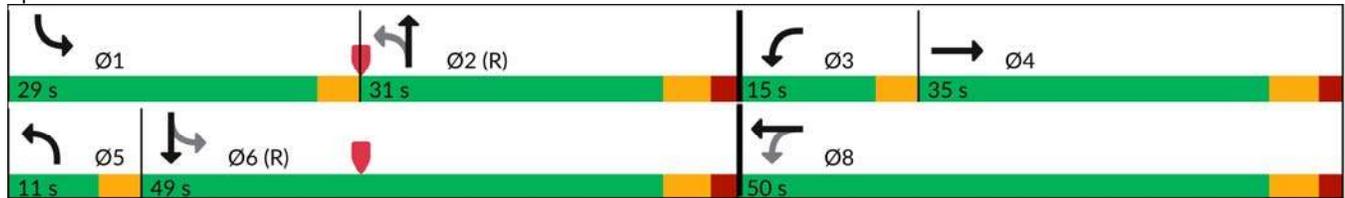


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
v/c Ratio		0.71		0.23	0.51		0.04	0.31		0.40	0.11	
Control Delay (s/veh)		52.5		27.9	31.7		9.9	16.6		10.7	11.0	
Queue Delay		1.3		0.0	0.0		0.0	0.0		0.4	0.2	
Total Delay (s/veh)		53.8		27.9	31.7		9.9	16.6		11.1	11.2	
LOS		D		C	C		A	B		B	B	
Approach Delay (s/veh)		53.8			31.3			16.2			11.2	
Approach LOS		D			C			B			B	
Queue Length 50th (ft)		154		27	131		7	94		69	35	
Queue Length 95th (ft)		187		51	163		22	166		103	54	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		913		271	1383		722	1693		719	1950	
Starvation Cap Reductn		279		0	0		0	0		164	1178	
Spillback Cap Reductn		0		0	0		0	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.68		0.19	0.34		0.04	0.31		0.43	0.28	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.71
Intersection Signal Delay (s/veh):	27.4
Intersection LOS:	C
Intersection Capacity Utilization:	58.9%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 7th TWSC
 10: Gross Point Road & Wellington Court

04/16/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	1	5	636	0	4	579
Future Vol, veh/h	1	5	636	0	4	579
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	5	691	0	4	629

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1015	691	0	0	691	0
Stage 1	691	-	-	-	-	-
Stage 2	323	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	249	443	-	-	901	-
Stage 1	496	-	-	-	-	-
Stage 2	707	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	247	443	-	-	901	-
Mov Cap-2 Maneuver	247	-	-	-	-	-
Stage 1	496	-	-	-	-	-
Stage 2	703	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s/v	14.35	0	0.11
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	392	25	-
HCM Lane V/C Ratio	-	-	0.017	0.005	-
HCM Control Delay (s/veh)	-	-	14.3	9	0.1
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0	-

HCM 7th TWSC
 12: East Site Access & Central Street

04/16/2025

Intersection						
Int Delay, s/veh	5.6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↓		↑
Traffic Vol, veh/h	23	0	42	13	0	44
Future Vol, veh/h	23	0	42	13	0	44
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	25	0	46	14	0	48

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	-	25	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	4.12	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	2.218	-	-
Pot Cap-1 Maneuver	-	0	1589	-	0
Stage 1	-	0	-	-	0
Stage 2	-	0	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1589	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s/v	0	5.6	8.59
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT
Capacity (veh/h)	1051	-	1375	-
HCM Lane V/C Ratio	0.045	-	0.029	-
HCM Control Delay (s/veh)	8.6	-	7.3	0
HCM Lane LOS	A	-	A	A
HCM 95th %tile Q(veh)	0.1	-	0.1	-

HCM 7th TWSC
 14: Gross Point Road & Access Drive

04/16/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations		↗		↑	↕	
Traffic Vol, veh/h	0	15	0	636	562	17
Future Vol, veh/h	0	15	0	636	562	17
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	16	0	691	611	18

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	315	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.93	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.319	-	-	-	-
Pot Cap-1 Maneuver	0	682	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	682	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s/v10.41		0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NET SELn1	SWT	SWR
Capacity (veh/h)	- 682	-	-
HCM Lane V/C Ratio	- 0.024	-	-
HCM Control Delay (s/veh)	- 10.4	-	-
HCM Lane LOS	- B	-	-
HCM 95th %tile Q(veh)	- 0.1	-	-

HCM 7th TWSC
 16: West Site Access & Central Street

04/16/2025

Intersection						
Int Delay, s/veh	0.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	22	1	1	12	0	1
Future Vol, veh/h	22	1	1	12	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	24	1	1	13	0	1

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	25	0	40
Stage 1	-	-	-	-	24
Stage 2	-	-	-	-	15
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1589	-	972
Stage 1	-	-	-	-	998
Stage 2	-	-	-	-	1008
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1589	-	971
Mov Cap-2 Maneuver	-	-	-	-	971
Stage 1	-	-	-	-	998
Stage 2	-	-	-	-	1007

Approach	EB	WB	NB
HCM Control Delay, s/v	0	0.56	8.43
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1052	-	-	138	-
HCM Lane V/C Ratio	0.001	-	-	0.001	-
HCM Control Delay (s/veh)	8.4	-	-	7.3	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

Capacity Analysis Summary Sheets
Weekday Evening Peak Hour – 2031 Projected
Conditions

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/16/2025



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations		↕		↕	↕			↕	↕		↕	
Traffic Volume (vph)	22	31	5	438	30	6	18	258	265	0	325	14
Future Volume (vph)	22	31	5	438	30	6	18	258	265	0	325	14
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		95	0		0
Storage Lanes	0		0	1		0	0		1	0		0
Taper Length (ft)	25			25			25			25		
Lane Util. Factor	1.00	1.00	1.00	0.95	0.95	1.00	1.00	1.00	1.00	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.989			0.996				0.850		0.994	
Flt Protected		0.981		0.950	0.959			0.997				
Satd. Flow (prot)	0	1843	0	1681	1679	0	0	1860	1599	0	3554	0
Flt Permitted		0.981		0.950	0.959			0.955				
Satd. Flow (perm)	0	1843	0	1681	1679	0	0	1781	1599	0	3554	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		3			1				267			5
Link Speed (mph)		30			30			30				30
Link Distance (ft)		363			221			310				255
Travel Time (s)		8.3			5.0			7.0				5.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	0%	0%	2%	8%	0%	0%	2%	1%	100%	1%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)				46%								
Lane Group Flow (vph)	0	60	0	246	247	0	0	288	276	0	354	0
Turn Type	Split	NA		Split	NA		Perm	NA	Perm		NA	
Protected Phases	4	4		8	8			2			6	
Permitted Phases							2		2			
Detector Phase	4	4		8	8		2	2	2		6	
Switch Phase												
Minimum Initial (s)	5.0	5.0		5.0	5.0		5.0	5.0	5.0		5.0	
Minimum Split (s)	18.0	18.0		24.0	24.0		24.0	24.0	24.0		24.0	
Total Split (s)	18.0	18.0		38.0	38.0		54.0	54.0	54.0		54.0	
Total Split (%)	16.4%	16.4%		34.5%	34.5%		49.1%	49.1%	49.1%		49.1%	
Yellow Time (s)	4.0	4.0		4.0	4.0		4.0	4.0	4.0		4.0	
All-Red Time (s)	2.0	2.0		2.0	2.0		2.0	2.0	2.0		2.0	
Lost Time Adjust (s)		0.0		0.0	0.0			0.0	0.0		0.0	
Total Lost Time (s)		6.0		6.0	6.0			6.0	6.0		6.0	
Lead/Lag												
Lead-Lag Optimize?												
Recall Mode	None	None		C-Max	C-Max		Min	Min	Min		Min	
Act Effct Green (s)		8.8		61.1	61.1			24.4	24.4		24.4	
Actuated g/C Ratio		0.08		0.56	0.56			0.22	0.22		0.22	

Lanes, Volumes, Timings
 3: Gross Point Road & Central Street

04/16/2025

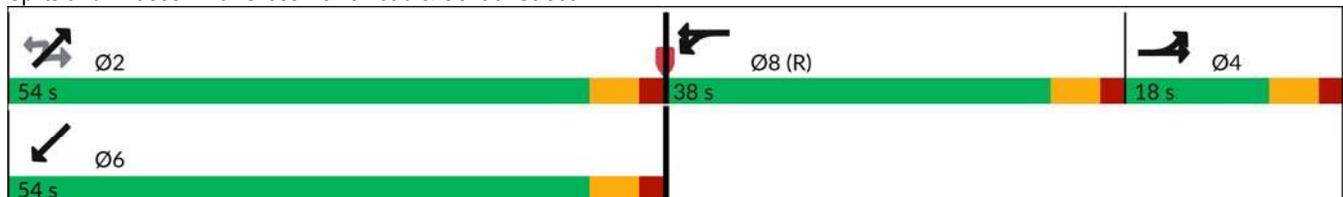


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.40		0.26	0.26			0.73	0.49		0.45	
Control Delay (s/veh)		52.9		20.7	20.7			49.8	7.4		19.9	
Queue Delay		0.0		1.6	1.6			0.0	0.0		0.1	
Total Delay (s/veh)		52.9		22.3	22.2			49.8	7.4		20.0	
LOS		D		C	C			D	A		C	
Approach Delay (s/veh)		52.9			22.3			29.1			20.0	
Approach LOS		D			C			C			C	
Queue Length 50th (ft)		39		74	74			190	5		124	
Queue Length 95th (ft)		80		151	152			255	65		157	
Internal Link Dist (ft)		283			141			230			175	
Turn Bay Length (ft)									95			
Base Capacity (vph)		203		933	933			777	848		1553	
Starvation Cap Reductn		0		511	510			0	0		338	
Spillback Cap Reductn		0		0	0			0	16		0	
Storage Cap Reductn		0		0	0			0	0		0	
Reduced v/c Ratio		0.30		0.58	0.58			0.37	0.33		0.29	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	20 (18%), Referenced to phase 8:WBTL, Start of Green
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.73
Intersection Signal Delay (s/veh):	25.6
Intersection LOS:	C
Intersection Capacity Utilization:	58.1%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 3: Gross Point Road & Central Street



Lanes, Volumes, Timings
6: Gross Point Road & Crawford Avenue

04/16/2025

												
Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
Lane Configurations												
Traffic Volume (vph)	11	308	66	0	273	131	48	238	0	182	273	17
Future Volume (vph)	11	308	66	0	273	131	48	238	0	182	273	17
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	78		0
Storage Lanes	0		0	0		1	0		0	1		0
Taper Length (ft)	25			25			25			95		
Lane Util. Factor	0.95	0.95	0.95	1.00	0.95	1.00	0.95	0.95	0.95	1.00	1.00	1.00
Ped Bike Factor												
Frt		0.974				0.850					0.991	
Flt Protected		0.999						0.992		0.950		
Satd. Flow (prot)	0	3274	0	0	3574	1599	0	3534	0	1787	1857	0
Flt Permitted		0.943						0.822		0.367		
Satd. Flow (perm)	0	3090	0	0	3574	1599	0	2928	0	690	1857	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		28				147						4
Link Speed (mph)		30			30			30				30
Link Distance (ft)		386			211			255				600
Travel Time (s)		8.8			4.8			5.8				13.6
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89	0.89
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	12%	2%	0%	0%	1%	1%	3%	1%	0%	1%	1%	8%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)	0	0	0									
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	432	0	0	307	147	0	321	0	204	326	0
Turn Type	Perm	NA			NA	Perm	Perm	NA		pm+pt	NA	
Protected Phases		6			2			4		3	8	
Permitted Phases	6					2	4			8		
Detector Phase	6	6			2	2	4	4		3	8	
Switch Phase												
Minimum Initial (s)	5.0	5.0			5.0	5.0	5.0	5.0		5.0	5.0	
Minimum Split (s)	24.0	24.0			24.0	24.0	24.0	24.0		9.5	24.0	
Total Split (s)	53.0	53.0			53.0	53.0	41.0	41.0		16.0	57.0	
Total Split (%)	48.2%	48.2%			48.2%	48.2%	37.3%	37.3%		14.5%	51.8%	
Yellow Time (s)	4.0	4.0			4.0	4.0	4.0	4.0		3.5	4.0	
All-Red Time (s)	2.0	2.0			2.0	2.0	2.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0			0.0	0.0		0.0		0.0	0.0	
Total Lost Time (s)		6.0			6.0	6.0		6.0		3.5	6.0	
Lead/Lag							Lag	Lag		Lead		
Lead-Lag Optimize?							Yes	Yes		Yes		
Recall Mode	C-Max	C-Max			C-Max	C-Max	None	None		None	None	
Act Effct Green (s)		65.1			65.1	65.1		17.3		35.4	32.9	
Actuated g/C Ratio		0.59			0.59	0.59		0.16		0.32	0.30	

Lanes, Volumes, Timings
 6: Gross Point Road & Crawford Avenue

04/16/2025

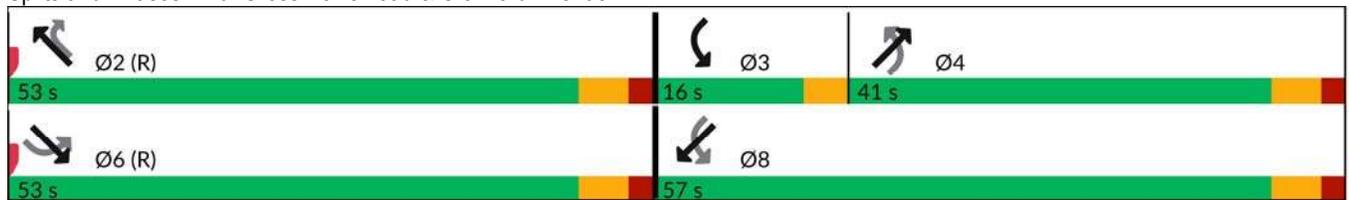


Lane Group	SEL	SET	SER	NWL	NWT	NWR	NEL	NET	NER	SWL	SWT	SWR
v/c Ratio		0.23			0.15	0.15		0.70		0.60	0.59	
Control Delay (s/veh)		10.9			10.5	2.4		51.4		35.5	36.4	
Queue Delay		0.0			0.5	0.7		0.0		0.0	0.0	
Total Delay (s/veh)		10.9			11.0	3.1		51.4		35.5	36.4	
LOS		B			B	A		D		D	D	
Approach Delay (s/veh)		10.9			8.5			51.4			36.0	
Approach LOS		B			A			D			D	
Queue Length 50th (ft)		66			30	3		80		109	192	
Queue Length 95th (ft)		104			46	14		118		159	262	
Internal Link Dist (ft)		306			131			175			520	
Turn Bay Length (ft)										78		
Base Capacity (vph)		1840			2115	1006		931		346	863	
Starvation Cap Reductn		0			1386	613		20		0	0	
Spillback Cap Reductn		0			0	0		0		0	0	
Storage Cap Reductn		0			0	0		0		0	0	
Reduced v/c Ratio		0.23			0.42	0.37		0.35		0.59	0.38	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NWT and 6:SETL, Start of Green
Natural Cycle:	60
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.70
Intersection Signal Delay (s/veh):	25.4
Intersection LOS:	C
Intersection Capacity Utilization:	57.1%
ICU Level of Service:	B
Analysis Period (min):	15

Splits and Phases: 6: Gross Point Road & Crawford Avenue



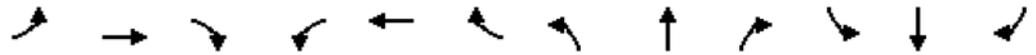
Lanes, Volumes, Timings
8: Crawford Avenue & Central Street

04/16/2025

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	0	283	15	117	450	142	23	264	86	171	313	1
Future Volume (vph)	0	283	15	117	450	142	23	264	86	171	313	1
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	90		0	85		0	58		0
Storage Lanes	0		0	1		0	1		0	1		0
Taper Length (ft)	25			60			60			48		
Lane Util. Factor	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95	1.00	0.95	0.95
Ped Bike Factor												
Frt		0.992			0.964			0.963				
Flt Protected				0.950			0.950			0.950		
Satd. Flow (prot)	0	3514	0	1787	3404	0	1805	3425	0	1787	3539	0
Flt Permitted				0.348			0.554			0.498		
Satd. Flow (perm)	0	3514	0	655	3404	0	1053	3425	0	937	3539	0
Right Turn on Red			Yes			Yes			Yes			Yes
Satd. Flow (RTOR)		5			46			39				
Link Speed (mph)		30			30			35				30
Link Distance (ft)		221			385			590				211
Travel Time (s)		5.0			8.8			11.5				4.8
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	0%	2%	0%	1%	2%	3%	0%	2%	0%	1%	2%	0%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)						0						
Mid-Block Traffic (%)		0%			0%			0%				0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	311	0	122	617	0	24	365	0	178	327	0
Turn Type		NA		pm+pt	NA		pm+pt	NA		pm+pt	NA	
Protected Phases		4		3	8		5	2		1	6	
Permitted Phases				8			2			6		
Detector Phase		4		3	8		5	2		1	6	
Switch Phase												
Minimum Initial (s)		5.0		5.0	5.0		5.0	5.0		5.0	5.0	
Minimum Split (s)		24.0		9.5	24.0		9.5	24.0		9.5	24.0	
Total Split (s)		35.0		15.0	50.0		11.0	35.0		25.0	49.0	
Total Split (%)		31.8%		13.6%	45.5%		10.0%	31.8%		22.7%	44.5%	
Yellow Time (s)		4.0		3.5	4.0		3.5	4.0		3.5	4.0	
All-Red Time (s)		2.0		0.0	2.0		0.0	2.0		0.0	2.0	
Lost Time Adjust (s)		0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)		6.0		3.5	6.0		3.5	6.0		3.5	6.0	
Lead/Lag		Lag		Lead			Lead	Lag		Lead	Lag	
Lead-Lag Optimize?		Yes		Yes			Yes	Yes		Yes	Yes	
Recall Mode		None		None	None		None	C-Max		None	C-Max	
Act Effct Green (s)		14.9		31.2	28.7		64.4	55.8		71.8	63.3	
Actuated g/C Ratio		0.14		0.28	0.26		0.59	0.51		0.65	0.58	

Lanes, Volumes, Timings
 8: Crawford Avenue & Central Street

04/16/2025

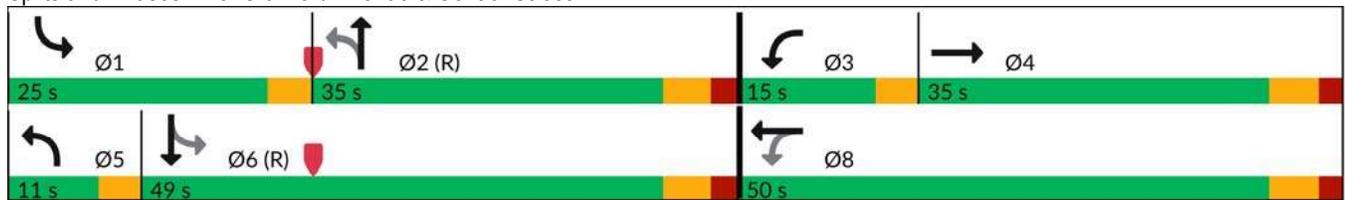


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
v/c Ratio		0.65		0.42	0.67		0.04	0.21		0.26	0.16	
Control Delay (s/veh)		54.1		33.6	36.9		8.5	14.8		7.8	10.8	
Queue Delay		0.2		0.0	0.0		0.0	0.0		0.6	0.4	
Total Delay (s/veh)		54.3		33.6	36.9		8.5	14.8		8.4	11.2	
LOS		D		C	D		A	B		A	B	
Approach Delay (s/veh)		54.3			36.4			14.4			10.2	
Approach LOS		D			D			B			B	
Queue Length 50th (ft)		104		65	188		6	64		56	61	
Queue Length 95th (ft)		142		107	236		17	108		86	84	
Internal Link Dist (ft)		141			305			510			131	
Turn Bay Length (ft)				90			85			58		
Base Capacity (vph)		930		304	1389		680	1756		777	2036	
Starvation Cap Reductn		164		0	0		0	0		329	1232	
Spillback Cap Reductn		0		0	64		67	0		0	0	
Storage Cap Reductn		0		0	0		0	0		0	0	
Reduced v/c Ratio		0.41		0.40	0.47		0.04	0.21		0.40	0.41	

Intersection Summary

Area Type:	Other
Cycle Length:	110
Actuated Cycle Length:	110
Offset:	0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green, Master Intersection
Natural Cycle:	70
Control Type:	Actuated-Coordinated
Maximum v/c Ratio:	0.67
Intersection Signal Delay (s/veh):	28.0
Intersection LOS:	C
Intersection Capacity Utilization:	51.0%
ICU Level of Service:	A
Analysis Period (min):	15

Splits and Phases: 8: Crawford Avenue & Central Street



HCM 7th TWSC
 10: Gross Point Road & Wellington Court

04/16/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	NWL	NWR	NET	NER	SWL	SWT
Lane Configurations						
Traffic Vol, veh/h	0	1	540	0	5	763
Future Vol, veh/h	0	1	540	0	5	763
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1	587	0	5	829

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	1013	587	0	0	587	0
Stage 1	587	-	-	-	-	-
Stage 2	426	-	-	-	-	-
Critical Hdwy	6.63	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.83	-	-	-	-	-
Follow-up Hdwy	3.519	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	250	509	-	-	986	-
Stage 1	555	-	-	-	-	-
Stage 2	628	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	248	509	-	-	986	-
Mov Cap-2 Maneuver	248	-	-	-	-	-
Stage 1	555	-	-	-	-	-
Stage 2	623	-	-	-	-	-

Approach	NW	NE	SW
HCM Control Delay, s/v12.09		0	0.12
HCM LOS	B		

Minor Lane/Major Mvmt	NET	NER	NWLn1	SWL	SWT
Capacity (veh/h)	-	-	509	23	-
HCM Lane V/C Ratio	-	-	0.002	0.006	-
HCM Control Delay (s/veh)	-	-	12.1	8.7	0.1
HCM Lane LOS	-	-	B	A	A
HCM 95th %tile Q(veh)	-	-	0	0	-

HCM 7th TWSC
 12: East Site Access & Central Street

04/16/2025

Intersection						
Int Delay, s/veh	6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑			↓		↑
Traffic Vol, veh/h	9	0	42	20	0	49
Future Vol, veh/h	9	0	42	20	0	49
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	10	0	46	22	0	53

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	-	10	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	4.12	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	2.218	-	-
Pot Cap-1 Maneuver	-	0	1610	-	0
Stage 1	-	0	-	-	0
Stage 2	-	0	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1610	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	NB
HCM Control Delay, s/v	0	4.95	8.54
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT
Capacity (veh/h)	1072	-	1219	-
HCM Lane V/C Ratio	0.05	-	0.028	-
HCM Control Delay (s/veh)	8.5	-	7.3	0
HCM Lane LOS	A	-	A	A
HCM 95th %tile Q(veh)	0.2	-	0.1	-

HCM 7th TWSC
 14: Gross Point Road & Access Drive

04/16/2025

Intersection						
Int Delay, s/veh	0.1					
Movement	SEL	SER	NEL	NET	SWT	SWR
Lane Configurations		↗		↑	↕	
Traffic Vol, veh/h	0	16	0	540	747	16
Future Vol, veh/h	0	16	0	540	747	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	0	0	0	2	2	0
Mvmt Flow	0	17	0	587	812	17

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	-	415	-	0	-	0
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-
Critical Hdwy	-	6.9	-	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-
Follow-up Hdwy	-	3.3	-	-	-	-
Pot Cap-1 Maneuver	0	592	0	-	-	-
Stage 1	0	-	0	-	-	-
Stage 2	0	-	0	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	-	592	-	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-

Approach	SE	NE	SW
HCM Control Delay, s/v	11.26	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NET SELn1	SWT	SWR
Capacity (veh/h)	- 592	-	-
HCM Lane V/C Ratio	- 0.029	-	-
HCM Control Delay (s/veh)	- 11.3	-	-
HCM Lane LOS	- B	-	-
HCM 95th %tile Q(veh)	- 0.1	-	-

HCM 7th TWSC
 16: West Site Access & Central Street

04/16/2025

Intersection						
Int Delay, s/veh	0.5					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↔			↔	↔	
Traffic Vol, veh/h	8	0	1	19	0	1
Future Vol, veh/h	8	0	1	19	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	0	1	21	0	1

Major/Minor	Major1	Major2	Minor1	Minor2	Minor3
Conflicting Flow All	0	0	9	0	32
Stage 1	-	-	-	-	9
Stage 2	-	-	-	-	23
Critical Hdwy	-	-	4.12	-	6.42
Critical Hdwy Stg 1	-	-	-	-	5.42
Critical Hdwy Stg 2	-	-	-	-	5.42
Follow-up Hdwy	-	-	2.218	-	3.518
Pot Cap-1 Maneuver	-	-	1611	-	982
Stage 1	-	-	-	-	1014
Stage 2	-	-	-	-	1000
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1611	-	982
Mov Cap-2 Maneuver	-	-	-	-	982
Stage 1	-	-	-	-	1014
Stage 2	-	-	-	-	999

Approach	EB	WB	NB
HCM Control Delay, s/v	0	0.36	8.36
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1073	-	-	90	-
HCM Lane V/C Ratio	0.001	-	-	0.001	-
HCM Control Delay (s/veh)	8.4	-	-	7.2	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0	-	-	0	-

19. IDOT Review Letter



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

PERMITS

Location: Gross Point Road at Central Street (SWC)

Municipality: City of Evanston, Cook County

Re: Proposed Kensington School

Reference No.: 016-108623

May 27, 2022

Mr. Michael Werthmann, P.E. VIA EMAIL
KLOA, Inc.
9575 W. Higgins Road
Suite 400
Rosemont, IL 60018

Dear Mr. Werthmann:

We have completed our review of your Traffic Impact Study for the subject location. Our comments are detailed below:

Traffic Permit Comments:

1. Include a Pedestrian Study within the Traffic Impact Study. It is likely, minimally during warmer months, parents may walk to drop off and pick up children. Additionally, evaluate the existing pedestrian equipment and crosswalks at nearby intersections and provide any recommendations for upgrades as a part of this development.
2. Include all recorded Plats of Subdivision and/or Consolidation (Williamsburg, Unity, etc.) referenced within the Alta/NSPS Survey. Please note, access requirements may change based upon our review of these documents.

Traffic Programs – Studies Comments:

1. The Bureau of Traffic Operations (BOT) does not concur with the proposed full access driveway along Gross Point Road. With the existing 4-lane cross section on Gross Point Road and existing triangle configuration of the Gross Point Road/Central Street/Crawford Avenue roadways, interactions with queuing from the Central Street traffic signal and traffic platoons present safety and operational concerns for motorists. Instead, a Right-In/Right-Out (RI-RO) can be permitted.
2. It is recommended that any full access be placed along Central Street. If considered, reassign outbound left-turn movements from the development destined for northeast-bound Gross Point Road to Central Street. Reassign inbound left-turn movements from northeast-bound Gross Point Road to the Central Street intersection and full access.

Please revise your Traffic Impact Study in accordance with the above comments and resubmit to Jonathan.Karabowicz@illinois.gov, along with a written disposition to all comments, to continue the review process.

Location: Gross Point Road at Central Street

May 27, 2022

Page 2

If you have any questions regarding this matter, please contact Jonathan Karabowicz at (847) 705-4149.

Very truly yours,

Jose Rios, P.E.
Region One Engineer

By: *Kalpana Kannan-Hosadurga* ^{AR}

Kalpana Kannan-Hosadurga, P.E., PTOE
Arterial Traffic Operations Engineer

Cc: Sat Nagar – City of Evanston (VIA EMAIL)

20. Market Feasibility Statement



Kensington School of Evanston
Market Analysis & Site Efficacy Study
February 8, 2022

**Kensington School
Administration Office**
743 McClintock Drive
Burr Ridge, IL

Kensington School
LaGrange, IL

**Kensington School
Of Hinsdale**
Hinsdale, IL

**Kensington School
Lincoln Park**
Chicago, IL

**Kensington School
of Glenview**
Glenview, IL

**Kensington School
of the Highlands**
LaGrange, IL

**Kensington School
of Western Springs**
Western Springs, IL

**Kensington School
of St. Charles**
St. Charles, IL

**Kensington School
of Geneva**
Geneva, IL

**Kensington School
of Naperville**
Naperville, IL

**Kensington School
of Wheaton**
Wheaton, IL

**Kensington School
of South Naperville**
Naperville, IL

**Kensington School
of Elmhurst**
Elmhurst, IL

**Kensington School
Arlington Heights**
Arlington Heights, IL

**Kensington School
Of Evanston**
Evanston, IL

Kensington School proposes to develop a new child care center and preschool at 3434 Central Street. As Kensington School has been providing early childhood education to the Chicago area for over 53 years, it has a strong understanding of what demographic, real estate and market landscape conditions are prerequisites for project realization.

Evanston is a city in Cook County, Illinois. It is approximately 12 miles north of the City of Chicago with an approximate population of 75,000. For the purposes of this analysis, one, two- and three-mile radii will be targeted as the optimal trade area for Kensington School although in various communities which it operates, some students come from as far as five miles.

As shown on the enclosed table, one, two and three-mile 2020 population of 0-4 years of age are 857, 3403 and 7,270 respectively which meet or exceed comparable metrics at Kensington School's current locations. Income average of \$172,279 at one mile and \$136,524 at three miles also meet or exceed existing locations.

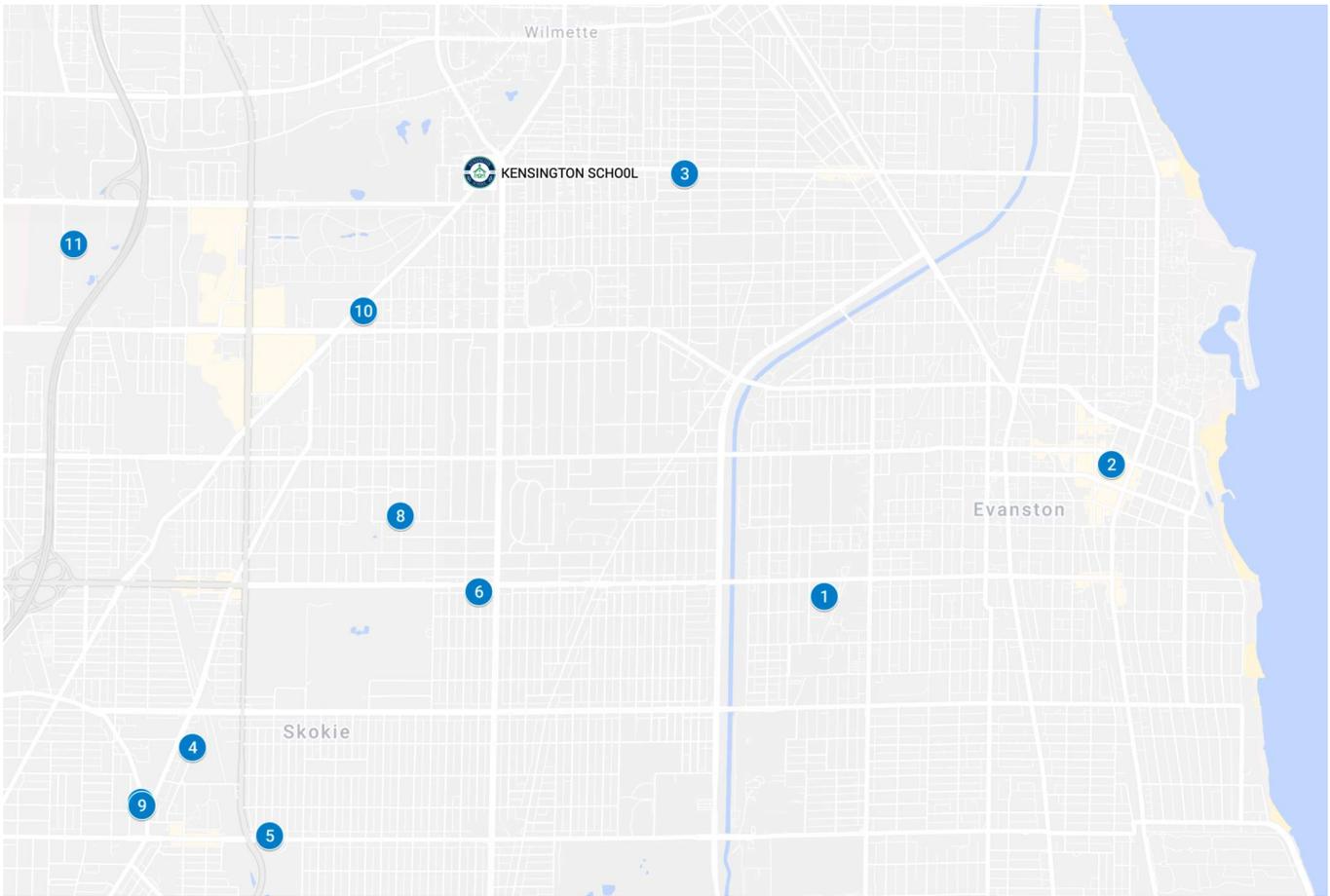
The site selected is located at 3434 Central Street. Currently, the building operating as a Unity Church, resides on the property neighboring existing R1 single family homes. It is placed on a rather well trafficked corner at Gross Point Rd. and Central Street.

The national market landscape for Child Care is a highly fragmented one with many providers having solely one location, many of which operate out of homes and churches. For the purposes of this study, Child Care Centers operating out of entities that are for profit and therefore contribute to local tax rolls are included. Home, church and non-for-profit Centers have been omitted.

As shown in the Market Study Map, there are currently only four for-profit Child Care Centers operating in Evanston. All are within the three-mile trade area of Kensington Schools proposed location. Also, within this three-mile area are additional child care centers in Skokie which are also included in this study. The total number of day spaces combined in this market area is only 1323 children. Ages served range from six weeks to as old as 12 years old for some centers that offer before and after school programs to elementary school aged children. Kensington School offers programming to only infants through five years old. With 7,270 children under four years old in this three-mile area, only 1323 available day spaces available and almost all of these spaces utilized, there is clearly a substantial need for quality center-based child care in Evanston. Approximately 81% of children in this area do not have a child care option available should they need it.

It's still too early to tell what impact the pandemic had on the child care market in Evanston, what has become clear is how vital child care is to working families whether they work from home or are working in person. While many child care center locations across the state have struggled for one reason or another, all twelve Kensington School locations are enrolled to near licensed capacity. This superlative performance in these neighboring markets is a strong indication as to how resilient its program is and how well received Kensington School will be in Evanston.

**EVANSTON AREA CHILD CARE CENTERS
3-MILE RADIUS OF KENSINGTON SCHOOL OF EVANSTON**



School Name	Street	City	Zip	Age Range	Capacity
1 KINDERCARE LEARNING CENTER	2014 DEMPSTER ST	EVANSTON	60202	6W TO 12Y	130
2 BRIGHT HORIZONS AT EVANSTON	1629 ORRINGTON AVE	EVANSTON	60201	6W TO 12Y	307
3 LITTLE GREEN TREE HOUSE	2814 CENTRAL ST	EVANSTON	60201	6W TO 6Y	151
4 SWIFT CHILD CARE	8260 ELMWOOD AVE	SKOKIE	60077	6W TO 12Y	147
5 MOSAIC MONTESSORI	4700 OAKTON ST	SKOKIE	60076	6W TO 6Y	46
6 MOSAIC EARLY CHILDHOOD INFANT CENTER	4035 DEMPSTER ST	SKOKIE	60076	6W TO 24M	12
7 SWIFT CHILD CARE	8106 LINCOLN AVE	SKOKIE	60077	6W TO 35M	26
8 CRADLES CRIBS & CRAYONS	9000 KILDARE AVENUE	SKOKIE	60076	6W TO 6Y	75
9 SWIFT CHILD CARE	8064 LINCOLN AVE	SKOKIE	60077	6W TO 6Y	79
10 THE GODDARD SCHOOL	9651 GROSS POINT RD	SKOKIE	60076	6W TO 6Y	188
11 BRIGHT HORIZONS AT SKOKIE	9877 WOODS DR	SKOKIE	60077	6W TO 6Y	162

Total Capacity in 3 Miles 1323

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Building Type: **General Retail**
 Secondary: -
 GLA: **60,000 SF**
 Year Built: **1950**

Total Available: **0 SF**
 % Leased: **100%**
 Rent/SF/Yr: -



Radius	1 Mile	2 Mile	3 Mile
Population			
2026 Projection	17,661	70,580	146,801
2021 Estimate	17,978	71,787	149,107
2010 Census	18,655	74,171	153,016
Growth 2021 - 2026	-1.76%	-1.68%	-1.55%
Growth 2010 - 2021	-3.63%	-3.21%	-2.55%
2021 Population by Age			
	17,978	71,787	149,107
Age 0 - 4	743 4.13%	3,194 4.45%	7,623 5.11%
Age 5 - 9	940 5.23%	3,775 5.26%	7,821 5.25%
Age 10 - 14	1,185 6.59%	4,687 6.53%	8,802 5.90%
Age 15 - 19	1,227 6.83%	4,990 6.95%	9,623 6.45%
Age 20 - 24	1,097 6.10%	4,686 6.53%	10,576 7.09%
Age 25 - 29	1,030 5.73%	4,682 6.52%	12,409 8.32%
Age 30 - 34	737 4.10%	3,511 4.89%	9,650 6.47%
Age 35 - 39	712 3.96%	3,177 4.43%	7,819 5.24%
Age 40 - 44	856 4.76%	3,497 4.87%	7,424 4.98%
Age 45 - 49	1,103 6.14%	4,360 6.07%	8,534 5.72%
Age 50 - 54	1,247 6.94%	4,922 6.86%	9,399 6.30%
Age 55 - 59	1,432 7.97%	5,583 7.78%	10,602 7.11%
Age 60 - 64	1,468 8.17%	5,607 7.81%	10,611 7.12%
Age 65 - 69	1,245 6.93%	4,682 6.52%	8,837 5.93%
Age 70 - 74	960 5.34%	3,626 5.05%	6,848 4.59%
Age 75 - 79	656 3.65%	2,526 3.52%	4,766 3.20%
Age 80 - 84	482 2.68%	1,820 2.54%	3,406 2.28%
Age 85+	859 4.78%	2,460 3.43%	4,358 2.92%
Age 65+	4,202 23.37%	15,114 21.05%	28,215 18.92%
Median Age	47.10	44.60	40.20
Average Age	43.80	42.40	40.80

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Radius	1 Mile	2 Mile	3 Mile
2021 Population By Race	17,978	71,787	149,107
White	14,402 80.11%	52,475 73.10%	105,377 70.67%
Black	394 2.19%	4,991 6.95%	10,451 7.01%
Am. Indian & Alaskan	27 0.15%	196 0.27%	448 0.30%
Asian	2,725 15.16%	12,121 16.88%	28,465 19.09%
Hawaiian & Pacific Island	5 0.03%	27 0.04%	46 0.03%
Other	425 2.36%	1,976 2.75%	4,320 2.90%
Population by Hispanic Origin	17,978	71,787	149,107
Non-Hispanic Origin	17,060 94.89%	65,965 91.89%	135,817 91.09%
Hispanic Origin	918 5.11%	5,821 8.11%	13,290 8.91%
2021 Median Age, Male	44.60	41.70	37.80
2021 Average Age, Male	42.00	40.80	39.40
2021 Median Age, Female	49.20	46.70	42.50
2021 Average Age, Female	45.50	43.80	42.10
2021 Population by Occupation Classification	14,861	59,132	122,938
Civilian Employed	8,568 57.65%	33,905 57.34%	68,280 55.54%
Civilian Unemployed	448 3.01%	2,242 3.79%	4,493 3.65%
Civilian Non-Labor Force	5,845 39.33%	22,985 38.87%	50,147 40.79%
Armed Forces	0 0.00%	0 0.00%	18 0.01%
Households by Marital Status			
Married	4,071	15,627	29,623
Married No Children	2,110	8,196	15,920
Married w/Children	1,961	7,431	13,703
2021 Population by Education	13,292	52,599	109,287
Some High School, No Diploma	397 2.99%	2,665 5.07%	6,023 5.51%
High School Grad (Incl Equivalency)	1,176 8.85%	6,255 11.89%	12,681 11.60%
Some College, No Degree	1,787 13.44%	8,041 15.29%	16,971 15.53%
Associate Degree	506 3.81%	2,144 4.08%	4,625 4.23%
Bachelor Degree	4,220 31.75%	15,776 29.99%	33,420 30.58%
Advanced Degree	5,206 39.17%	17,718 33.69%	35,567 32.54%

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Radius	1 Mile		2 Mile		3 Mile	
2021 Population by Occupation	17,909		70,427		140,522	
Real Estate & Finance	961	5.37%	3,564	5.06%	6,929	4.93%
Professional & Management	7,682	42.89%	26,648	37.84%	51,267	36.48%
Public Administration	146	0.82%	756	1.07%	1,277	0.91%
Education & Health	2,409	13.45%	9,918	14.08%	21,018	14.96%
Services	787	4.39%	3,906	5.55%	8,086	5.75%
Information	205	1.14%	645	0.92%	1,415	1.01%
Sales	1,428	7.97%	6,483	9.21%	13,164	9.37%
Transportation	1,498	8.36%	5,710	8.11%	10,606	7.55%
Retail	692	3.86%	3,195	4.54%	6,020	4.28%
Wholesale	251	1.40%	743	1.05%	1,746	1.24%
Manufacturing	616	3.44%	2,480	3.52%	5,057	3.60%
Production	377	2.11%	2,266	3.22%	5,042	3.59%
Construction	169	0.94%	881	1.25%	2,101	1.50%
Utilities	272	1.52%	1,405	1.99%	2,946	2.10%
Agriculture & Mining	1	0.01%	26	0.04%	81	0.06%
Farming, Fishing, Forestry	1	0.01%	12	0.02%	21	0.01%
Other Services	414	2.31%	1,789	2.54%	3,746	2.67%
2021 Worker Travel Time to Job	7,545		30,793		62,767	
<30 Minutes	3,468	45.96%	14,724	47.82%	32,079	51.11%
30-60 Minutes	2,810	37.24%	11,506	37.37%	22,465	35.79%
60+ Minutes	1,267	16.79%	4,563	14.82%	8,223	13.10%
2010 Households by HH Size	6,997		27,214		56,205	
1-Person Households	1,832	26.18%	6,560	24.11%	15,325	27.27%
2-Person Households	2,182	31.18%	8,471	31.13%	17,315	30.81%
3-Person Households	1,109	15.85%	4,340	15.95%	8,713	15.50%
4-Person Households	1,162	16.61%	4,504	16.55%	8,460	15.05%
5-Person Households	505	7.22%	2,213	8.13%	4,139	7.36%
6-Person Households	149	2.13%	720	2.65%	1,431	2.55%
7 or more Person Households	58	0.83%	406	1.49%	822	1.46%
2021 Average Household Size	2.60		2.70		2.60	
Households						
2026 Projection	6,597		25,781		53,813	
2021 Estimate	6,719		26,249		54,691	
2010 Census	6,997		27,215		56,204	
Growth 2021 - 2026	-1.82%		-1.78%		-1.61%	
Growth 2010 - 2021	-3.97%		-3.55%		-2.69%	

Demographic Detail Report

3434 Central St, Evanston, IL 60201

Radius	1 Mile	2 Mile	3 Mile
2021 Households by HH Income	6,720	26,249	54,689
<\$25,000	544 8.10%	2,332 8.88%	7,073 12.93%
\$25,000 - \$50,000	729 10.85%	3,422 13.04%	7,593 13.88%
\$50,000 - \$75,000	824 12.26%	3,355 12.78%	6,821 12.47%
\$75,000 - \$100,000	550 8.18%	2,507 9.55%	4,971 9.09%
\$100,000 - \$125,000	550 8.18%	2,442 9.30%	5,390 9.86%
\$125,000 - \$150,000	543 8.08%	2,120 8.08%	4,084 7.47%
\$150,000 - \$200,000	840 12.50%	2,703 10.30%	5,351 9.78%
\$200,000+	2,140 31.85%	7,368 28.07%	13,406 24.51%
2021 Avg Household Income	\$160,740	\$148,872	\$136,963
2021 Med Household Income	\$132,504	\$115,443	\$104,112
2021 Occupied Housing	6,719	26,249	54,691
Owner Occupied	5,442 80.99%	20,193 76.93%	38,584 70.55%
Renter Occupied	1,277 19.01%	6,056 23.07%	16,107 29.45%
2010 Housing Units	7,375	29,032	60,646
1 Unit	5,674 76.94%	20,086 69.19%	36,451 60.10%
2 - 4 Units	178 2.41%	2,498 8.60%	6,293 10.38%
5 - 19 Units	557 7.55%	2,104 7.25%	5,573 9.19%
20+ Units	966 13.10%	4,344 14.96%	12,329 20.33%
2021 Housing Value	5,443	20,193	38,585
<\$100,000	46 0.85%	170 0.84%	564 1.46%
\$100,000 - \$200,000	315 5.79%	1,315 6.51%	2,668 6.91%
\$200,000 - \$300,000	343 6.30%	2,371 11.74%	5,914 15.33%
\$300,000 - \$400,000	838 15.40%	3,519 17.43%	7,783 20.17%
\$400,000 - \$500,000	716 13.15%	2,671 13.23%	4,508 11.68%
\$500,000 - \$1,000,000	2,404 44.17%	7,003 34.68%	11,324 29.35%
\$1,000,000+	781 14.35%	3,144 15.57%	5,824 15.09%
2021 Median Home Value	\$596,402	\$503,606	\$452,428
2021 Housing Units by Yr Built	7,375	29,049	60,746
Built 2010+	41 0.56%	345 1.19%	1,225 2.02%
Built 2000 - 2010	134 1.82%	1,438 4.95%	3,685 6.07%
Built 1990 - 1999	265 3.59%	890 3.06%	2,049 3.37%
Built 1980 - 1989	260 3.53%	693 2.39%	1,468 2.42%
Built 1970 - 1979	527 7.15%	2,351 8.09%	4,714 7.76%
Built 1960 - 1969	1,403 19.02%	4,891 16.84%	9,366 15.42%
Built 1950 - 1959	2,051 27.81%	8,223 28.31%	16,226 26.71%
Built <1949	2,694 36.53%	10,218 35.18%	22,013 36.24%
2021 Median Year Built	1954	1954	1954

Demographic Summary Report

3434 Central St, Evanston, IL 60201

Building Type: **General Retail** Total Available: **0 SF**
 Secondary: - % Leased: **100%**
 GLA: **60,000 SF** Rent/SF/Yr: -
 Year Built: **1950**



Radius	1 Mile	2 Mile	3 Mile
Population			
2026 Projection	17,661	70,580	146,801
2021 Estimate	17,978	71,787	149,107
2010 Census	18,655	74,171	153,016
Growth 2021 - 2026	-1.76%	-1.68%	-1.55%
Growth 2010 - 2021	-3.63%	-3.21%	-2.55%
2021 Population by Hispanic Origin	919	5,821	13,288
2021 Population	17,978	71,787	149,107
White	14,402 80.11%	52,475 73.10%	105,377 70.67%
Black	394 2.19%	4,991 6.95%	10,451 7.01%
Am. Indian & Alaskan	27 0.15%	196 0.27%	448 0.30%
Asian	2,725 15.16%	12,121 16.88%	28,465 19.09%
Hawaiian & Pacific Island	5 0.03%	27 0.04%	46 0.03%
Other	425 2.36%	1,976 2.75%	4,320 2.90%
U.S. Armed Forces	0	0	17
Households			
2026 Projection	6,597	25,781	53,813
2021 Estimate	6,719	26,249	54,691
2010 Census	6,997	27,215	56,204
Growth 2021 - 2026	-1.82%	-1.78%	-1.61%
Growth 2010 - 2021	-3.97%	-3.55%	-2.69%
Owner Occupied	5,442 80.99%	20,193 76.93%	38,584 70.55%
Renter Occupied	1,277 19.01%	6,056 23.07%	16,107 29.45%
2021 Households by HH Income	6,720	26,249	54,689
Income: <\$25,000	544 8.10%	2,332 8.88%	7,073 12.93%
Income: \$25,000 - \$50,000	729 10.85%	3,422 13.04%	7,593 13.88%
Income: \$50,000 - \$75,000	824 12.26%	3,355 12.78%	6,821 12.47%
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Income: \$200,000+	2,140 31.85%	7,368 28.07%	13,406 24.51%
2021 Avg Household Income	\$160,740	\$148,872	\$136,963
2021 Med Household Income	\$132,504	\$115,443	\$104,112

21. City Staff Review Comments and Applicant Responses



City of Evanston
Planning Division
2100 Ridge Ave.
Evanston, IL, 60201-2798
T 847.448.4311
www.cityofevanston.org

June 9, 2025

Charles Marlas
Kensington School
743 McClintock Dr
Burr Ridge, IL 60527

RE: Planned Development Application – 25PLND-0021, Round 1 PRO Review Comments
3434 Central Street, Kensington School

Dear Mr. Marlas,

Staff has reviewed your Planned Development submittal for the Kensington School daycare proposed at 3434 Central Street. Before finalizing a hearing date with the Land Use Commission, please see the comments below and provide a point-by-point response that addresses each comment, along with revised plans/documents (as applicable):

Based upon review of the application, the following zoning approvals are required:

- A Special Use Permit for a Planned Development to allow a 2-story, 19,952 square foot Child Daycare Center (Section 6-8-1-10(D) and 6-8-3-3).
- Site Development Allowance to vary the requirements of Section 6-8-1-10(B) to allow a 5' wide landscape strip along the entire western property line and a 0' wide landscape strip along a portion of the southern property line, where Code requires a minimum 10' wide landscape strip.
- Site Development Allowance to vary the requirements of Section 6-8-3-9 to allow a 63.45% impervious surface lot coverage where impervious surface lot coverage is restricted to a maximum of 55%.
- Site Development Allowance to vary the requirements of Sections 6-8-3-7(B)1, 6-8-3-7(B)2, and 6-16-2-1(C) to allow parking within a front yard setback and a side yard setback where parking is prohibited in front and side yard setbacks
- Site Development Allowance to vary the requirements of Section 6-4-6-3(A)2 to allow an accessory use (southern patio) in a side yard and an accessory use (refuse enclosure) in a front yard where accessory uses are prohibited from side yards and front yards in residential districts.
- Site Development Allowance to vary the requirements of Section 6-4-6-7(F)3.a to allow a 6' tall dumpster enclosure fence and a 6' tall playground fence within the front yard where fence heights in a front yard are restricted to 4' tall.
- Site Development Allowance to vary the requirements of Section 6-4-6-3(B), Table 4-A, to allow open off-street parking in a residential district that is greater than 30' setback from a rear property

line where open off-street parking in a residential district shall be setback no greater than 30' from a rear property line.

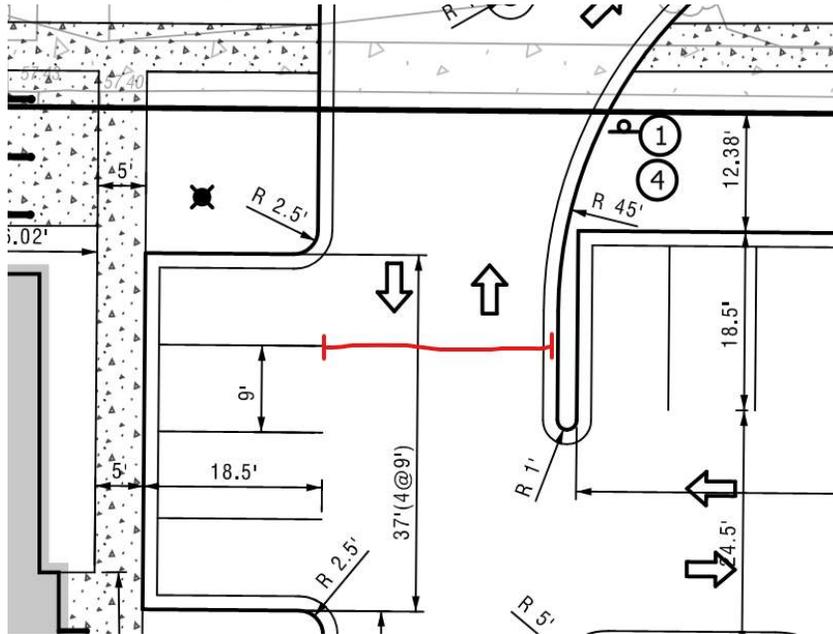
Planning & Zoning - General

1. Please provide a full copy of the Summary Judgement that includes the exhibits.
2. Was the relocation of utilities addressed during Circuit Court Case #2023-CH-06757? Does Kensington School have the authority to reroute the sewer line through the Co-op property and the water line onto the Kensington Property?
3. Who will be responsible for maintenance and snow removal of the Co-op drive aisle on the subject property?
4. The proposed access easement on Sheet A1-0 appears to encroach outside of the paved area, resulting in portions of the access easement that are not accessible by a vehicle. Please clarify.
5. Assuming the "Litigation" as referenced in the Amendment to the Purchase and Sale agreement ended on January 21, 2025, the 120-day "Zoning Approval Period" ended in May. Was a 60-day extension requested by the Buyer and approved by the Seller?
6. Please confirm that the total number of employees will be 23, the total child capacity will be 165, and whether there will be any vehicles stored on the site (such as a bus or van for staff /child transport)
7. It is recommended that an enhanced PD public benefits package be proposed. Consider green/sustainable design features, such as geothermal heating/cooling, a rooftop solar system, a green roof, or EV charging parking stalls.
8. Please clarify the specific PD public benefits that are proposed, which benefits go above and beyond minimum code requirements.
9. Per Section 6-3-6-7(D), a community meeting may be required. Please reach out to Councilmember Sufferdin to determine if a community meeting will be required and keep staff aware of any required community meeting.
10. Please ensure all plans, studies, and documents that are revised as a result of these comments include a revision date.

Planning & Zoning - Site Related

11. Consider the inclusion of permeable pavers to decrease the extent of the impervious surface lot coverage Site Development Allowance, which may also decrease the size and cost of the required stormwater vault.
12. The existing fence at the southwest corner of the property appears to belong to 3508 Central Street and encroaches onto the Kensington School property and will be removed. Have you coordinated with the neighboring property owner and will Kensington School be relocating/replacing this portion of the fence on the 3508 Central property?
13. There are at least 2 existing trees on the along the western property located on the Kensington School property that are not proposed for removal on the Tree Preservation plan but are not shown as to remain with preservation protection. Please include these trees in the Tree Inventory and verify that the proposed plantings along western property line are viable given these existing tree locations. Adjust the landscape plan as necessary.
14. One of the bike racks is proposed within the ROW. Staff is verifying with the Public Work Administration whether this will be allowed.
15. Confirm that the proposed parking lot lights will not exceed a maximum of 3000K CCT and will not include a glass lens (i.e. they will be Dark Sky Compliant).

16. The new public sidewalk shall be extended to the edge of the western property line and shall not terminate within the drive aisle.
17. Will playground equipment include any chimes or noise making features? If so, please provide a picture or product spec sheet.
18. Will a shed be proposed? If so, please show this on the plans and provide elevations.
19. Confirm the driveway width in the location as shown below, which shall be measured from face of curb to rear of parking stall:



Planning & Zoning – Building Related

20. Will the elevator overrun on the roof be taller than the sloped parapet walls? If so, please dimension the height and show the elevator overrun on the elevations.
21. Will any building-mounted equipment/meters/panels (larger than 1 square foot) be proposed? If so, please indicate these features on the elevations. Any such infrastructure shall be appropriately sited and screened.
22. Please be aware that the City of Evanston has adopted the Illinois Stretch Energy Code. The Stretch Code will be in effect for projects submitting building permit applications on or after October 1, 2025.

Public Works – Engineering:

23. Per IDOT's comments for a pedestrian study, what pedestrian equipment and crosswalk improvements are being planned with this development?

Public Works/Waste

24. Please confirm if there will be recycling and composting offered at the proposed daycare.

Fire:

25. Adopted codes being used are: 2021 - IFC, IBC, NFPA 101. 2019 - NFPA 13 and NFPA 72
26. Ensure OFSM requirements are met.
27. Knox Box (key box) requirement must be met.
28. Access roads for fire department vehicles during and after construction must be properly sized - 20' wide.

29. The building is required to be fully sprinklered.
30. The building is required to have a fire monitoring and notification system.
31. Fire safety evacuation and lock down plans must be developed and presented to the Fire Department for review and approval.

Sustainability:

32. Please be aware that compliance with the [Bird Friendly Ordinance](#) and the [Green Building Ordinance](#) shall be required.
33. It is recommended that the existing building be deconstructed and recycled/salvaged as opposed to demolished and disposed.
34. It is recommended that the proposed building be designed with all-electric HVAC and water heating equipment, along with the inclusion of high-efficiency appliances.
35. You're encouraged to incorporate the use of recycled and [low embodied carbon building materials](#) in construction of the proposed daycare building.

Health Department:

36. A rodent control plan during construction and immediately after shall be required. The plan must be provided and operated by a licensed rodent control company.
37. A waste management plan shall be required at time of building permit application.
38. A plan regarding debris and dust minimization during construction shall be required as part of any building permit application.
39. Environmental assessment shall be required prior to construction to ascertain the level of environmental hazards or contaminants such as lead, arsenic, PCB's etc., and shall establish a means to mitigate them if determined present.

As outlined above, to continue working towards a tentative date with the Land Use Commission, please provide a point-by-point response to the above review comments, along with revised plans/documents (as applicable) to address the comments contained within this letter. Should you have any questions or concerns about the above comments, please do not hesitate to contact me directly at shubbard@cityofevanston.org.

Sincerely,



Sam Hubbard
Interim Zoning Administrator/Senior Planner

Cc: Jeremiah Bebo, Development Services Supervisor
Elizabeth Williams, Planning Manager
Sarah Flax, Director of Community Development

October 17, 2025

From: Charles Marlas
Kensington School
743 McClintock Dr.
Burr Ridge, IL 60527

To: Sam Hubbard, City of Evanston Zoning
RE: Zoning Analysis Application – 25PLND-0021, 3434 Central St., Kensington School
Round 1 PRO Applicant Comments, **Responses in Bold**

Dear Mr. Hubbard:

Responses to the Round 1 Review Letter are below in **bold**:

- A Special Use Permit for a Planned Development to allow a 2-story, 19,952 square foot Child Daycare Center (Section 6-8-1-10(D) and 6-8-3-3) with the following Site Development Allowances:
 - To vary the requirements of Section 6-8-1-10(B) to allow a 5' wide landscape strip along the entire western property line and a 0' wide landscape strip along a portion of the southern property line, where Code requires a minimum 10' wide landscape strip.
Response: Reduced landscape strips are necessary due to the land used to provide private access for the Co-op property.
 - To vary the requirements of Section 6-8-3-9 to allow a 63.45% impervious surface lot coverage where impervious surface lot coverage is restricted to a maximum of 55%.
Response: The existing impervious surface lot coverage for Unity Church is 69% where 55% is allowed. Kensington School proposes to reduce that to 62% by utilizing less hardscape and incorporating brick pavers where feasible. Furthermore, underground stormwater detention is provided in compliance with MWRD requirements to restrict the stormwater flow to the storm sewer connection. However, the underground stormwater vault will feature a permeable bottom so that held stormwater can further infiltrate into the ground. When considering the 4,350 sq. ft. underground vault's permeable base, the overall impervious surface lot coverage reduces to 55% (fully compliant). Furthermore, if the property is reviewed without the western portion of the lot where the private driveway for the Co-op exists, the remaining Kensington area's impervious surface lot coverage is 58% at grade, and reduces further to 50% (fully compliant) when adding in the permeable underground vault.

- To vary the requirements of Sections 6-8-3-7(B)1, 6-8-3-7(B)2, and 6-16-2-1(C) to allow parking within a front yard setback and a side yard setback where parking is prohibited in front and side yard setbacks
Response: Open parking is within the front and street side yards rather than the building to maintain extended visibility around the busy traffic intersection.
- To vary the requirements of Section 6-4-6-3(A)2 to allow an accessory use (southern patio) in a side yard and an accessory use (refuse enclosure) in a front yard where accessory uses are prohibited from side yards and front yards in residential districts.
Response: The “southern patio” is a compliant at-grade terrace that is considered part of the principal structure and is permitted when within the building envelope and attached to the building. The “refuse enclosure” is not an accessory use and is not listed as an accessory use in 6-4-6-3, but is a fence as noted below for relief. These items have not been considered noncompliant in the past as noted. However, if they are considered such, then they are requested or the enclosure around the dumpster could be eliminated to achieve compliance.
- To vary the requirements of Section 6-4-6-7(F)3.a to allow a 6’ tall dumpster enclosure fence and a 6’ tall playground fence within the front yard where fence heights in a front yard are restricted to 4’ tall.
Response: Confirmed; fencing is needed for child safety.
- To vary the requirements of Section 6-4-6-3(B), Table 4-A, to allow open off-street parking in a residential district that is greater than 30’ setback from a rear property line where open off-street parking in a residential district shall be setback no greater than 30’ from a rear property line.
Response: Since no alley access is present, it is not feasible to locate all open parking within the rearmost 30’ of the property. Parking within the front yard improves the traffic intersection visibility and reduces potential impact on the adjacent residential properties.

Planning & Zoning - General

1. Please provide a full copy of the Summary Judgement that includes the exhibits.
Response: See attached.
2. Was the relocation of utilities addressed during Circuit Court Case #2023-CH-06757? Does Kensington School have the authority to reroute the sewer line through the Co-op property and the water line onto the Kensington Property?
Response: The sewer and water service for the Co-op will remain as is on the Co-op property. A new water line will be installed for Kensington entirely on the Kensington property with a connection at Central Street. A new sewer line will be installed for Kensington entirely on the Kensington property except at the sewer connection point which is within the Co-op property. The result is that all lines are separated for the benefit of all property owners.

3. Who will be responsible for maintenance and snow removal of the Co-op drive aisle on the subject property?

Response: Since the drive aisle is solely for the use of the Co-op and leads to private Co-op parking on Co-op property, maintenance and snow removal shall be covered by the Co-op.

4. The proposed access easement on Sheet A1-0 appears to encroach outside of the paved area, resulting in portions of the access easement that are not accessible by a vehicle. Please clarify.

Response: The easement area has been updated to align with the curb/paved area.

5. Assuming the "Litigation" as referenced in the Amendment to the Purchase and Sale agreement ended on January 21, 2025, the 120-day "Zoning Approval Period" ended in May. Was a 60-day extension requested by the Buyer and approved by the Seller?

Response: Yes. See attached latest approved extension.

6. Please confirm that the total number of employees will be 23, the total child capacity will be 165, and whether there will be any vehicles stored on the site (such as a bus or van for staff /child transport).

Response: The total maximum number of employees anticipated is 26 for a maximum of 166 children, and no company vehicles or transport vehicles will be stored on the site.

7. It is recommended that an enhanced PD public benefits package be proposed. Consider green/sustainable design features, such as geothermal heating/cooling, a rooftop solar system, a green roof, or EV charging parking stalls.

Response: Sustainability measures are proposed that will achieve LEED Gold equivalent and comply with the IL Stretch Energy Code. We are exploring a variety of public benefits.

8. Please clarify the specific PD public benefits that are proposed, which benefits go above and beyond minimum code requirements.

Response: Public benefits are currently being explored and will be further refined following the public hearing with the Land Use Commission based on suggestions from the neighborhood. Current potential public benefits include:

1. Scholarships for reduced tuition for qualifying Evanston families
 2. Reduced tuition for sustainable drop-off/pick-ups to discourage vehicle usage in the area
 3. \$25,000 contribution for new public sidewalks within the surrounding neighborhood
9. Per Section 6-3-6-7(D), a community meeting may be required. Please reach out to Councilmember Sufferdin to determine if a community meeting will be required and keep staff aware of any required community meeting.

Response: Understood. We are in contact with Councilmember Suffredin. No meeting is planned at this time, but may be in the future.

10. Please ensure all plans, studies, and documents that are revised as a result of these comments include a revision date.

Response: Noted and addressed.

Planning & Zoning - Site Related

11. Consider the inclusion of permeable pavers to decrease the extent of the impervious surface lot coverage Site Development Allowance, which may also decrease the size and cost of the required stormwater vault.

Response: See updated plans - permeable pavers are added into parking areas where possible.

12. The existing fence at the southwest corner of the property appears to belong to 3508 Central Street and encroaches onto the Kensington School property and will be removed. Have you coordinated with the neighboring property owner and will Kensington School be relocating/replacing this portion of the fence on the 3508 Central property?

Response: The fence was temporary in nature and no longer exists.

13. There are at least 2 existing trees on the along the western property located on the Kensington School property that are not proposed for removal on the Tree Preservation plan but are not shown as to remain with preservation protection. Please include these trees in the Tree Inventory and verify that the proposed plantings along western property line are viable given these existing tree locations. Adjust the landscape plan as necessary.

Response: The Tree Preservation plan has been updated to clarify these trees will be removed.

14. One of the bike racks is proposed within the ROW. Staff is verifying with the Public Work Administration whether this will be allowed.

Response: The bike rack will be relocated if necessary.

15. Confirm that the proposed parking lot lights will not exceed a maximum of 3000K CCT and will not include a glass lens (i.e. they will be Dark Sky Compliant).

Response: The proposed parking lot lights will not exceed 3000K CCT and will not include a glass lens.

16. The new public sidewalk shall be extended to the edge of the western property line and shall not terminate within the drive aisle.

Response: This has been revised per the comment.

17. Will playground equipment include any chimes or noise making features? If so, please provide a picture or product spec sheet.

Response: No, noise making features are not proposed on any of the outdoor playground equipment.

18. Will a shed be proposed? If so, please show this on the plans and provide elevations.

Response: No, a shed is not proposed.

19. Confirm the driveway width in the location as shown below, which shall be measured from face of curb to rear of parking stall.

Response: Addressed. The driveway width is 24.5 feet (to back of 6" curb), see below.

via a left-hand turn will be subject to disciplinary action, including potential disenrollment of their child(ren).

Please see the attached community letters and visual diagrams that depict the changes.

Additionally, Traffic Engineer KLOA found the following: pedestrian traffic signals exist on all legs of the intersections of Gross Point/Central, Gross Point/Crawford, and Central/Crawford. With the additional sidewalk as proposed on the Kensington site, complete pedestrian paths are achieved that utilize controlled intersections.

Public Works/Waste

24. Please confirm if there will be recycling and composting offered at the proposed daycare.

Response: Recycling will be offered and composting is being considered.

Fire

25. Adopted codes being used are: 2021 - IFC, IBC, NFPA 101. 2019 - NFPA 13 and NFPA 72 Ensure OFSM requirements are met.

Response: Noted. This will be addressed on construction documents at the time of building permit application.

26. Knox Box (key box) requirement must be met.

Response: Noted. This will be addressed on construction documents at the time of building permit application.

27. Access roads for fire department vehicles during and after construction must be properly sized - 20' wide.

Response: Noted. This will be addressed within the Construction Management Plan.

28. The building is required to be fully sprinklered.

Response: Noted. This will be addressed on construction documents at the time of building permit application.

29. The building is required to have a fire monitoring and notification system.

Response: Noted. This will be addressed on construction documents at the time of building permit application.

30. Fire safety evacuation and lock down plans must be developed and presented to the Fire Department for review and approval.

Response: Noted. This will be addressed on construction documents at the time of building permit application.

Sustainability

31. Please be aware that compliance with the Bird Friendly Ordinance and the Green Building Ordinance shall be required.

Response: Noted.

32. It is recommended that the existing building be deconstructed and recycled/salvaged as opposed to demolished and disposed.

Response: Deconstruction and/or recycling of building materials is being explored.

33. It is recommended that the proposed building be designed with all-electric HVAC and water heating equipment, along with the inclusion of high-efficiency appliances.
Response: The building will meet the sustainability requirements of LEED Gold/3 Green Globes equivalent, the Green Building Ordinance, and will follow the IL Stretch Energy Code, so the building will operate primarily as electric.
34. You're encouraged to incorporate the use of recycled and low embodied carbon building materials in construction of the proposed daycare building.
Response: Such building materials are being considered.

Health Department

35. A rodent control plan during construction and immediately after shall be required. The plan must be provided and operated by a licensed rodent control company.
Response: Noted. This will be addressed within demolition documents and permitting.
36. A waste management plan shall be required at time of building permit application.
Response: Noted. This will be included at the time of building permit application.
37. A plan regarding debris and dust minimization during construction shall be required as part of any building permit application.
Response: Noted. This will be included at the time of building permit application.
38. Environmental assessment shall be required prior to construction to ascertain the level of environmental hazards or contaminants such as lead, arsenic, PCB's etc., and shall establish a means to mitigate them if determined present.
Response: Noted. This will be done and documentation attesting to this information will be provided within demolition documents and permitting.

We look forward to the city's review of this submittal for the subject development and hope to be scheduled for an upcoming Land Use Commission public hearing this fall. Please do not hesitate to contact me directly with any follow up questions in the interim.

Sincerely,



Charles Marlas
Kensington School



City of Evanston
Planning Division
2100 Ridge Ave.
Evanston, IL, 60201-2798
T 847.448.4311
www.cityofevanston.org

November 18, 2025 – **Corrected 11-20-25**

Charles Marlas
Kensington School
743 McClintock Dr
Burr Ridge, IL 60527

RE: Planned Development Application – 25PLND-0021, Round 2 PRO Review Comments
3434 Central Street, Kensington School

Dear Mr. Marlas,

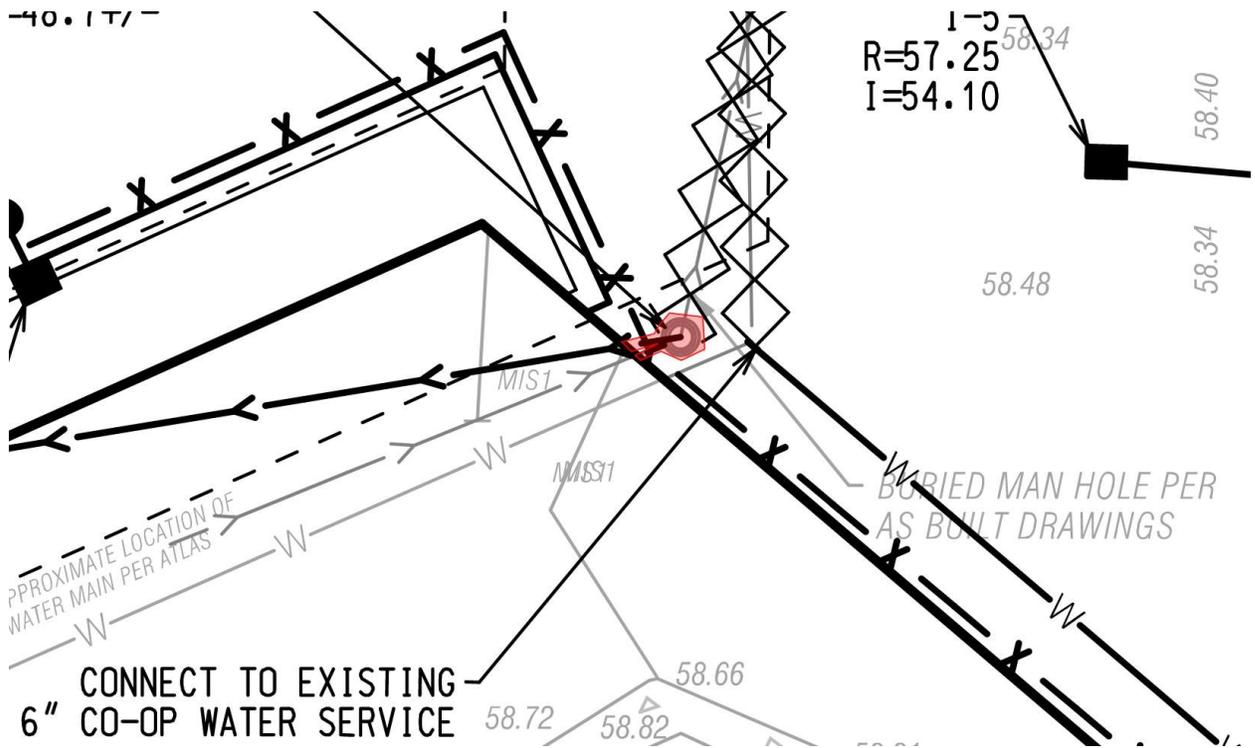
Staff has reviewed your response to the Round 1 PRO review comments, along with the revised submittal materials for the Kensington School daycare proposed at 3434 Central Street. Please see our Round 2 PRO review comments below.

Based upon review of the revised application, the following zoning approvals are required:

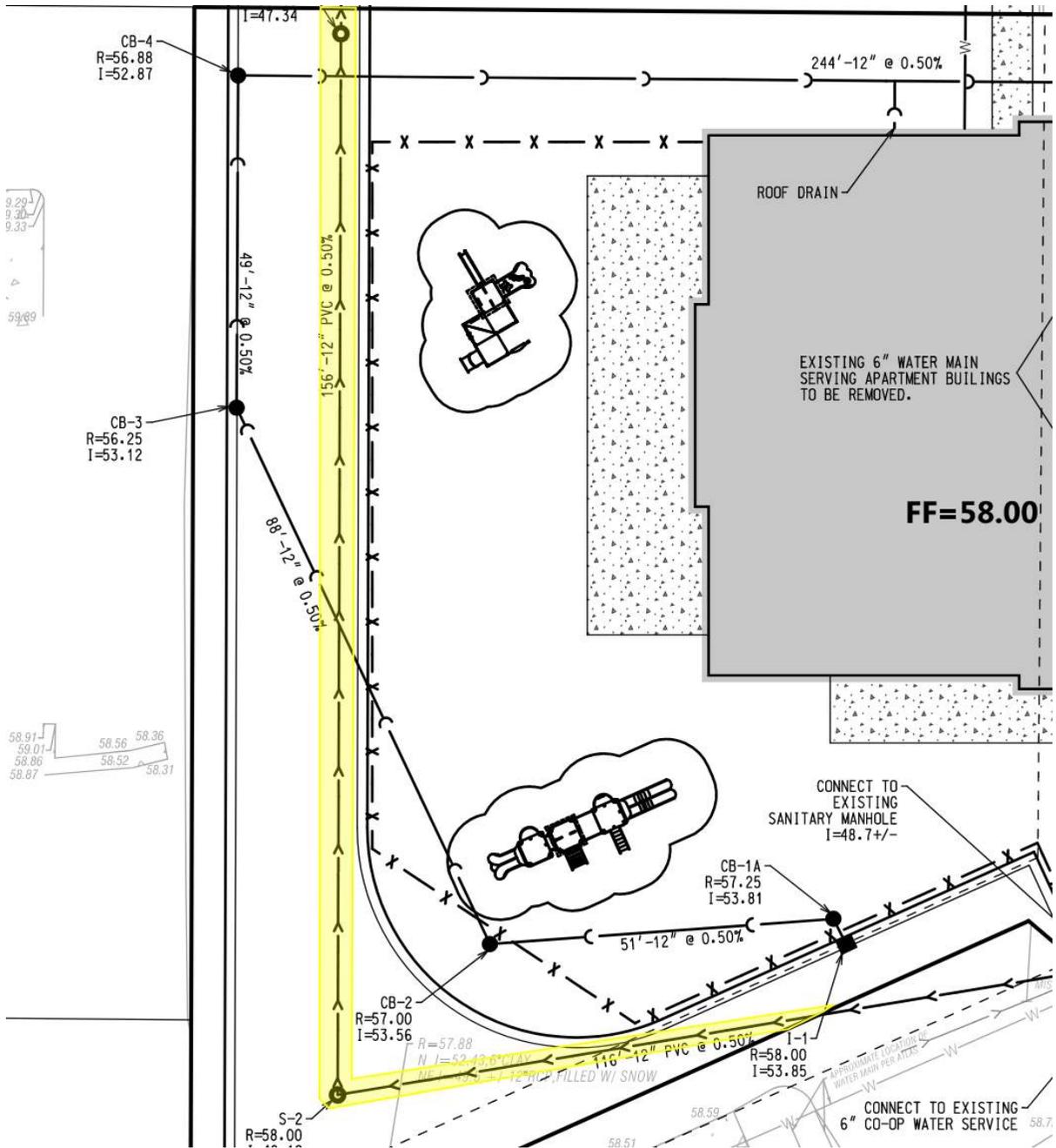
- A Special Use Permit for a Planned Development to allow a 2-story, 19,952 square foot Child Daycare Center (Section 6-8-1-10(D) and 6-8-3-3).
- Site Development Allowance to vary the requirements of Section 6-8-1-10(B) to allow a 5' wide landscape strip along the entire western property line and a 0' wide landscape strip along a portion of the southern property line, where Code requires a minimum 10' wide landscape strip.
- Site Development Allowance to vary the requirements of Section 6-8-3-9 to allow a 62.22% impervious surface lot coverage where impervious surface lot coverage is restricted to a maximum of 55%.
- Site Development Allowance to vary the requirements of Sections 6-8-3-7(B)1, 6-8-3-7(B)2, and 6-16-2-1(C) to allow parking within a front yard setback and a side yard setback where parking is prohibited in front and side yard setbacks.
- Site Development Allowance to vary the requirements of Section 6-4-6-7(F)3.a to allow a 6' tall dumpster enclosure fence and a 6' tall and 4'-5' tall playground fence within the front yard where fence heights in a front yard are restricted to 4' tall.
- Site Development Allowance to vary the requirements of Section 6-4-6-3(B), Table 4-A, to allow open off-street parking in a residential district that is greater than 30' setback from a rear property line where open off-street parking in a residential district shall be setback no greater than 30' from a rear property line.
- Site Development Allowance to vary the requirements of Sections 6-16-2-7(A) to allow a 16.5' wide drive aisle where a minimum drive aisle width of 24' is required (this applies to the drive aisle adjacent to the First Williamsburg Co-op parking stalls).

Planning & Zoning

- 40. Your responses to the required Site Development Allowances are noted. An underground open-bottom stormwater vault does not make a concrete parking lot qualify for the credit allowed to paver blocks. However, we agree that the patio can be considered as attached to the building and part of the principal structure and therefore not subject to the accessory structure requirements. Additionally, we agree that the dumpster enclosure can be considered a fence and not subject to the accessory use standards, although it should be noted that the list of "Permitted accessory buildings, structures and uses" in Table 4-A is not all encompassing and there are structures, uses, and buildings that may qualify as accessory but may not be specifically listed within the table.
- 41. The responses to the following comments are acceptable: #1, #3, #6, #9, #10, #12, #13, #15, #16, #17, #18, #19, #20, #21, and #22.
- 42. The response to comment is noted, however, the existing easements on the site need to be clarified. The plans show a new private sanitary sewer line for the co-op is proposed on the Kensington school site. The area in red below must be within a private utility easement to the benefit of the co-op, or evidence of the existence of an easement in this area must be provided (the Plat of Survey only shows a ComEd easement in this area):



Furthermore, the newly proposed sanitary line serving the co-op, shown in yellow (below) falls within the easement area that is required by the Summary Judgement. Please confirm that the easement required to be recorded per #2 of the Summary Judgement will address not only ingress/egress but also this private sanitary line:



A new City water main will be constructed on the Kensington School property. The Plat of Survey does not show that a public utility easement exists to cover this water main, it only references an easement with ComEd for electrical utilities. Please confirm that a new public easement for the water main will be established.

Finally, the response to comment #2 did not address whether the developer has the rights to make these changes to the private sanitary sewer line serving the co-op. Additionally, is the co-op aware that a new sanitary line will be routed through a portion of their property?

43. The response to comment #4 is noted, however, the revised easement area now no longer aligns with the Summary Judgement, Exhibit 1, which shows the easement as 26' in width. Please clarify.
44. The response to comment #5 is noted, however, the contract extension document appears to have extended the Zoning Approval Period to July 2025. Please clarify.
45. The response to comment #7 is noted. Please be aware that LEED Gold or equivalent is a Code requirement and compliance with the IL Stretch Energy Code is a Code requirement; compliance with Code is not considered a public benefit. You're encouraged to continue developing the public benefits.
46. The response to comment #8 is noted.
47. The response to comment #10 is acceptable. Please continue to include revision dates on any plan revised as a result of the Round 2 review comments.
48. The response to comment #11 is noted. Further incorporation of pavers is recommended to further reduce the extent of the variation requested. Additionally, please be aware that parking stalls are only required to be a minimum of 8.5' in width. The eastern most parking row, with 17 parking stalls at 9' in width, could be modified to 8.5' in width to pick up an extra parking stall or to remove a parking stall and preserve greenspace/lessen the impervious surface variation. Per Code, only 37 parking stalls are required.
49. The response to comment #14 is noted. Please consider flipping the orientation of the bike racks to align east/west as opposed to north/south so that no rack encroaches into the ROW.
50. Will any above ground utility infrastructure be proposed (transformer, generator, utility pedestals, etc.)? If so, please show these elements on the site plan and ensure they are appropriately sited and screened. Please note that if a transformer is required, it must be located in the rear yard and the only viable located within the rear yard appears to be within the easement area as shown on Exhibit 1 of the summary judgement, which may not be allowed based on the easement language.
51. Please be aware that Section 6-8-1-10(A)4 allows 1 year from PD approval to obtain a building permit unless an extension is approved by the City Council. Upon request, a 1-year extension can be built into the approval ordinance, granting a total of 2 years to obtain a building permit upon approval of the PD. Any such request would need to be received prior to drafting an ordinance granting approval of the PD. Any further extension beyond this initial period of 2 years would need to be approved by the City Council.
52. Tree #1 does not appear feasible for preservation given the proposed new driveway for the co-op. Please reevaluate and modify the tree preservation plan as necessary.
53. Confirm whether tree #13 is viable for preservation given the proposed water line. Consider a revised location for the water service line to preserve the tree if preservation of the tree is not feasible given the proposed location.
54. Confirm whether tree #14 is viable for preservation given the proposed removal of the existing sanitary line and new line to be constructed in proximity.
55. At least 2-3 new parkway trees should be proposed along Central Street given the proposed tree removals. Please modify the landscape plan to incorporate additional parkway trees.
56. Please update the Statement of Development Allowances for Planned Developments to address the drive aisle width Site Development Allowance.

Public Works – Engineering:

57. IDOT completed work on this intersection in 2025.

Public Works – Water & Sewer

58. To clarify, portions of WCA's water and sewer services will be relocated to easements on the subject property.

Public Works - Waste

59. Please be aware of the services offered through the [Commercial Franchise](#).

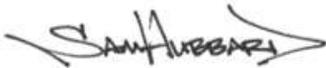
60. Consider calculating the amount of waste that is likely to be generated from this facility and determine the various container options on the webpage. The refuse enclosure must contain sufficient space to allow staff to access the full length of the dumpster for efficient use. Additionally, the refuse enclosure must contain sufficient space to hold both a recycling and trash dumpster; the drivers from LRS must have the ability to service either container without having to pivot, turn, or move other containers to service them.

Health Department:

61. A lead hazard mitigation plan must be submitted by a lead certified abatement professional that details containment, removal and disposal of lead components. It appears the building that is proposed to be demolished was built prior to 1978. This shall be required prior to demolition.

Notwithstanding the easement issues, it appears that the modifications required per the Round 2 comments above are fairly straightforward. Based on this, we can tentatively target the January 14, 2026, Land Use Commission meeting date for a public hearing on this application. However, in order to secure this date, please provide a response to these comments, along with revised plans, by December 4th to stay on track for the January 14th meeting date. Please note that only the documents that are revised as a result of these comments must be resubmitted; resubmission of documents that have not been revised are not required. Should you have any questions or concerns about the above comments, please do not hesitate to contact me directly at shubbard@cityofevanston.org.

Sincerely,



Sam Hubbard
Senior Planner

Cc: Jeremiah Bebo, Development Services Supervisor
Elizabeth Williams, Planning Manager
Sarah Flax, Director of Community Development

December 8, 2025

From: Charles Marlas
Kensington School
743 McClintock Dr
Burr Ridge, IL60527

To: Sam Hubbard
City of Evanston Zoning Division

RE: Planned Development Application – 25PLND-0021, Round 2 PRO Review Comments 3434
Central Street, Kensington School - **Responses in Bold**

Dear Mr. Hubbard,

Responses to the Round 1 Review Letter are below in **bold**:

Based upon review of the revised application, the following zoning approvals are required:

- A Special Use Permit for a Planned Development to allow a 2-story, 19,952 square foot Child Daycare Center (Section 6-8-1-10(D) and 6-8-3-3). **Confirmed.**
- Site Development Allowance to vary the requirements of Section 6-8-1-10(B) to allow a 5' wide landscape strip along the entire western property line and a 0' wide landscape strip along a portion of the southern property line, where Code requires a minimum 10' wide landscape strip. **Confirmed.**
- Site Development Allowance to vary the requirements of Section 6-8-3-9 to allow a 62.22% impervious surface lot coverage where impervious surface lot coverage is restricted to a maximum of 55%. **Confirmed.**
- Site Development Allowance to vary the requirements of Sections 6-8-3-7(B)1, 6-8-3-7(B)2, and 6-16-2-1(C) to allow parking within a front yard setback and a side yard setback where parking is prohibited in front and side yard setbacks. **Confirmed.**
- Site Development Allowance to vary the requirements of Section 6-4-6-7(F)3.a to allow a 6' tall dumpster enclosure fence and a 6' tall and 4'-5' tall playground fence within the front yard where fence heights in a front yard are restricted to 4' tall. **Confirmed.**
- Site Development Allowance to vary the requirements of Section 6-4-6-3(B), Table 4-A, to allow open off-street parking in a residential district that is greater than 30' setback from a rear property line where open off-street parking in a residential district shall be setback no greater than 30' from a rear property line. **Confirmed.**
- Site Development Allowance to vary the requirements of Sections 6-16-2-7(A) to allow a 16.5' wide drive aisle where a minimum drive aisle width of 24' is required (this applies to the drive aisle adjacent to the First Williamsburg Co-op parking stalls). **Confirmed.**

Planning & Zoning

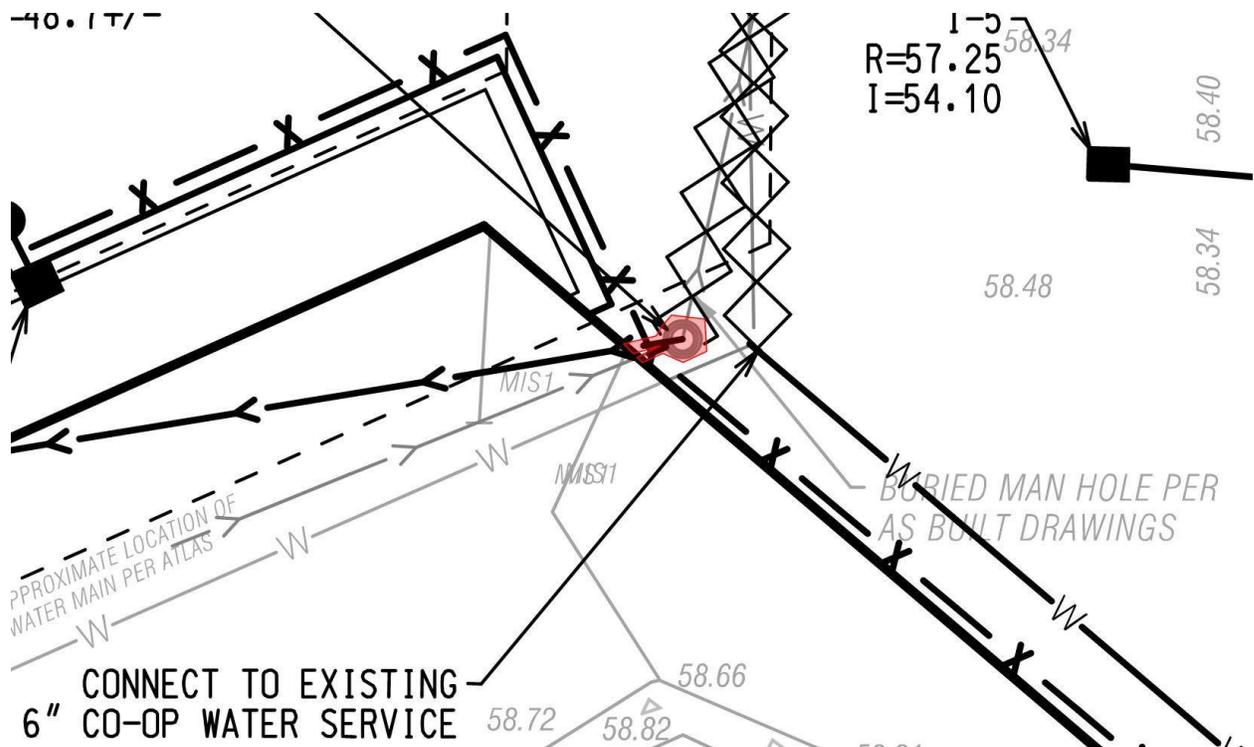
40. Your responses to the required Site Development Allowances are noted. An underground open-bottom stormwater vault does not make a concrete parking lot qualify for the credit allowed to paver blocks. However, we agree that the patio can be considered as attached to the building and part of the principal structure and therefore not subject to the accessory structure requirements. Additionally, we agree that the dumpster enclosure can be considered a fence and not subject to the accessory use standards, although it should be noted that the list of "Permitted accessory buildings, structures and uses" in Table 4-A is not all encompassing and there are structures, uses, and buildings that may qualify as accessory but may not be specifically listed within the table.

Response: Understood.

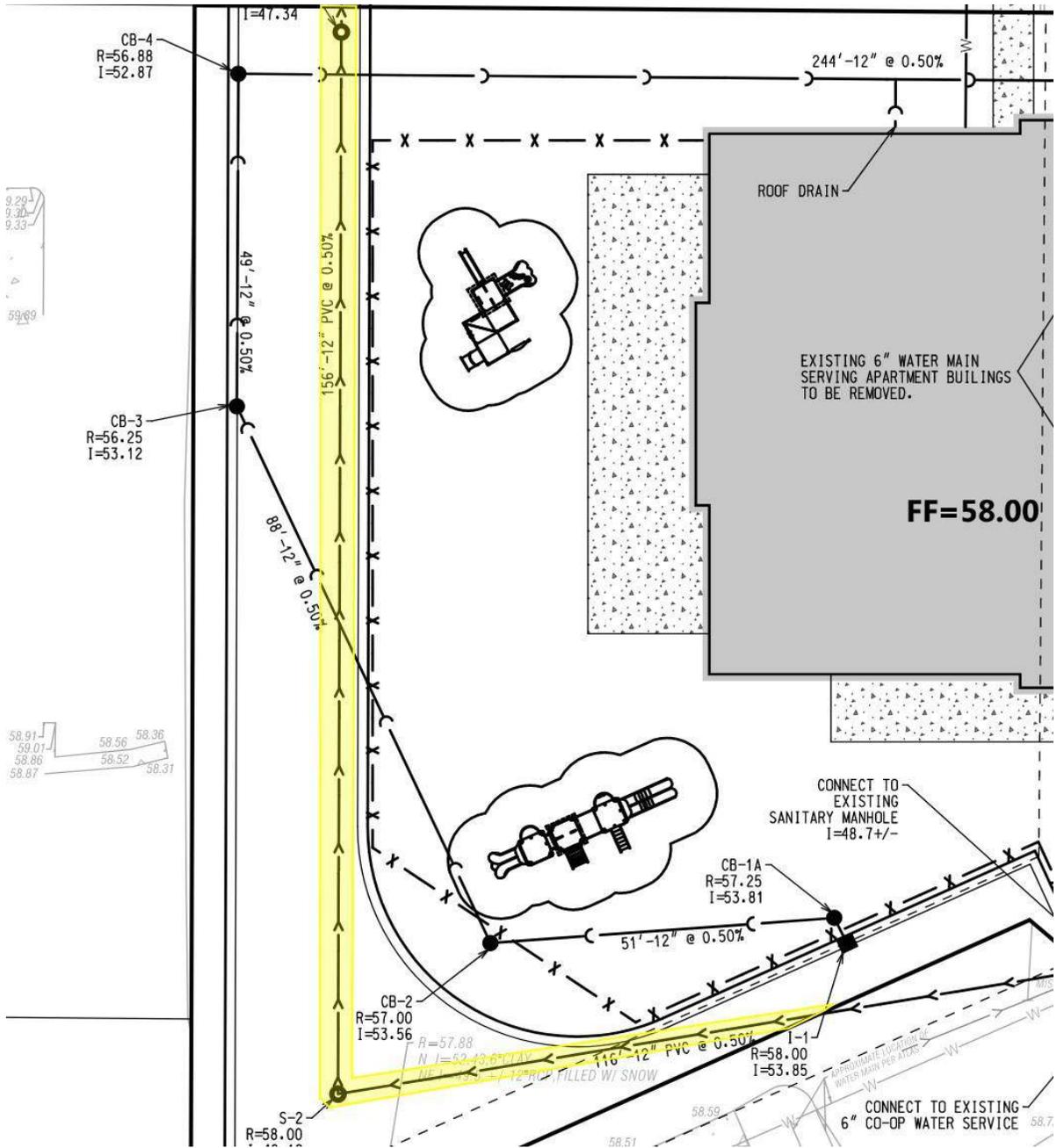
41. The responses to the following comments are acceptable: #1, #3, #6, #9, #10, #12, #13, #15, #16, #17, #18, #19, #20, #21, and #22.

Response: Understood.

42. The response to comment is noted, however, the existing easements on the site need to be clarified. The plans show a new private sanitary sewer line for the co-op is proposed on the Kensington school site. The area in red below must be within a private utility easement to the benefit of the co-op, or evidence of the existence of an easement in this area must be provided (the Plat of Survey only shows a ComEd easement in this area):



Furthermore, the newly proposed sanitary line serving the co-op, shown in yellow (below) falls within the easement area that is required by the Summary Judgement. Please confirm that the easement required to be recorded per #2 of the Summary Judgement will address not only ingress/egress but also this private sanitary line:



A new City water main will be constructed on the Kensington School property. The Plat of Survey does not show that a public utility easement exists to cover this water main, it only references an easement with ComEd for electrical utilities. Please confirm that a new public easement for the water main will be established.

Finally, the response to comment #2 did not address whether the developer has the rights to make these changes to the private sanitary sewer line serving the co-op. Additionally, is the co-op aware that a new sanitary line will be routed through a portion of their property?

Response: See updated site plan for revisions to the easement area and utility details. An enlargement of the proposed access easement is now shown to encompass the existing sanitary structure of the co-op. This area will also encompass the existing/future water line connection. Yes, the easement will also address the new private sanitary line. Yes, a new public utility easement will be established to cover the proposed water main that will serve the co-op. The proposed new

sanitary line for the co-op has been redesigned to be fully on Kensington property and within the proposed easement on the property, thus the line will not be routed through the co-op property. After Planned Development approval and prior to building permit submittal, this access easement and other easements still must be designed, reviewed, approved and finally recorded.

43. The response to comment #4 is noted, however, the revised easement area now no longer aligns with the Summary Judgement, Exhibit 1, which shows the easement as 26' in width. Please clarify.
Response: See updated site plan for revisions to the easement area that now align with City requirements. Per the previous City comments, we adjusted the easement area to be only the paved portion for access, and not any landscaped areas. We have now also extended a small easement area around the sanitary and water connections per this round of City comments. After Planned Development approval and prior to building permit submittal, this access easement and other easements still must be designed, reviewed, approved and finally recorded. The current Summary Judgement allows for the easement based on the site plan/City requirements and states the site plan exhibit clarifies the Summary Judgement approval. If the approved easement is a modification from the previous Summary Judgement, the new modification will be recorded to confirm compliance.
44. The response to comment #5 is noted, however, the contract extension document appears to have extended the Zoning Approval Period to July 2025. Please clarify.
Response: See extension. Further extensions will be provided to the City as needed.
45. The response to comment #7 is noted. Please be aware that LEED Gold or equivalent is a Code requirement and compliance with the IL Stretch Energy Code is a Code requirement; compliance with Code is not considered a public benefit. You're encouraged to continue developing the public benefits.
Response: We are continuing to explore public benefits.
46. The response to comment #8 is noted.
Response: Understood.
47. The response to comment #10 is acceptable. Please continue to include revision dates on any plan revised as a result of the Round 2 review comments.
Response: Understood.
48. The response to comment #11 is noted. Further incorporation of pavers is recommended to further reduce the extent of the variation requested. Additionally, please be aware that parking stalls are only required to be a minimum of 8.5' in width. The eastern most parking row, with 17 parking stalls at 9' in width, could be modified to 8.5' in width to pick up an extra parking stall or to remove a parking stall and preserve greenspace/lessen the impervious surface variation. Per Code, only 37 parking stalls are required.
Response: Additional pavers are unduly cost prohibitive. Kensington is already covering the cost for a new entrance point and access easement for the Co-Op parking, which is a significant hardscape area. If that hardscape area (pavement and land) is removed from the calculations, the remaining Kensington site is compliant with impervious surface coverage. Underground stormwater vaults are incorporated that also continue to release stormwater into the ground and not just into the release sewer. Parking spaces are purposefully 9 feet in width so that parents have ample space to get children into and out of carseats. The amount of parking provided is based on the anticipated need.
49. The response to comment #14 is noted. Please consider flipping the orientation of the bike racks to align east/west as opposed to north/south so that no rack encroaches into the ROW.
Response: Bike racks will not encroach into the ROW. Racks are reoriented.
50. Will any above ground utility infrastructure be proposed (transformer, generator, utility pedestals, etc.)? If so, please show these elements on the site plan and ensure they are appropriately sited and screened. Please note that if a transformer is required, it must be located in the rear yard and the only viable location within the rear yard appears to be within the easement area as shown on Exhibit 1 of the summary judgement, which may not be allowed based on the easement language. **Response: We do not know at this juncture if an electrical ground transformer will be required by the utility company. If it is required, then we will work with the utility provider and the City to make sure it is installed in an approved location. There are other utility easements to the south on our property. The easement shown in the judgement is primarily for Co-op access. When the property is replatted, easements for utilities will be formalized. We do not foresee any other ground utility infrastructure needed.**
51. Please be aware that Section 6-8-1-10(A)4 allows 1 year from PD approval to obtain a building permit

unless an extension is approved by the City Council. Upon request, a 1-year extension can be built into the approval ordinance, granting a total of 2 years to obtain a building permit upon approval of the PD. Any such request would need to be received prior to drafting an ordinance granting approval of the PD. Any further extension beyond this initial period of 2 years would need to be approved by the City Council.

Response: Although we do not expect to need additional time, we request one additional year to each time limitation in case of unforeseen circumstances.

52. Tree #1 does not appear feasible for preservation given the proposed new driveway for the co-op. Please reevaluate and modify the tree preservation plan as necessary.

Response: Tree will be removed and replaced. See updated tree plan.

53. Confirm whether tree #13 is viable for preservation given the proposed water line. Consider a revised location for the water service line to preserve the tree if preservation of the tree is not feasible given the proposed location.

Response: Tree #13 will be preserved. We will consider relocating the water line. However, if it cannot be relocated, we will directional bore under the root zone of the existing tree, with boring limits determined by an arborist.

54. Confirm whether tree #14 is viable for preservation given the proposed removal of the existing sanitary line and new line to be constructed in proximity.

Response: Tree #14 will be preserved and will not be impacted. The existing sanitary line will be abandoned in place, with ends capped as required by the City and MWRD.

55. At least 2-3 new parkway trees should be proposed along Central Street given the proposed tree removals. Please modify the landscape plan to incorporate additional parkway trees.

Response: Two new parkway trees will be proposed along Central Street. See updated tree plan.

56. Please update the Statement of Development Allowances for Planned Developments to address the drive aisle width Site Development Allowance.

Response: See update.

Public Works – Engineering:

57. IDOT completed work on this intersection in 2025.

Response: Understood.

Public Works – Water & Sewer

58. To clarify, portions of WCA's water and sewer services will be relocated to easements on the subject property.

Response: The Co-op's existing water and sewer lines that are on their property will not be altered. The existing water and sewer lines from the City connect to the Co-op's lines on Kensington's property. We will make the new connections at these same points, which are entirely within the proposed easement area on Kensington's property.

Public Works - Waste

59. Please be aware of the services offered through the [Commercial Franchise](#).

Response: Understood.

60. Consider calculating the amount of waste that is likely to be generated from this facility and determine the various container options on the webpage. The refuse enclosure must contain sufficient space to allow staff to access the full length of the dumpster for efficient use. Additionally, the refuse enclosure must contain sufficient space to hold both a recycling and trash dumpster; the drivers from LRS must have the ability to service either container without having to pivot, turn, or move other containers to service them.

Response: The refuse enclosure is sufficient in size and will have two containers that will meet the above criteria.

Health Department:

61. A lead hazard mitigation plan must be submitted by a lead certified abatement professional that

details containment, removal and disposal of lead components. It appears the building that is proposed to be demolished was built prior to 1978. This shall be required prior to demolition.
Response: Understood.

Sincerely,

A handwritten signature in black ink, appearing to read 'Charles Marlas', written in a cursive style.

Charles Marlas

Kensington School



City of Evanston
Planning Division
2100 Ridge Ave.
Evanston, IL, 60201-2798
T 847.448.4311
www.cityofevanston.org

December 22, 2025

Charles Marlas
Kensington School
743 McClintock Dr
Burr Ridge, IL 60527

RE: Planned Development Application – 25PLND-0021, Round 3 PRO Review Comments
3434 Central Street, Kensington School

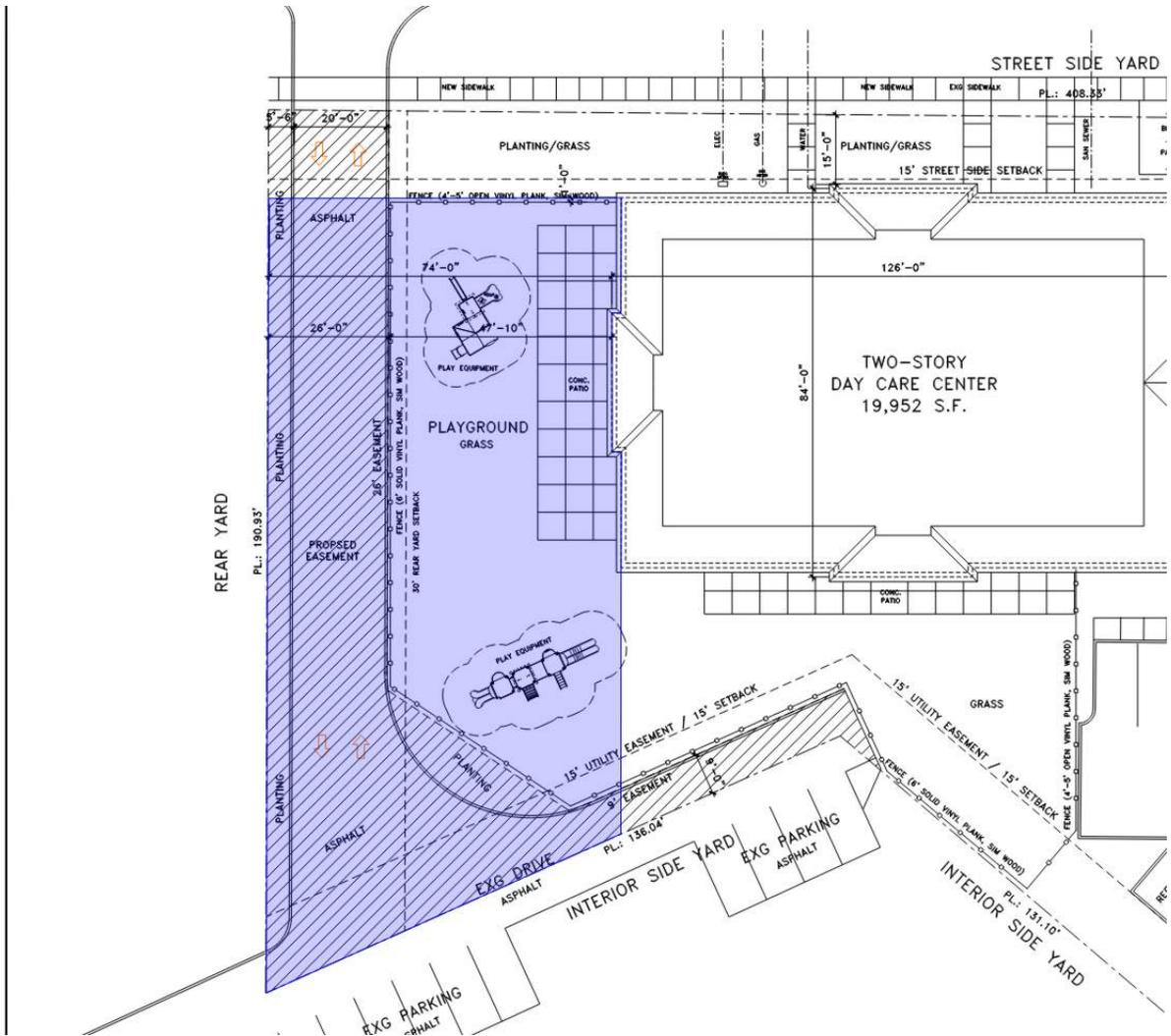
Dear Mr. Marlas,

Staff has reviewed the response to the Round 2 PRO review comments, along with the revised submittal materials for the Kensington School daycare proposed at 3434 Central Street. Please see our Round 3 PRO review comments below.

Based upon review of the revised application, the required zoning approvals are unchanged from the Round 2 review.

Planning & Zoning

62. The responses to the following comments are acceptable: #40, #41, #47, #49, #51-#56.
63. The response to comment #42 is acceptable and staff notes that easements shall be executed and recorded prior to any permit issuance.
64. The response to comment #43 is noted. The easement area aligns exactly with the physical area on the plans where access for the First Williamsburg Co-op will occur and utilities will be located. The easement area is in general alignment with the Summary Judgement. The final easement shall be executed and recorded prior to permit issuance.
65. The response to comment #44 is noted. Please provide evidence of further contract extension beyond the Dec. 6th date, as well as acknowledgement from the seller.
66. The responses to comments #45 and #46 are noted. Prior to appearing before the Planning & Development Committee of the City Council, the applicant shall finalize the proposed public benefits and provide details to the City.
67. The response to comment #48 is noted.
68. The response to comment #50 states that the property will be re-platted. Please clarify what is intended for re-platting? Should a transformer be required, please be aware that the only Code-compliant location would be in the area shown in purple below and any transformer in this area would need to be located no less than 3' setback from any property line.



Public Works – Engineering:

69. The response to easement issues and civil engineering plans are sufficient at this point.

Public Works – Water & Sewer

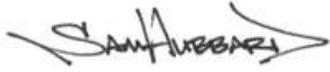
70. The Water Production Bureau has maintenance concerns for the new water main that will be installed off Gross Point Road. There will be access issues and restoration issues in the future due to the private infrastructure that will be placed over the new water main. It is recommended that the water lines for each individual building be replaced all the way to the water main on Gross Point Road.

71. The Sewer Division has a concern regarding the proposed sanitary line route, specifically the extreme bend out of Manhole (MH) S-2 and the existing manhole, which could lead to maintenance issues. Please clarify who will be responsible for maintaining the new sanitary line. It is strongly recommended that the new sanitary line be reconfigured to avoid extreme changes in the direction of flow, which will minimize potential problems and long-term maintenance needs.

As you're aware, this project is scheduled for a public hearing on January 14, 2026. To avoid a potential continuance at the hearing, please provide a written response to these comments, along with revised

plans (as applicable), no later than January 2, 2026. Should you have any questions or concerns about the above comments, please do not hesitate to contact me directly at shubbard@cityofevanston.org.

Sincerely,

A handwritten signature in black ink that reads "SAM HUBBARD". The signature is stylized with a large, sweeping initial "S" and a long, horizontal stroke extending to the right.

Sam Hubbard
Senior Planner

Cc: Jeremiah Bebo, Interim Planning Manager
Sarah Flax, Director of Community Development

January 2, 2026

From: Charles Marlas
Kensington School
743 McClintock Dr
Burr Ridge, IL 60527

To: Sam Hubbard
City of Evanston Zoning Division

RE: Planned Development Application – 25PLND-0021, Round 3 PRO Review Comments
3434 Central Street, Kensington School **Responses in Bold**

Dear Mr. Hubbard,

Responses to Round 3 PRO Review are below in **bold**:

Planning & Zoning

62. The responses to the following comments are acceptable: #40, #41, #47, #49, #51-#56.

Response: Understood.

63. The response to comment #42 is acceptable and staff notes that easements shall be executed and recorded prior to any permit issuance.

Response: Understood.

64. The response to comment #43 is noted. The easement area aligns exactly with the physical area on the plans where access for the First Williamsburg Co-op will occur and utilities will be located. The easement area is in general alignment with the Summary Judgement. The final easement shall be executed and recorded prior to permit issuance.

Response: Understood.

65. The response to comment #44 is noted. Please provide evidence of further contract extension beyond the Dec. 6th date, as well as acknowledgement from the seller.

Response: See attached attorney letter regarding the contract with the seller. The seller also intends to be present at the Land Use Commission meeting and speak in support of Kensington School.

66. The responses to comments #45 and #46 are noted. Prior to appearing before the Planning & Development Committee of the City Council, the applicant shall finalize the proposed public benefits and provide details to the City.

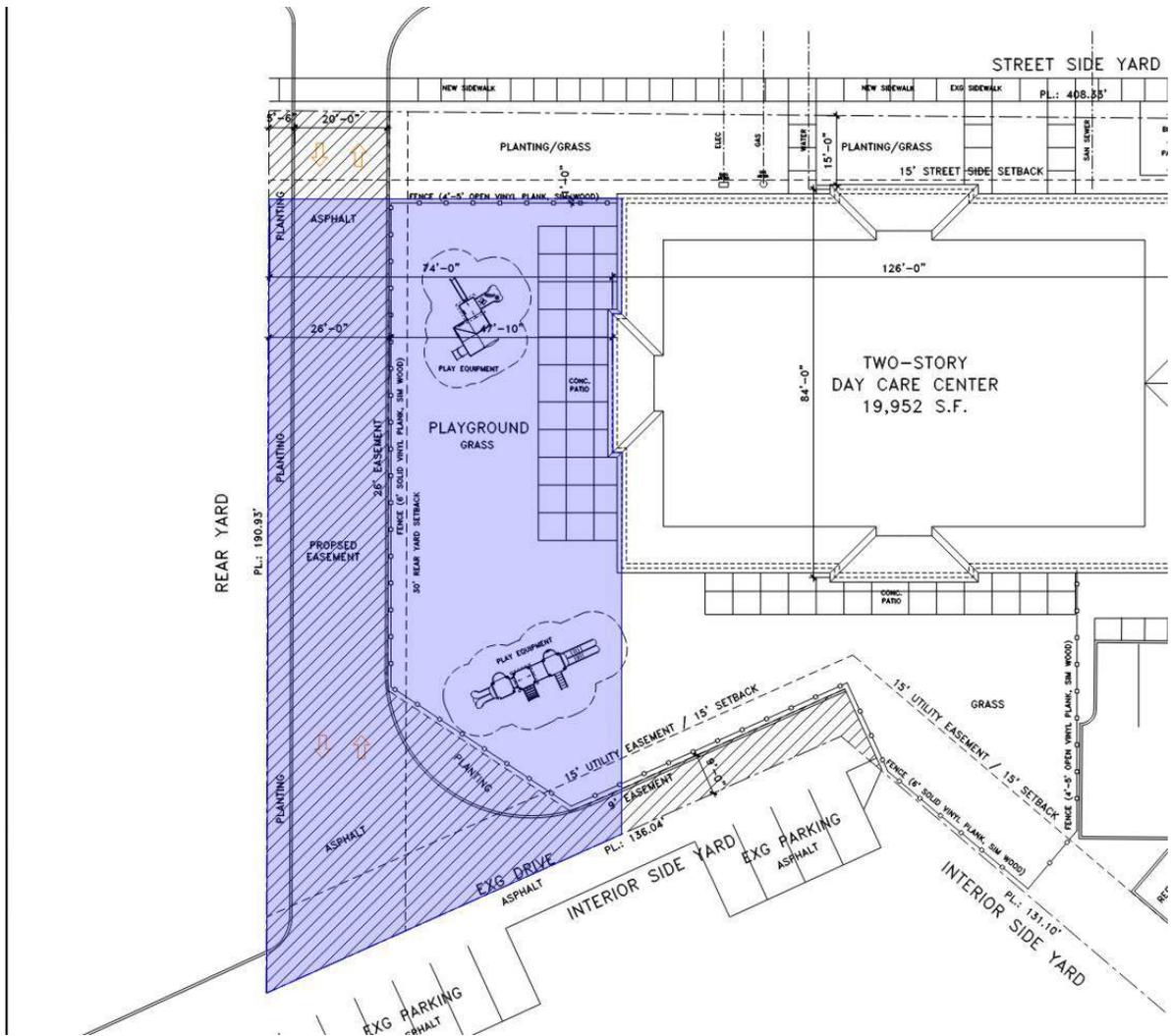
Response: See attached finalized public benefits.

67. The response to comment #48 is noted.

Response: Understood.

68. The response to comment #50 states that the property will be re-platted. Please clarify what is intended for re-platting? Should a transformer be required, please be aware that the only Code-compliant location would be in the area shown in purple below and any transformer in this area would need to be located no less than 3' setback from any property line.

Response: The replatting refers to the formal easement dedicating process (not a division of land). In our case, we need to vacate the existing access easement and grant new easements for access and utilities. The compliant transformer location is noted and is likely to be in or adjacent to the easement.



Public Works – Engineering:

69. The response to easement issues and civil engineering plans are sufficient at this point.

Response: Understood.

Public Works – Water & Sewer

70. The Water Production Bureau has maintenance concerns for the new water main that will be installed off Gross Point Road. There will be access issues and restoration issues in the future due to the private infrastructure that will be placed over the new water main. It is recommended that the water lines for each individual building be replaced all the way to the water main on Gross Point Road.

Response: The proposed new water main to serve the co-op is located in the utility easement as shown. We do not propose any infrastructure over the new water main other than grass. Our proposed design connects to the co-op's existing water services entirely on Kensington property and within the future easement. This design does not require work on the co-op's property of which we do not control. It is our understanding that the co-op would be responsible to replace water lines on their property if the City recommends such. If this recommendation for individual lines is desired and approved by the co-op, then Kensington would cooperate and facilitate.

71. The Sewer Division has a concern regarding the proposed sanitary line route, specifically the extreme bend out of Manhole (MH) S-2 and the existing manhole, which could lead to maintenance issues. Please clarify who will be responsible for maintaining the new sanitary line. It is strongly recommended that the new sanitary line be reconfigured to avoid extreme changes in the direction of flow, which will minimize potential problems and long-term maintenance needs.

Response: The proposed sanitary has been reconfigured per the recommendation. See attached revised plan. This sanitary line is solely for the use of the co-op, and will be entirely within the easement. We believe that the co-op will be responsible for maintaining their line.

Sincerely,



Charles Marlas

Kensington School

22. Public Comments

Fwd: 3434 Central St. / 25PLND-0021 / Planned Development1 message

On Mon, Jan 5, 2026 at 1:27 PM Lawrence Raffel <lawrence@lra.com> wrote:

The information contained in this email represents my comments relative to the complete opposition to the proposed development of Kensington School at the currently zoned residential property occupied by the Unity Church.

I have lived at 3509 Central St. (directly across Central St. from the proposed development) for 16 years. The proposed development clearly does not fit the site nor the character of the neighborhood as noted by the rendering and the request for six variances to place an extremely large development in a small triangular parcel. In fact, this development was previously denied at the corner of Hurd Ave. and Park Pl., and was rejected at this location by LUC at a previous public hearing.

The property is R2 Single-Family Residential and is bordered by residential lots on all sides. Mostly in Mid-Century architecture.

However, the most important detriment to the proposed development is safety. The traffic generated by Kensington School and the clear traffic patterns represent an extreme hazard to the neighborhood. The intersection of Central, Gross Point and Crawford is one of the worst intersections in Evanston. Thru traffic is currently prohibited on Central St. west of Gross Point and despite what is proposed by the Developer, users of the school will always take the easiest route. We currently only have a 30 inch wide carriage walk on the northside of the street. Most residents walk their strollers and dogs in the street. Traffic from the school is a serious accident waiting to happen as well as a clear traffic backup on west Central. St.

I am presenting these comments as a neighborhood resident. However, I am a Registered Landscape Architect and Professional Planner having retired after 40 years of professional practice. I have represented municipalities and developers before various approving and permitting agencies and I have been retained by all levels of governments to review and make recommendations on development projects relative to their communities. I have also served on the Evanston Plan Commission and the Recreation and Park Board. In all my experience, the development of Kensington School on this parcel represents one of the most inappropriate examples of site use and planning. And one of the most hazardous.

Again, I sincerely hope the Land Use Commission will use their good judgement and reject the argument by the Developer and the Unity Church that this is a desirable development for this parcel.

Sincerely,

Lawrence Raffel ASLA, PLA
Landsc
Email

Fwd: Kensington school

From:
Date: January 9, 2026 at 2:01:44 PM CST
To: zoning@cityofevanston.com
Subject: Kensington school

Hi,

As a 45 year, long time owner at 3429 Central, I am not in favor of Kensington school going in across the street where the church is.

Kensington was voted down a couple years ago with the traffic impact being one of the main problems. What they were proposing was just too big and high density for the space and the neighborhood .

Now they're back? Sounds like they added a couple parking spots and that's about it.

I'm out of town and will miss the meeting but I strongly oppose Kensington moving into and disrupting our quiet neighborhood.

Thank you,
Rita Heitzinger
Sent from my iPhone

Name: Craig McClure

Address of Residence: 2507 Princeton Ave

Phone: 

How would you like to make your public comment?: Written (see below)

Provide Written Comment Here: My husband and I live in the 2500 block of Princeton Avenue, around the corner from the proposed site for Kensington. We are opposed to the proposed Kensington School development at 3434 Central St as it is a commercial use for that residential parcel that is much too large and intense for our community. It will cause traffic issues throughout our neighborhood every weekday morning and evening. Kensington School's LaGrange location is evidence of the traffic issues that we anticipate.

On Friday September 12, 2025, I had an opportunity to park near Kensington School's LaGrange location from approximately 4:30 pm until almost 5:30 pm in order to observe the school during a busy pickup time. I took some videos of the activity from a distance as well which I have published into YouTube, and will send links separately.

I chose the LaGrange location at 125 N. Kensington Rd in LaGrange because after reviewing all of the Kensington locations on the map and then zooming in on them in Google Earth, LaGrange is the only Kensington location that is situated in a residential community similar to what is proposed here in Evanston. All of their other locations are in more commercial locations, with access off of busier streets, which given the fact that they are a large scale commercial daycare operator, is much more suitable.

A key point that I want to make in my comments is this – many of the parents who were picking up that afternoon I observed LaGrange DID NOT pull into Kensington's parking lot while they were going in to pick up their child. Many parked on either side of Kensington Rd while they ran in for pickup.

This points out a significant flaw in the proposed design for Kensington here in Evanston. An entrance into Kensington off of Central that cars going eastbound cannot turn into will not prevent what I saw in LaGrange. Many parents will simply park on Central St on either side for drop off or pickup. Kensington can say that they will enforce rules against that, but it is doubtful that Kensington would be willing over the long term to hire security to be outside during both morning and afternoon. And Evanston should not have to bear the cost of enforcing that either.

The proposed location here in Evanston also appears much larger than LaGrange, so the traffic issues will undoubtedly be even worse.

Overall, the proposed Kensington School is much too intense of a commercial use for that parcel. And the price point that they are at for

their daycare will attract traffic from affluent communities to the north of us who will come across Thayer, down Princeton and across Central to get to the school.

We understand Unity Church's need to sell, however this proposed commercial use is the wrong use for that residential parcel.

**Agenda Item
(or comment
on item not on
the agenda):**

3434 Central St - 25PLND-0021 (Kensington School)

**Position on
Agenda Item:**

Opposed



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Intellistack, 50 South Steele Street, Suite 500, Denver, CO 80209

Name (first and last)	Address of residence or property owned	Meeting date	Agenda Item (Property address or description of agenda item)	Position on Agenda Item	How would you like to make your public comment?	If you are providing a written comment, please leave here:	Are you representing yourself as an individual or speaking on behalf of a group?	Please name the group of people for whom you are the designated speaker.
John Cooper	3614 Hillside Road	1/14/2026	Kensington School Resubmission	Opposed	Written comment	The primary issue is traffic/safety. As before, the intersections of Central, Gross Point, and Crawford are currently stressed. Currently, at times in the morning and evening, back ups occur daily during the week. In addition, cars often just pile up within the triangle blocking other traffic. This is a known issue. As I understand the new proposal does nothing to change this issue and, in fact, could not. It is impossible to see how the intersection could accommodate the predictable traffic. This was a primary reason to reject in the first go around. What are they thinking? Evanston does not need this in this location.		
Lisa Faremouth Weber	2752 Marcy Ave		Unity Church	In favor	Written comment	I am in full support of Kensington School purchasing the property where Unity Church is currently located because: It will return Unity's property to the city's property tax rolls (as a church, the property is currently tax-exempt). Kensington School designs aesthetically pleasing buildings that fit well within surrounding neighborhoods. Traffic studies show minimal impact to the neighborhood, and the overall benefits outweigh the limited traffic footprint. Quality childcare is a necessary and much-needed service for our community. The project will create new jobs, including both construction and childcare positions. Too much time has been wasted on blocking this opportunity for change and growth.		
Cheryl Judice	1000 Maple Avenue, Evanston, IL. 60202	1/14/2026	Sale of Unity Church to Kensington School	In favor	Written comment	I am writing in support of the sale of the Unity Church property located at 3434 Central Street in Evanston to Kensington School. Selling the church property to Kensington School would return it to the tax rolls, while providing a valuable service (childcare) to the Evanston community. Thank you.		
John Lindner	3214 Thayer Street	1/14/2026	Unity on the Northshore/Kensington Schools	In favor	Written comment	I fully support the purchase of the Unity property by Kensington schools. Evanston needs more properties on the tax rolls to keep costs down and our community vital. Kensington School has been a loyal and patient partner to the church. This has been going on for years now. Their company designs buildings that fit well within surrounding neighborhoods. The worry about the traffic is unfounded. Neighbors believe that traffic will flow like it does at an elementary. The school has demonstrated that as a PRE SCHOOL, traffic is scattered throughout the day as these little ones are not on a fixed schedule. They have addressed ingress and egress issues causing minimal impact to the neighborhood. The project is good for Evanston by creating jobs, including construction and ongoing staff positions. Please stop making it so hard to do business in Evanston.		
David Baker	3147 Grant Street	1/14/2026	Sale of Unity property to Kensington- 3434 Central St	In favor	Written comment	Dear LUC members, I strongly support the sale of the property at 3434 Central St from Unity on the North Shore to Kensington preschools. I have been an Evanston resident for 24 years, and I am a member of Unity and President of the Board. This sale will be beneficial to our community, and it will allow Unity to move on after several years of legal challenges. The main Unity building is unusable, and we have been meeting in the basement for four years! Approval of this sale will allow our congregation to move forward, ideally from a new location that is more accessible to all Evanston residents. In addition, Evanston needs quality childcare, and the project will create new jobs, including both construction and childcare positions. Kensington School designs beautiful buildings and makes sure their buildings are appropriate for their neighborhoods. Concerns have been raised about traffic, but studies show there would be minimal impact to the neighborhood, and the overall benefits outweigh any minor effects. Finally, the city will have Unity's property returned to its tax rolls (as a church, the property is currently tax-exempt). Please vote in favor of this sale!		
Catherine Lindner	3020 Hartzell Street	1/14/2026	Kensington School's proposal to purchase the Unity of the Northshore Church	In favor	Written comment	I believe that the sale of the Unity of the Northshore's property to Kensington School should be approved for several reasons: 1. Kensington School is an attractive building that fits in well with the neighborhood. 2. Traffic studies show that there will be minimal impact to traffic. Frankly, any development will be more than the current use but given church attendance in general it is not plausible to believe that Unity could sell to another congregation. The limited traffic during key times for drop off and pick up are certainly less than a business that is open 7 days a week. 3. Quality child care is lacking in the area and Kensington School provides another solution for working families. 4. The project creates jobs both in building and running the school! 5. MOST IMPORTANTLY - it returns the property to the tax rolls! Evanston needs the tax revenue! Please approve the proposal. Sincerely, 25 year Evanston resident and home owner - Catherine Lindner		

Ann Baker	3147 Grant St. 60201		Unity Church Property sale to MacKenzie School at 3434 Central St.	In favor	Written comment	As a member of the Unity Church and local resident I am in favor of the sale of Unity church property to the preschool for many reasons. Kensington school will provide a needed service to the community. They have a track record of being good neighbors and building nice looking buildings that would add value to the whole area and return the property to taxable status. Traffic studies have shown minimal impact. I negotiate that intersection frequency and have never encountered issues. Unity as an organization has been on hold for the past 5 years waiting for approval to sell their property. This has negatively impacted the community as future planning has been impossible. Please vote in favor of this to allow both the neighborhood and Unity community to move forward. Thank you.		
Michael Cholewa	3419 Central St Evanston Illinois 60201	1/14/2026	Kensington School	Opposed	Written comment	There is way to much traffic especially monday through friday 630am -8am and 3pm-6:30pm. This area cannot accomodate any more mass structures that attract more people during this time. Adding a school here would be a disaster for Evanston and the people that live here. I am totally against this proposal. Do not allow it.		

23. Public Notification Documents

Sold To:
City Of Evanston (Print) - CU00039704
2100 Ridge Ave
Evanston,IL 60201-2716

Bill To:
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Certificate of Publication:

Order Number: 7909785
Purchase Order: LUC Meeting 01.14.26

State of Illinois - Cook

Chicago Tribune Media Group does hereby certify that it is the publisher of the Evanston Review. The Evanston Review is a secular newspaper, has been continuously published Weekly for more than fifty (50) weeks prior to the first publication of the attached notice, is published in the City of Evanston, Township of Evanston, State of Illinois, is of general circulation throughout that county and surrounding area, and is a newspaper as defined by 715 IL CS 5/5.

This is to certify that a notice, a true copy of which is attached, was published 1 time(s) in the Evanston Review, namely one time per week or on 1 successive weeks. The first publication of the notice was made in the newspaper, dated and published on 12/25/2025, and the last publication of the notice was made in the newspaper dated and published on 12/25/2025.

This notice was also placed on a statewide public notice website as required by 715 ILCS 5/2. 1.

PUBLICATION DATES: **Dec 25, 2025.**

Evanston Review

In witness, an authorized agent of The Chicago Tribune Media Group has signed this certificate executed in Chicago, Illinois on this

26th Day of December, 2025, by

Chicago Tribune Media Group



Jeremy Gates

OFFICIAL NOTICE - CITY OF EVANSTON

The Evanston Land Use Commission will conduct a public hearing in the James C. Lytle Council Chambers, on the second floor of the City Hall, 909 Davis St., Evanston, Illinois, at 7:00 p.m., on Wednesday, January 14, 2026. All persons who are interested are invited to attend the public hearing in person.

For those unable to attend, they may provide comments in advance by completing the Land Use Commission's online form at <https://bit.ly/lucpubliccomment> or by emailing the Planning & Zoning Division at zoning@cityofevanston.org. Community members may also watch the Land Use Commission meeting in progress online at www.cityofevanston.org/channel16 or on Cable Channel 16. The Land Use Commission will consider the following matters:

Planned Development | 3434 Central Street | #25PLND-0021

Charles Marlas, applicant, requests approval of a Special Use for a Planned Development and a Special Use to allow a "Daycare Center, Child", all to allow demolition of the existing church and construction of a two-story children's daycare on the subject property in the R2 Single-Family Residential District. The application seeks approval of the following Site Development Allowances:

- Section 6-8-1-10(B): to allow a 5' wide landscape strip along the entire western property line and a 0' wide landscape strip along a portion of the southern property line, where Code requires a minimum 10' wide landscape strip.
- Section 6-8-3-9: to allow a 62.22% impervious surface lot coverage where impervious surface lot coverage is restricted to a maximum of 55%.
- Sections 6-8-3-7(B)1, 6-8-3-7(B)2, and 6-16-2-1(C): to allow parking within a front yard setback and a side yard setback where parking is prohibited in front and side yard setbacks.
- Section 6-4-6-7(F)3.a: to allow a 6' tall dumpster enclosure fence and a 6' tall and 4'-5' tall playground fence within the front yard where fence heights in a front yard are restricted to 4' tall.
- Section 6-4-6-3(B), Table 4-A: to allow open off-street parking in a residential district that is greater than 30' setback from a rear property line where open off-street parking in a residential district shall be setback no greater than 30' from a rear property line.
- Section 6-16-2-7(A): to allow a 16.5' wide drive aisle where a minimum drive aisle width of 24' is required (this applies to the drive aisle adjacent to the First Williamsburg Co-op parking stalls).

The applicant may seek and the Land Use Commission may consider additional Site Development Allowances as may be necessary or desirable for the proposed development. The Land Use Commission makes a recommendation to the City Council, the determining body for this case, in accordance with Section 6-3-6 of the Evanston Zoning Ordinance. PIN: 10-10-200-073-0000.

Special Use | 1710 Maple Avenue | #25ZMJV-0051

Viewpoint Consulting LLC d/b/a Dogtopia of Downtown Evanston, applicant, requests a Special Use to allow a Daycare Center - Domestic Animal and Kennel (section 6-12-2-3) at 1710 Maple Avenue in the RP Research Park District. The use currently operates at 900 Clark Street and is proposing to relocate to the Maple Avenue location. The Land Use Commission makes a recommendation to the City Council, the determining body for this case, in accordance with Section 6-3-5 of the Evanston Zoning Ordinance. PIN: 11-18-117-013-0000

The discussion may be adjourned (continued) from time to time to dates certain without additional notice. The meeting agenda with application materials and staff report will be posted on the City's website for public viewing, <https://www.cityofevanston.org/government/boards-commissions-and-committees/land-use-commission>, by Friday, January 9, 2026. Questions regarding these applications can be directed to Meagan Jones, Neighborhood and Land Use Planner, at mmjones@cityofevanston.org or (847) 448-4311. 12/25/2025 7909785

NOTICE OF A PUBLIC HEARING

Land Use Commission

Wednesday, January 14, 2026 at 7:00 pm

909 Davis Street - Council Chambers

Please be advised, as you own, or otherwise may have interest in a property within 1,000 ft. of the address listed below, the following proposal will be discussed:

3434 Central St. | 25PLND-0021 | Planned Development

Charles Marlas, applicant, of the Kensington School, submits for a Special Use for a Planned Development and Special Use to allow a “Daycare Center, Child” to allow demolition of the existing building on the property and construction of a new two-story children’s daycare on the subject property in the R2 Single-Family Residential District. The applicant requests the following Site Development Allowances: 1) to allow a 5’ wide landscape strip along the western property line and a 0’ wide landscape strip along a portion of the southern property line where Code requires a 10’ wide landscape strip; 2) to allow a 62.22% impervious surface lot coverage where coverage is limited to a maximum of 55%; 3) to allow parking within a front yard setback (east) and a side yard setback (north) where parking is prohibited; 4) to allow a 6’ tall dumpster enclosure fence and a 6’ tall and 4’-5’ tall playground fence within the front yard where fence heights are restricted to 4’ tall; 5) to allow open off-street parking in a residential district that is greater than 30’ setback from a rear property line where a setback is limited to no greater than 30’; 6) to allow a 16.5’ wide drive aisle where a minimum drive aisle width of 24’ is required (this applies to the drive aisle adjacent to the First Williamsburg Co-op parking stalls. The applicant may seek, and the Land Use Commission may consider, additional Site Development Allowances as may be necessary or desirable for the proposed development. The Land Use Commission makes a recommendation to the City Council, the determining body for this case. PIN: 10-10-200-073-0000



Those wishing to make public comments at the Land Use Commission meeting may submit written comments in advance or sign up to provide public comment by calling/texting 847-448-4311 or completing the Land Use Commission online comment form available online here: https://arts.formstack.com/forms/land_use_commission_public_comment. Information about the Land Use Commission is available online at www.cityofevanston.org/government/land-use-commission. Questions can be directed to 847-448-4311. The City of Evanston is committed to making all public meetings accessible to persons with disabilities. Any citizen needing mobility or communications access assistance should contact the Community Development Department 48 hours in advance of the scheduled meeting so that accommodations can be made at 847-448-8170 (Voice) or 847-866-5095 (TDD). La ciudad de Evanston está obligada a hacer accesibles todas las reuniones públicas a las personas minusválidas o las quines no hablan inglés. Si usted necesita ayuda, favor de ponerse en contacto con la Oficina de Administración del Centro a 847-448-4311 (voz) o 847-866-5095 (TTY).



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A black sign on a metal stand with a white background. The top line reads "PUBLIC HEARING" in bold black letters. Below this, there is a small graphic of a house and some illegible text. The sign is placed on a grassy area next to a sidewalk.